

# “PlanGretna”

Gretna Comprehensive Plan 2009

Ordinance No. 946 - March 3, 2009

Gretna Comprehensive Plan Update 2014

Ordinance No. 1051 - December 16th, 2014



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*Gretna Chamber of Commerce*

*Gretna Area Development Corporation*



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# Introduction to PlanGretna

Introduction

The Purpose of Comprehensive Planning

The Comprehensive Planning Process

Comprehensive Plan Components

Governmental and Jurisdictional  
Organization



# INTRODUCTION

Gretna is one of the fastest growing communities in the State of Nebraska. While growth is necessary and a sign of prosperity for a city, it is also a challenge to manage that growth. Cities that take the time to plan for and manage growth typically see a longer growth period and maintain their status as a desirable place to live. With the development of this Comprehensive Plan, referred to as “PlanGretna”, the City of Gretna is well on its way to maintaining their status as one of Nebraska’s premier cities to live and work.

PlanGretna was a joint effort between the City of Gretna, Gretna School District, Gretna Chamber of Commerce, and the Gretna Area Development Corporation. The city and the school district contributed each 40% of the cost, while the chamber and development corporation each contributed 10%. This joint effort utilized many resources and analyzed many opportunities not only within Gretna’s corporate limits, but it’s current one-mile extraterritorial jurisdiction, two and three mile future growth areas, and the Gretna School District. This comprehensive approach to planning for the City of Gretna and its surrounding area will certainly benefit the community through efficient quality growth as all levels of develop occur. This “team approach” had been recognized with an award by the Nebraska Planning and Zoning Association in 2008. PlanGretna also won Best Practice in Excellence in Planning in 2010 from American Planning Association Nebraska Chapter.

## **2014 Update to PlanGretna**

PlanGretna was adopted in the spring of 2009. After five years of growth and new census information available, the City of Gretna decided to update their comprehensive plan. A new Profile chapter includes the energy element initiated by Nebraska legislation LB 997.

New development, such as the widening of Highway 370 and the redevelopment of the outlet mall, prompted the need for master planning. The quarter-sections along the Highway 370 corridor east of town and quarter-section along Highway 6/31 south to the new outlet mall were looked at with greater detail. Therefore, updated special character areas and corresponding maps are incorporated into the 2014 Comprehensive Plan update.

## **Location**

Gretna is located on Interstate 80 about 10 miles southwest of Omaha and 40 miles northeast of Lincoln, Nebraska. The Platte and Elkhorn Rivers are only a few miles to the west giving Gretna residents easy access to some of the Nebraska’s best outdoor recreation as well as the State’s two largest employment centers.

## **Climate**

The temperature for Gretna and Sarpy County is varied, ranging from an average low of 13 degrees during the month of January to an average high of 88 during the month of July. The average annual precipitation is 28.4 inches.

### **Topography and Soils**

The terrain in the current corporate limits is level to gently rolling, however outside the corporate limits there are many areas of steep slopes and the Platte River Floodplain is only a few miles west. Throughout Sarpy County the terrain is quite diverse, ranging from level to hilly. The soils in the area around Gretna are in the Ponca-Ida association, which are deep, well-drained, strongly sloping to steep silty soils on bluffs adjacent to the Elkhorn River Valley, and the Marshall-Ponca association which are deep, well-drained, nearly level to moderately steep silty soils on loess uplands.

### **History**

Gretna was founded in 1886 on 80 acres of land that was purchased from the Lincoln Land Company for \$560 along the new Burlington Northern Railroad line. This new rail line essentially moved the town of Forrest City to what is now Gretna. Incorporated on July 10<sup>th</sup>, 1889 Gretna's name is inspired by Scotland's Gretna Green, many of the early settlers of the village came from Scottish backgrounds/ancestry.

## **THE PURPOSE OF COMPREHENSIVE PLANNING**

The Gretna Comprehensive Development Plan is designed to promote orderly growth and development for the City, provide policy guidelines, and enable citizens and elected officials to make informed decisions about the future of the City. The Plan acts as a tool to “Develop a road map or blueprint that guides the community through change as it occurs tomorrow and ten to twenty years from now.”

The Comprehensive Development Plan will provide a guideline for the location of any future developments within the planning jurisdiction of Gretna. The Gretna Comprehensive Plan is intended to encourage a strong economic base so the goals of the City are achieved.

PlanGretna will assist the community in evaluating the impacts of development (i.e. economic, social, fiscal, service and amenity provision, health, safety and general welfare) and encourage appropriate land uses throughout the jurisdictional area of Gretna. The Plan assists the City in balancing the physical, social, economic, and aesthetic features as it responds to private sector interests. Planned growth will make Gretna more effective in serving residents, more efficient in using resources, and able to meet the standard of living and quality of life every individual desires.

### **MISSION STATEMENT**

*“PlanGretna influences the advancement of our community through the understanding of commonly held preferences, establishment of intelligent policies, and encouragement of flexible decision-making strategies.”*

## THE COMPREHENSIVE PLANNING PROCESS

Comprehensive planning begins with the data collection phase including demographic information from the 2000 and 2010 Census, information obtained from city staff, public input, and field data collection. Data are collected that provide a snapshot of the past and present conditions of Gretna. Analysis of data provides the basis for developing forecasts for future land-use demands in the City.

The second phase of the planning process is the development of general goals and objectives or policies, based upon the issues facing the City prioritized by the community through public input. These are practical guidelines for improving existing conditions and guiding future growth. The Comprehensive Plan is a vision presented in text, graphics, and tables that represent the desires of the City for the future.

PlanGretna represents a blueprint designed to identify, assess, and develop actions and policies in the areas of population, land use, transportation, housing, economic development, community facilities, and utilities. The Comprehensive Plan contains recommendations that when implemented will be of value to the City of Gretna and its residents.

Implementation is the final phase of the process. A broad range of development policies and programs are required to implement the Comprehensive Plan. PlanGretna identifies the tools, programs, and methods necessary to fulfill the recommendations. Nevertheless, the implementation of the development policies contained within the Comprehensive Plan is dependent upon the adoption of the plan by the governing body, and the leadership exercised by the present and future elected and appointed officials of the City.

PlanGretna was prepared under the direction of the Comprehensive Plan Steering Committee. The 15-member steering committee was composed of members of city staff, city council, planning commission, board of adjustment, chamber of commerce, Gretna Area Economic Corporation, and concerned citizens of Gretna. Review and recommendations by the Gretna Planning Commission and Gretna School Board were completed prior to adoption of PlanGretna by the Gretna City Council. The planning time period for achieving goals, objectives, programs, and developments identified in the Gretna Comprehensive Plan is up to 20 years. However, the City should review the plan annually or biannually and update the document completely every ten to fifteen years, or when a pressing need is identified. Updating the Comprehensive Plan will allow the City to incorporate ideas and developments that were not known at the time of the present comprehensive planning process.

## COMPREHENSIVE PLAN COMPONENTS

Nebraska State Statutes require the inclusion of certain elements in a Comprehensive Plan. A “Comprehensive Development Plan,” as defined in Neb. Rev. Stat. § 19-903 (Reissue 1997), “shall consist of both graphic and textual material and shall be designed to accommodate anticipated long-range future growth.” PlanGretna is comprised of the following components:

- Profile Gretna
- Envision Gretna
- Achieve Gretna
- Implement Gretna

Analyzing past and existing demographic, housing, economic, and social trends permits the projection of likely conditions in the future. Projections and forecasts are useful tools in planning for the future; however, these tools are not always accurate and may change due to unforeseen factors. Past trends may also be skewed or inaccurate, creating a distorted picture of past conditions. Therefore, it is important for Gretna to closely monitor population, housing and economic conditions that may impact the city. Through periodic monitoring, the City can adapt and adjust to changes at the local level. Having the ability to adapt to socioeconomic change allows the City to maintain an effective Comprehensive Plan for the future, to enhance the quality of life, and to raise the standard of living for all residents in Gretna and its jurisdiction.

PlanGretna records where Gretna has been, where it is now, and where it will likely be in the future. This is an information and management tool for City leaders to use in their decision-making process when considering future developments. The Comprehensive Plan is not a static document; it should evolve as changes in the land-use, population, or local economy occur during the planning period. This information is the basis for Gretna’s evolution as it achieves its physical, social, and economic goals.

## GOVERNMENTAL AND JURISDICTIONAL ORGANIZATION

The planning jurisdiction of the City of Gretna includes the area within one mile of the corporate limits as authorized under the authority of Section 17-001, Nebraska Revised Statutes, 1943 (amended). The City may enforce zoning and subdivision regulations including building, electrical, and plumbing codes within its planning jurisdiction. PlanGretna also recognizes the current future land use designations of the adjacent jurisdictions of Sarpy and Douglas Counties and has established a two and three-mile growth area for Gretna. Recognition of surrounding land uses within Gretna's School District and planning for future growth outside its current jurisdiction will help the City of Gretna establish land use designations and criteria when future annexations and extension of zoning jurisdiction occur.

The Gretna City Council, which is a board of elected officials including the Mayor, performs the governmental functions for the City. The planning and zoning jurisdiction of Gretna, pursuant to Neb. Rev. Stat. § 19-901 through 19-933 (Reissue 1997), includes all of the incorporated portions of the City, including an established extraterritorial jurisdiction of one-mile. As the City reaches a first-class city designation, the City of Gretna may decide to adopt a two-mile zoning jurisdiction.

# Profile Gretna

Introduction

Demographic Profile

Housing Profile

Economic and Employment Profile

Community Facility Profile

Existing Land Use Profile



# Profile Gretna

## Introduction

“Profile Gretna” is a snapshot of the community as it is today. The existing features, the people, land use, economics, and community facilities are all important aspects of a community that need to be acknowledged and understood before future plans can be proposed.

This section of the comprehensive plan is comprised of a demographic profile, housing profile, economic and employment profile, community facility profile, and existing land use. The demographic and housing profiles identify and analyze population trends, age structure, migration, race characteristics, and housing characteristics in Gretna. Residential land use needs and population projections are then generated from this information. The economic and employment profile illustrates where residents are working and how sustainable the community is.

The last two sections of Profile Gretna are related more specifically to how the land in and around Gretna is used and what services are available. The community facility profile identifies all the public facilities and services available in Gretna and the surrounding area, including recreation, education, fire and police protection, City, communication and health facilities; public utilities; and environmental conditions. The final section pertaining to existing land use identifies how the land is used in the corporate limits, one-mile jurisdiction, and school district by placing uses into one of 11 existing land use categories.

# Demographic Profile

- Population Trends and Analysis
- Age Structure Analysis
- Migration Analysis
- Race Characteristics
- Population Projections



# Demographic Profile

Population is the driving force behind housing, local employment, economic, and the fiscal stability of the community. It is important for the community to understand where it has been, where it is, and where it appears to be going. Population statistics aid decision-makers by painting a picture of the community. Historic population conditions assist in developing demographic projections, which in turn assist in determining future housing, retail, medical, employment and educational needs within the community. Projections provide an estimate for the community, from which to base future land-use and development decisions. However, population projections are only an educated calculation for the future, and unforeseen factors can significantly affect those projections.

## POPULATION TRENDS AND ANALYSIS

Table 1 identifies the population for Gretna and other incorporated communities in Sarpy County for the years 1980, 1990, 2000, and 2010. This information provides the community with an understanding of the county and communities' population trends and changes. Gretna's population for 2010 was 4,905 persons, which was an increase of 3,296 persons, or 204.8%, from 1980. Sarpy County's population, as a whole, changed by 84.7%; with all communities/incorporated areas increasing their population by 125.8%.

TABLE 1 POPULATION COMPARISONS

	1980	1990	CHANGE '80 - '90	2000	CHANGE '90 - '00	2010*	CHANGE '00 - '10	TOTAL CHANGE	2008-2012*
<b>Gretna</b>	<b>1,609</b>	<b>2,249</b>	<b>40%</b>	<b>2,355</b>	<b>5%</b>	(4,441) <b>4,905</b>	<b>89%</b>	<b>176%</b>	(4,807) <b>5,271</b>
Bellevue	21,813	30,982	42%	44,382	43%	50,137	13%	130%	51,293
La Vista	9,588	9,840	3%	11,699	19%	15,758	35%	64%	16,185
Papillion	6,399	10,372	62%	16,363	58%	18,894	16%	195%	19,618
Springfield	782	1,426	82%	1,450	2%	1,529	5%	96%	1,390
Incorporated Areas	40,191	54,869	37%	76,249	39%	90,759	19%	126%	93,293
Unincorporated Areas	45,824	47,714	4%	46,346	-3%	68,081	47%	49%	66,120
Sarpy County	86,015	102,583	19%	122,595	20%	158,840	30%	85%	159,413

\*estimates

Sources: U.S. Census Bureau, Census of Population and Housing, 1980 - 1990, 2000, 2010 and ACS 2008 - 2012 5-year estimates  
 \* Note: Gretna's 2010 Census Data is a missing census tract (464 people). The 2010 total population is corrected; however, multiple demographics were not updated. The 2008-2012 estimates use local surveys and 2010 Census statistics which has affected the associated estimates.

### 2010 Gretna Population

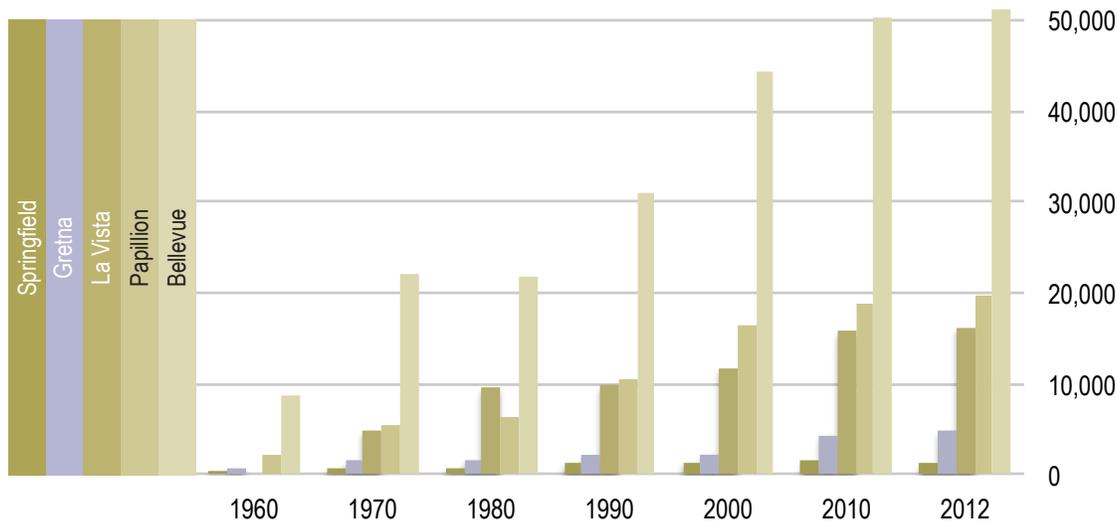
Gretna's 2010 Census Data was missing a census tract of 464 people. The 2010 total population was corrected to 4,905; however, multiple demographics categories were unable to be updated. The 2008-2012 estimates use local surveys and 2010 Census statistics which may have affected associated estimates.

Gretna has been the second fastest growing community in Sarpy County over the past 30 years as Papillion grew the most. The growth of these two cities is likely to continue at a faster rate than the rest of the county due to geographic issues. The two communities are growing toward one another along Nebraska Highway 370 and Interstate 80 (Exit 439).

The 2012 Population estimate derives from the US Census Bureau's methodology and continues to receive surveys throughout the year. The corrected 2012 Five-year estimate data shows Gretna surpassing the State of Nebraska's First Class City status based upon the population of 5,000. As a First Class City, Gretna will have the option to expand its Extraterritorial Jurisdiction to two miles.

Figure 1 shows a historical perspective of the population between 1960 and 2012 for communities within Sarpy County. This figure visualizes how the communities in the county have grown in comparison to one another. Bellevue is the most populous due its historical establishment and influenced by Offutt Air Force Base.

FIGURE 1 SARPY COUNTY POPULATIONS



Sources: U.S. Census Bureau, Census of Population and Housing, 1960 - 2010 and ACS 2008 - 2012 5-year estimates  
 \* Note: Some 2010 Gretna Census Data are missing

## AGE STRUCTURE ANALYSIS

Age structure is an important component of any population analysis. Analyzing age structure allows one to determine which age groups (cohorts) within Gretna are experiencing population shifts and changes. Each age cohort affects the overall population in different ways. For example, the existence of large younger cohorts (20-44 years) means there is a better potential to sustain future population growth than if the City had large older cohorts. On the other hand, if the large, younger cohorts maintain their relative size, but do not increase the population as expected, they will, as a group, tend to strain the resources of the area as they age. Understanding what is happening within the age groups of the community’s population is necessary to effectively plan for the future of that community.

Table 2 exhibits the age cohort structure for Gretna in 2000 and 2010. Examining the age structure indicates some significant changes in the different population segments throughout the community with both positive and negative swings in differing cohorts. Identifying these changing dynamics is critical in order to make informed decisions and maximize the future use of resources.

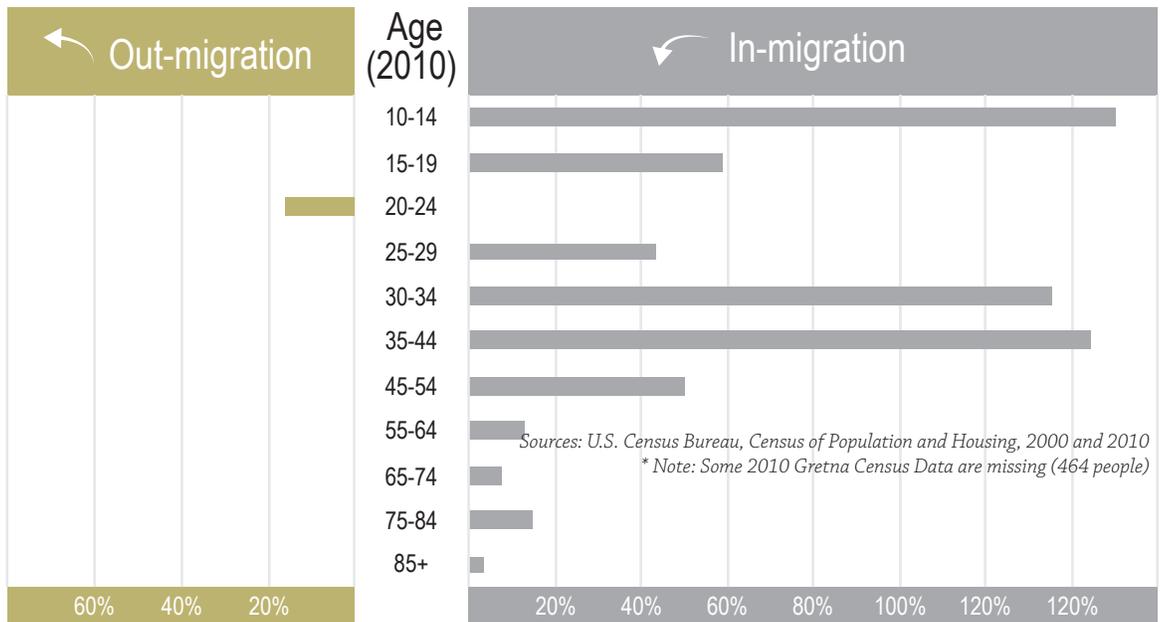
One method of analyzing cohort movement in a population involves comparing an age cohort in 2000 with the same age cohort 10 years later, in 2010; such as comparing the 0-4 cohort in 2000 with the 10-14 cohort in 2010. For example, Gretna had 151 children between the ages of 0 and 4 in 2000, and in 2010, there were 378 children between the ages of 10 and 14. In other words, Gretna saw a net in-migration within in this age cohort during the ten-year period (2000s) with 227 additional children. A review of population by this method permits a community to analyze which cohorts are moving in and out. The 0-4 and 5-9 age cohorts of 2010 were not born yet; therefore, were removed from Figure 2. A negative change in a particular cohort suggests out-migration; while a positive change suggests in-migration.

TABLE 2 AGE COHORT ANALYSIS (2000-2010)

2000		2010		2000-2010 COHORT CHANGE
Age "Then"	Population	Age "Now"	Population	
		0-4	403	403
		5-9	445	445
0-4	151	10-14	378	227
5-9	159	15-19	253	94
10-14	222	20-24	186	-36
15-19	192	25-29	276	84
20-24	134	30-34	316	182
25-29	153	35-44	685	405
30-34	127	45-54	591	197
35-44	394	55-64	359	41
45-54	318	65-74	213	15
55-64	198	75-84	182	24
65-74	158	85+	154	5
75+	149			
	<b>2,355</b>		<b>*4,441</b>	<b>1,238</b>

Sources: U.S. Census Bureau, Census of Population and Housing, 2000 and 2010  
 \* Note: Some 2010 Gretna Census Data are missing (464 people)

FIGURE 2 PERCENT CHANGE FROM MIGRATION / DEATH (2000-2010)



Sources: U.S. Census Bureau, Census of Population and Housing, 2000 and 2010  
 \* Note: Some 2010 Gretna Census Data are missing (464 people)

This analysis supports the theory that Gretna is growing in terms of young families. From Table 2, the positive age cohort changes found in the 0-14 and 30-44 years cohorts suggested that families with young children moved into the City. The cohort losses of young adults can be contributed to post-secondary education, lack of employment opportunities or atmosphere that small towns may not provide. The age cohort nearing retirement age has experienced modest in-migration. These age cohorts typically look for more services, such as medical practices, which Gretna provides. Gretna and adjacent communities provide the amenities that most age cohorts seek.

The United States continued to have an increase in the Median Age as an ongoing result of a large population known as “Baby Boomers.” The national aging trend is represented for Sarpy County communities in Table 3. Gretna had the lowest 20-year Median Age increase with six percent. Gretna deviated from the aging trend with a decrease of five percent in the 2000s due to the large influx of the 0-9 age cohorts. Without births and in-migration, Gretna’s Median Age would have continued to increase like other communities. Gretna’s population shift of the 2000s can be found in Figure 3.

TABLE 3 **MEDIAN AGE COMPARISON**

	1990	2000	CHANGE '90 - '00	2010	CHANGE '00 - '10	TOTAL CHANGE
<b>Gretna</b>	<b>32.4</b>	<b>36.4</b>	<b>12%</b>	<b>34.5</b>	<b>-5%</b>	<b>6%</b>
Bellevue	29.7	33.5	13%	34.8	4%	17%
La Vista	27.4	29.9	9%	32.1	7%	17%
Papillion	30.6	34.1	11%	36.8	8%	20%
Springfield	31.1	35.7	15%	37.3	5%	19%
Sarpy County	28.6	31.5	10%	32.9	4%	15%
Nebraska	33.0	35.3	7%	36.2	3%	10%

Sources: U.S. Census Bureau, Census of Population and Housing, 1990 - 2010

\* Note: Some 2010 Gretna Census Data are missing (464 people)

### Dependency Ratio

The dependency ratio examines the portion of a community’s earnings that is spent supporting age groups typically and historically dependent on the income of others.

$$\text{Dependency Ratio} = \frac{(\text{Under 18}) + (65 \text{ and older})}{(\text{Remainder of population})}$$

Ratios:

- < 1: 1 Independent resident able to support more than 1 Dependent resident
- = 1: 1 Independent resident able to support 1 Dependent resident
- > 1: 1 Independent resident able to support less than 1 Dependent resident

Table 4 is a comparison of the Dependency Ratio between Gretna and Sarpy County communities. Gretna had the highest dependency ratio in Sarpy County and the State of Nebraska in 2010. Although Gretna is able to support more than one dependent resident, its working population is closer to 1:1. Gretna’s Under 18 cohort was 1,408 and the 65 years and older had 549. Dependency ratio is based on Gretna’s dependent population of 1,900. With 2,484 residents in the working population, Gretna is able to “support” slightly more than the 1:1 ratio. In comparison, La Vista had a large working population with fewer dependent residents.

TABLE 4 **DEPENDENCY RATIO COMPARISONS**

	1990	2000	CHANGE '90 - '00	2010	CHANGE '00 - '10	TOTAL CHANGE
<b>Gretna</b>	<b>0.73</b>	<b>0.70</b>	<b>-4%</b>	<b>0.79</b>	<b>13%</b>	<b>8%</b>
Bellevue	0.53	0.59	10%	0.61	3%	15%
La Vista	0.57	0.52	-8%	0.50	-4%	-12%
Papillion	0.62	0.66	6%	0.62	-6%	0%
Springfield	0.83	0.60	-27%	0.60	0%	-28%
Sarpy County	0.59	0.59	0%	0.60	1%	1%
Nebraska	0.70	0.66	-6%	0.63	-5%	-10%

Sources: U.S. Census Bureau, Census of Population and Housing, 1990 - 2000, QTP1 2010

\* Note: Some 2010 Gretna Census Data Tables are missing (464 people)

Table 5 examines the City of Gretna’s age characteristics. This table specifically shows past trends of Gretna’s growth and compositions. The following information helps to determine the dependency rates. The proportion of persons less than 18 years of age decreased slightly between 1990 and 2000 and rebounded back in 2010 with 32%. The proportion of persons 65 years and older had minimal change going from 11% to 13%, and back down to 12%. The surprising statistics are the total changes within the past decade. The Under 18 increased by 746 children while the 65 and older demographic added 242 residents. As stated earlier, population is the driving force for housing and economic development. Gretna’s current dependency ratio indicates a strong population base within the community which is essential for any community that is growing.

**TABLE 5 GRETNA AGE CHARACTERISTICS**

	1990	2000	CHANGE '90 - '00	2010	CHANGE '00 - '10	TOTAL CHANGE	2008- 2012*
Under 18 years	697	662	-35	1,408	746	742	1,439
% of total population	31%	28%	-3%	32%	4%	-1%	30%
65 years +	250	307	57	549	242	322	572
% of total population	11%	13%	2%	12%	-1%	1%	12%
Median Age	32.4	36.4	4.0	34.50	-2	4.8	37.2
Dependency Ratio	0.73	0.70	-0.03	0.67	-0.03	-0.15	0.58
Total Population	2,249	2,355	106	*4,441	2,086	2,192	4,807

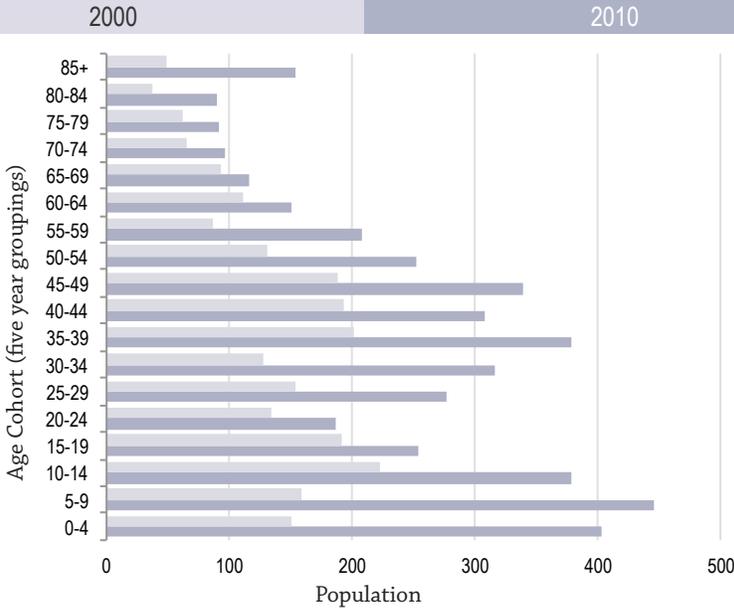
\*estimates

Sources: U.S. Census Bureau, Census of Population and Housing, 1990 - 2010 and ACS 2008 - 2012 5-year estimates

\* Note: Some 2010 Gretna Census Data Tables are missing a census tract (464 people)

Figure 3 shows a comparison between the total age cohorts for 2000 and 2010. These data show that Gretna continues to be a growing community with young families with the need for continued investments in schools and housing options. Gretna has grown in all age groups since 2000. Only two age cohorts had over 200 residents in 2000 while 2010 data revealed Gretna had almost every age cohort under the age of 60 with over 200 residents. Figure 3 illustrates the extent of how much growth occurred, including seven age cohorts having over 300 people in 2010.

FIGURE 3 AGE COHORT ANALYSIS (2000 & 2010)



Sources: U.S. Census Bureau, Census of Population and Housing, 2000 - 2010

**MIGRATION ANALYSIS**

Migration analysis allows a community to understand how in and out migrations are influencing the cities population. The migration number is determined by subtracting the natural change in population (i.e. births minus deaths) from the total change in population. Table 6 shows the total change, natural change and the total migration in population for Gretna from 1980-1989, 1990-1999 and 2000-2009. A negative number in the “Total Migration” column indicates the number of persons that have moved out of the community, while a positive number indicates the number of persons that have moved into the community.

Through migration analysis, it can be determined how much population change was due to persons moving into or out of the area and how much was due to births or deaths. For instance, Gretna had a 640 population increase from 1980-1989, subtract the 68 people they gained due to natural change, and the city gained a total of 572 people due to in-migration. Overall Gretna has gained 2,524 persons over the past 30 years due to people moving into the community and annexations of existing subdivisions.

TABLE 6 MIGRATION ANALYSIS

	Total Change (persons)	Natural Change (persons)	Total Migration (persons)	Migration Rate per year
<b>1980-1989</b>	640	68	572	3.6%
<b>1990-1999</b>	106	90	16	0.07%
<b>2000-2009</b>	2,086	150	1,936	8.2%
<b>Total</b>	<b>2,832</b>	<b>308</b>	<b>2,524</b>	<b>15.7%</b>

Sources: U.S. Census Bureau, Census of Population and Housing, 1990, 2000, and 2010  
NE Dept. of Health and Human Services Vital Records

Gretna has had a major influx of people in the past ten years therefore the migration rate for that time period is far more significant than the previous 20 years. It is the most recent rate that is the most crucial. This rate of growth seen in the 2000s is likely to continue for the next several years due to the influence of the expanding Omaha metropolitan area and limitations of building in Sarpy County.

## RACE CHARACTERISTICS

Another component of Gretna’s population is the racial composition of the overall population. The national trends show that minorities are growing and becoming a larger percentages in the United States demographics.

Table 7, below, illustrates the racial characteristics for the City of Gretna from 1990 to 2010. With all minorities representing less than four percent of the total 2010 population, Gretna does not have a diverse population as the vast majority of the Gretna population continues to be white. However, Gretna is beginning to diversify. Despite 2010 statistics showing only small percentages, the national trend of growing minorities is beginning to present itself in Gretna.

**TABLE 7 GRETNA RACIAL COMPOSITION**

	1990		2000		CHANGE '90 - '00	2010		CHANGE '00 - '10	TOTAL CHANGE
	Population	% of Total	Population	% of Total		Population	% of Total		
White, not Hispanic	2,246	99.9%	2,304	97.8%	-2.0%	4,282	96.4%	-1.4%	-3.4%
Black	1	0.0%	1	0.0%	-0.0%	27	0.6%	0.6%	0.6%
Am. Indian & AK. Native	0	0.0%	10	0.4%	0.4%	4	0.1%	-0.3%	0.1%
Asian & Pacific Islander	0	0.0%	8	0.3%	0.3%	20	0.5%	0.1%	0.5%
Other, not Hispanic	2	0.1%	5	0.2%	0.1%	38	0.9%	0.6%	0.8%
Hispanic	7	0.3%	27	1.1%	0.8%	70	1.6%	0.4%	1.3%

Sources: U.S. Census Bureau, Census of Population and Housing, 1990, 2000, and 2010  
 \* Note: Some 2010 Gretna Census Data are missing (464 people)

## POPULATION PROJECTIONS

Population projections allow Gretna to estimate the future population by looking at past and present population trends. Analyzing population changes in this manner allows the City to develop a representation of factors from which reasonable future scenarios can be modeled. A number of factors (demographics, economics, social, etc.) may affect projections either positively or negatively. At the present time, these projections are the best crystal ball Gretna has for predicting future population changes. There are many methods to project the future population trends. The methods identified below are intended to give Gretna a broad overview of the possible population changes that could occur in the future.

The following information was derived from several different sources in order to give a diverse representation of data in an attempt to gain the best picture of what is currently occurring in Gretna. In regards to what might occur with future population change, the analysis listed below uses a mixture of census data, vital statistics, Gretna School District data, and the Geographic Information System (GIS) data provided by Sarpy County.

According to the 2010 census, the Gretna Public Schools district had 15,513 people living within the district boundaries. This population does include small parts of Douglas County. The Gretna school district serves the growing population surrounding Gretna. This population represents the current annexation possibilities of Gretna’s future growth. Further annexation information is addressed in the Achieve portion of the Comprehensive Plan. The school board decisions are based upon a larger boundary than Gretna’s corporate limits and resemble that of the three-mile growth area. The needs and locations of services have continued to change with the growing population.

### TREND LINE ANALYSIS

Trend Line Analysis is a process of projecting future populations based upon changes during a specified period of time. In the analysis of Gretna, four different trend lines were reviewed: 1950 to 2010, 1970 to 2010, and 1990 to 2010, and 2000 to 2010. The data used to derive this trend line comes from the US decennial census records. A review of these trend lines indicates Gretna will continue to increase in population through 2030. Table 8 table is generated by calculating the annual growth Gretna has experienced in the reference periods. The 2000 to 2010 will have larger projections due to the extreme growth in the 2000s.

Reference Period	2020 Population	2030 Population	2035 Population
1950-2010	5,650	6,394	6,766
1970-2010	5,742	6,579	6,998
1990-2010	6,233	7,561	8,225
2000-2010	7,455	10,005	11,280

TABLE 8  
GRETNA TREND  
LINE ANALYSIS

Sources: U.S. Census Bureau, 1950 to 2010, JEO Consulting Group

In this second analysis of Gretna, another four trend lines were reviewed: 1950 to 2012 (62 year trend), 1970 to 2012 (42 year trend), and 1990 to 2012 (22 year trend), and 2000 to 2012 (12 year trend). This trend line, shown in Table 9, is different due to estimates and it represents the total growth within the reference periods. Therefore, the total growth between 1950 and 2012 was the largest; therefore it will have the largest projections. American Community Survey is a U.S. Census Bureau program that supplies supplemental data used by communities with most current five-year estimates. The data may not be as accurate since the City issues building permits only for construction within the ETJ these number can be skewed by the development that occurs outside of City limits but within the ETJ.

Reference Period	2020 Population	2030 Population	2035 Population
1950-2012	9,176	13,545	13,545
1970-2012	8,057	11,307	11,307
1990-2012	7,365	9,923	9,923
2000-2012	7,259	9,711	9,711

TABLE 9  
GRETNA SECOND  
TREND LINE  
ANALYSIS

Sources: U.S. Census Bureau, 1950 to 2010, ACS 2008-2012 Five-Year Estimates  
JEO Consulting Group

### COHORT SURVIVAL ANALYSIS

Cohort survival analysis applies local birth and death rates to the existing population over a series of years to project a vision of the future (Table 10). For example, the readily available public statistics are the Sarpy County birth and death rates which indicate population change due to natural fluctuation.

	Population
2010	*4,441
2015	4,534
2020	4,676
2025	4,871
2030	5,113
2035	5,348

TABLE 10  
GRETNA COHORT  
SURVIVAL ANALYSIS

\* Note: Some 2010 Gretna Census Data missing, age cohorts not available individually (464 people)

### American Community Survey (ACS)

The American Community Survey is a U.S. Census Bureau program that utilizes multi-year small-sample surveys to compile statistics.

Communities can use the generated estimates as a guide to supplement the centennial censuses as well as getting more recent information supplied by the local survey recipients. The 2010 Census was shortened and the corresponding information from 2000 can only be found in the ACS estimates.

Compiling 2008 through 2012 surveys results = ACS 2012 Five-year estimates

### Sanitary Improvement Districts (SIDs)

A Sanitary Improvement District (SID) is formed for the purposes of installing electric service lines and conduits, a sewer system, water system, street system, and various necessities through contracts and agreements with the local authority. The resulting plat and lots are intended to be sold or leased as housing developments or business districts. A developer is able to finance debt associated with public infrastructure within the district. To accomplish these tasks, the SID has the authority to issue bonds, levy taxes and special assessments, and affix rates for services.

SIDs are governed by a board of trustees, who appoint a chairman and clerk. This board is responsible for construction and maintenance of public improvements and the financial well-being of the SID, including setting its tax rate. An SID exists until the development is annexed into the city's legal limits.

### MODIFIED COHORT SURVIVAL ANALYSIS

The difference of the modified cohort survival is the addition of migration to natural population change. The modified cohort survival analysis represented in Table 11 projected the birth and death rates but also included the age cohort changes for a plausible composite of each age cohort. The successive periods these birth, death, and migration statistics project a conceivable growth scenario within the current corporate limits. This modified analysis does not represent future annexations or extending extraterritorial jurisdiction. This analysis can create a less accurate depiction for Gretna due to the majority of its population growth is dictated by the number of annexations it approves. The 2000s had increased populations of younger families and will continue to experience natural growth as Sanitary Improvement Districts and adjacent subdivisions are incorporated into the city of Gretna. If Gretna's unprecedented growth and in-migration continues, contiguous growth of adjacent properties can result in growth rates similar to Papillion.

**TABLE 11**  
**GRETNA**  
**MODIFIED COHORT**  
**SURVIVAL ANALYSIS**

	Population
<b>2010</b>	*4,441
<b>2015</b>	5,522
<b>2020</b>	6,629
<b>2025</b>	7,835
<b>2030</b>	9,173
<b>2035</b>	10,584

*\* Note: Some 2010 Gretna Census Data are missing, not available (464 people)*

### COMPARATIVE ANALYSIS

This analysis looks at the growth rate and trends of a similar city and applies those rates/trends to Gretna. The City of Papillion was used to "mimic" growth patterns due to good transportation access and development pressures strongly influenced by the City of Omaha. Papillion's growth percentages and weighted projections represent similar Gretna population trends in Table 12.

**TABLE 12 COMPARATIVE ANALYSIS (PAPILLION / GRETNA)**

	Papillion Population	Growth Rate				
<b>1960</b>	2,235					
<b>1970</b>	5,606	151%				
<b>1980</b>	6,399	14%				
<b>1990</b>	10,372	62%				
<b>2000</b>	16,363	58%				
<b>2010</b>	18,894	15%	<b>4,905 (actual)</b>	<b>108% (actual)</b>	<b>4,905 (actual)</b>	<b>108% (actual)</b>
<b>2020</b>			<b>5,597</b>	<b>14%</b>	<b>5,402</b>	<b>10%</b>
<b>2030</b>			<b>9,067</b>	<b>62%</b>	<b>7,806</b>	<b>45%</b>
<b>2040</b>			<b>14,307</b>	<b>58%</b>	<b>11,046</b>	<b>42%</b>

*\*factoring in 108% actual 2000-10 growth versus Papillion's 151% growth rate*

### PLATTED LOTS ANALYSIS

A platted lots analysis examines the planned and built residential subdivisions within the corporate limits and one-mile extraterritorial jurisdiction. The number of lots is multiplied by Gretna’s 2010 average household size of 2.72 to estimate the population. Adjacent rural subdivisions and SIDs totaled 1,736 platted lots at the time of the 2014 update. These residential developments near the City of Gretna would total 4,722 people.

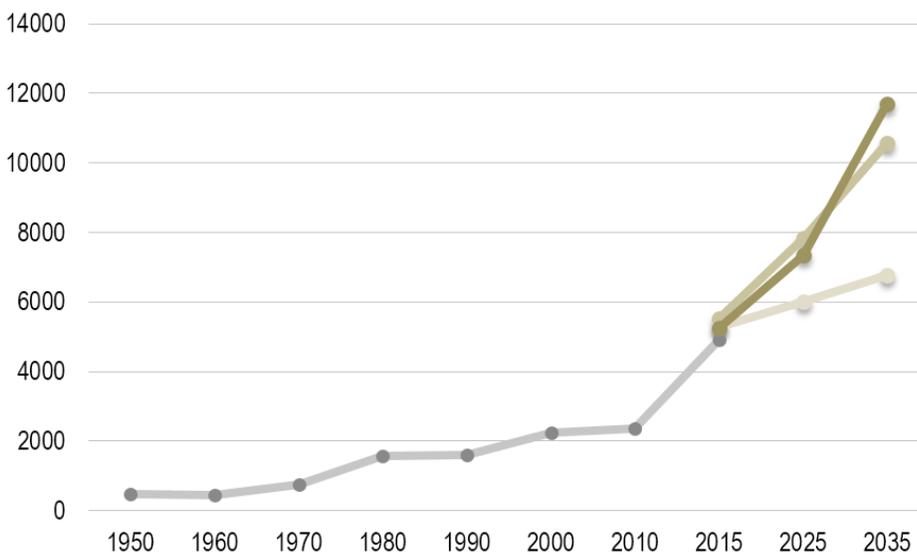
A large percentage of new housing is from 1990s and 2000s. As Gretna’s continue to grow, existing and new SIDs and rural subdivisions will be ‘eligible’ for annexation. Gretna’s continued growth will be dictated by new annexations that extend its extraterritorial jurisdiction, which in turn, will create new contiguous annexation possibilities.

### SUMMARY OF POPULATION PROJECTIONS

As discussed previously, Gretna’s growth is related directly to annexation for its growth within the corporate limits. The population projections in Figure 4 indicate three different scenarios for Gretna’s twenty-year growth period. The low series represents trend line growth as it has occurred annually between 1950 and 2010. The medium series depicts the modified cohort survival (natural growth with in-migration) that is similar to the total population of adjacent platted lots. This is a realistic population projection in the next twenty years. The high series represents Gretna’s growth if it were to mimic Papillion’s growth of the 1980s and 1990s. Figure 4 reviews the population history of Gretna between 1950 and 2010 with the projections three projection scenarios into the years 2025 and 2035.

**FIGURE 4 POPULATION TRENDS / PROJECTIONS**

	2015	2025	2035	
	5,597	7,332	11,687	High Series
	5,522	7,835	10,584	Medium Series
	5,278	6,022	6,766	Low Series



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# Housing Profile

- Age of Existing Housing Stock
- Housing Trends



# Housing Profile

The Housing Profile identifies existing housing characteristics and projected housing needs for the residents of Gretna. The primary goal of the housing profile is to allow the community to determine what issues to address in order to provide safe, acceptable, sanitary and affordable housing for every family and individual residing within the community. The housing profile is an analysis that aids in determining the composition of owner-occupied and renter-occupied units, as well as the existence of vacant units. It is important to evaluate information on the value of owner-occupied housing units, and monthly rents for renter-occupied housing units, and to determine if housing costs are a financial burden to the residents of Gretna.

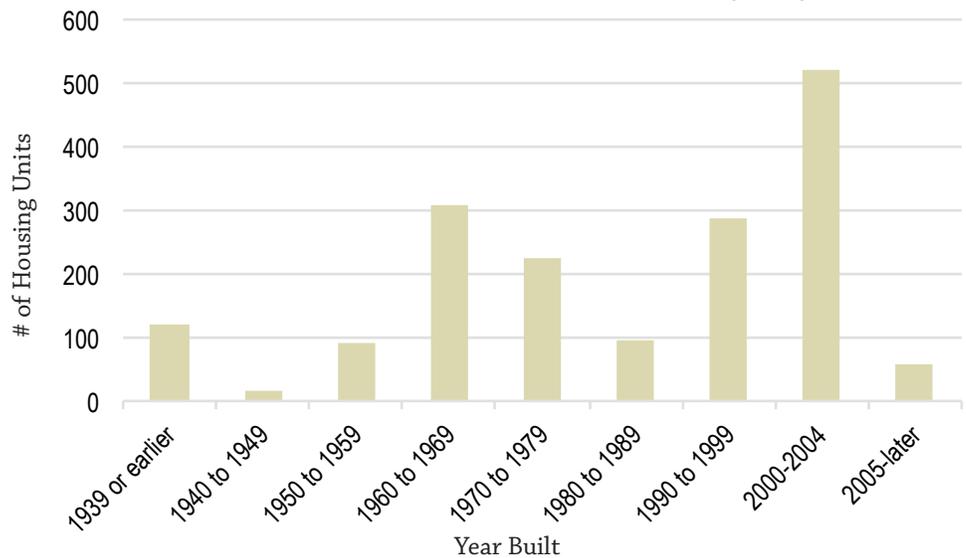
Projecting future housing needs requires consideration of several factors. These factors include population change, household income, employment rates, land use patterns, and residents' attitudes. The following tables and figures are intended to assist with determining future housing needs and develop policies designed to accomplish the housing goals for Gretna.

## AGE OF EXISTING HOUSING STOCK

An analysis of the age of Gretna's housing stock reveals a number of things about the population and economic conditions of the past. The age of the housing stock also indicates the level of need for rehabilitation efforts, or new construction within the community. Examination of the housing stock is important for understanding the overall quality of housing and the quality of life within Gretna.

According to the 2010 ACS estimates, Figure 5 indicates that more than 75% of Gretna's housing units were constructed after 1960. Therefore, Gretna has a relatively young housing stock. With new development occurring in recent years, the expected future housing stock will result in continued decreases of average residential year built. The original boundary of Gretna represents seven percent of its housing stock built before 1940. These housing units, particularly surrounding downtown, will need additional attention and restoration in order to maintain the aesthetic appeal and diversification of Gretna's housing stock. The housing developments in the 1940s and 1960s were built on the east side of Gretna toward Highway 6/31.

FIGURE 5 AGE OF EXISTING HOUSING STOCK (2010)



American Community Survey 2006 - 2010 5-year estimates

## HOUSING TRENDS

An analysis of housing trends can reveal a great deal about the different population groups in the community. Housing trends may also indicate the supply and demand of owner- or renter-occupied housing. Examining housing trends is important in order to understand the overall diversity of the population and the quality of life within Gretna.

Table 13 indicates the average number of persons per household decreased by seven percent since 1990. This is not surprising since this has been the trend of communities across the nation. However, Gretna’s growth in the past decade has increased person per household by five percent. There has been a significant increase in overall housing units, which has also increased the number of vacant units making it easier to find housing for people wishing to move to Gretna.

**TABLE 13 GRETNA HOUSING CHARACTERISTICS**

	1990	2000	CHANGE 90-00	2010	CHANGE 00-10	TOTAL CHANGE	2008- 2012
Population	2,249	2,355	5%	*4,441	89%	97%	4,807
Persons in Household	2,192	2,300	5%	4,330	88%	98%	4,736
Persons in Group Quarters	57	55	-4%	111	102%	95%	71
Persons per Household - Owner	3.11	2.80	-10%	3.06	9%	-2%	2.90
Persons per Household - Renter	2.14	2.01	-6%	1.93	-4%	-10%	1.93
Persons per Household	2.91	2.59	-11%	2.72	5%	-7%	2.65
Total Housing Units	768	917	19%	1,671	82%	118%	1,834
Occupied Housing Units	753	889	18%	1,594	79%	112%	1,790
Owner-occupied units	599	648	8%	1,109	71%	85%	1,316
Renter-occupied units	154	241	56%	485	101%	215%	474
Vacant Housing Units	15	28	87%	77	175%	413%	44
Owner-Occupied vacancy rate	0.5%	0.3%	-40%	0.9%	200%	80%	0.0%
Renter-Occupied vacancy rate	0.6%	4.0%	567%	7.8%	95%	1200%	0.0%
Single-family Units	659	746	13%	1,311	76%	99%	1,441
Duplex/Multiple-family units	106	163	54%	413	153%	290%	393
Mobile Homes, trailer, other	3	-	NA	-	NA	NA	-
<b>Median Contract Rent</b>							
<b>Gretna</b>	<b>\$313</b>	<b>\$556</b>	<b>78%</b>	<b>\$730*</b>	<b>31%*</b>	<b>133%*</b>	<b>\$800*</b>
Sarpy County	\$411	\$607	48%	\$813*	34%*	98%*	\$838*
State of Nebraska	\$282	\$491	74%	\$648*	32%*	130%*	\$687*
<b>Median Value of Owner-Occupied Units</b>							
<b>Gretna</b>	<b>\$54,400</b>	<b>\$102,000</b>	<b>88%</b>	<b>\$166,400*</b>	<b>63%*</b>	<b>206%*</b>	<b>\$168,800*</b>
Sarpy County	\$66,900	\$112,100	68%	\$158,600*	41%*	137%*	\$161,500*
State of Nebraska	\$50,400	\$88,000	75%	\$123,900*	41%*	146%*	\$126,700*

Sources: U.S. Census Bureau, Census of Population and Housing, 1990 (STF-1A), 2000 (DP-4), 2010 and ACS 2008 - 2012 5-year estimates

\* Note: Some 2010 Gretna Census Data are missing, not available

Gretna’s median contract rent per month increased from \$313 in 1990 to \$730 in the 2010 estimates. Gretna’s 133% increase in median monthly contract rent is comparable with Nebraska’s increased of 130%. Similarly the median value of owner-occupied units increased at a higher rate than the State of Nebraska meaning Gretna was above the rest of the State in terms of housing cost. In order to compare those costs to national economic conditions we use the cumulative rate of inflation for the years between 1990 and 2010. The rate for that time period was 66.8% which means both Gretna and the State of Nebraska increased by more than twice the standard; therefore residents of Gretna were paying more, in terms of real dollars, for housing in 2010 than they were in 1990.

Table 14 shows similar data to Table 13; however, the listed information is solely for 2010 and is a comparison between Gretna and the other communities in Sarpy County. This table is most useful for comparing vacancy rates, median contract rent and median value owner-occupied units. As far as vacancy rate, Gretna has the lowest owner-occupied rate and an average renter-occupied rate. When it comes to housing costs Gretna is below average in rent and above average in owner-occupied values suggesting there are more new owner-occupied units being built than there are renter-occupied units.

Table 15 shows tenure (owner-occupied and renter-occupied) of households by number and age of persons in each housing unit. Analyzing these data allow the City to understand the more detailed dynamics of the housing stock/market. In addition, the City could target efforts for housing rehabilitation and/or new construction for those segments of the population exhibiting the largest need.

**Table 14 SARPY HOUSING TRENDS COMPARISONS (2010)**

	<b>Gretna</b>	Bellevue	Papillion	La Vista	Springfield
Population	<b>*4,441</b>	50,137	18,894	15,758	1,529
Persons in Household	<b>4,330</b>	50,063	18,527	15,758	1,529
Persons in Group Quarters	<b>111</b>	74	367	0	0
Persons per Household - Owner	<b>3.06</b>	2.71	2.89	2.77	2.78
Persons per Household - Renter	<b>1.93</b>	2.43	2.19	2.08	2.25
Persons per Household	<b>2.72</b>	2.62	2.68	2.45	2.66
Total Housing Units	<b>1,671</b>	20,591	7,240	6,670	604
Occupied Housing Units	<b>1,594</b>	19,142	6,925	6,419	575
Owner-occupied Units	<b>1,109</b>	12,779	4,829	3,502	449
Renter-occupied Units	<b>485</b>	6,363	2,096	2,917	126
Vacant Housing Units	<b>77</b>	1,449	315	251	29
Owner-Occupied Vacancy Rate	<b>0.9%</b>	1.7%	1.2%	1%	2.4%
Renter-Occupied Vacancy Rate	<b>7.8%</b>	12.2%	8.2%	5.1%	6%
Single-family Units	<b>1,311</b>	15,519	5,477	4,084	464
Duplex/Multiple-family Units	<b>413</b>	4,540	1,709	2,405	92
Mobile Homes, Trailer, Other	<b>-</b>	439	16	-	20
Median Contract Rent	<b>\$730</b>	\$757	\$832	\$830	\$489
Median Value Owner-Occupied Units	<b>\$166,400</b>	\$137,800	\$163,800	\$143,700	\$120,100

Sources: U.S. Census Bureau, Census of Population and Housing 2010 and ACS 2006 - 2010 5-year estimates

\*Note: Some 2010 Gretna Census Data are missing, not available

Gretna increased its housing units substantially in the 2000s. There were an estimated 428 new owner-occupied housing while the renter-occupied units more than doubled. In 2000, the largest percentages of both owner-occupied and renter-occupied housing were the 1 and 2 persons per unit. According to the 2010 percent change, Gretna has seen a percentage increase in the owner-occupied population of households with 3 or more while the 1 and 2 person households decreased.

Both 2000 and 2010 data shows the most prominent age groups to own a house were between the ages of 35 and 54. These age groups totaled 47% in 2000 and 52% in 2010. The owner-occupied units owned by persons 55 years of age or older accounted for 31.2% of the total units. It can be expected to see some housing stock turnover, but it will not be significant for Gretna in the near future. Housing values and costs may lead to current owner-occupied housing units to be converted to renter-occupied housing. This will help Gretna provide housing options but may also be a concern for the city if this happens on a large scale in the older parts of Gretna with existing and aging housing stock.

By comparison, the renter-occupied units indicated a different scenario. Renter-occupied units were still more likely to be occupied by one- and two-person households with 48.7% and 26.6% respectively; these two household sizes make up 75.3% of the total units. The larger the household size, the less likely they were residing in a rental unit. In addition, the age groups likely to rent were the ages between 25 and 44 with 22.9% and 18.1%, respectively, combining for 41.0% of the total renter households. Surprising, the 75 and older demographic was a second largest population of the rental housing. A 9.6% increase of the 2000s led to 21.4% claim in rental housing units, which may be due to assisted living facilities.

TABLE 15 GRETNA HOUSING TENURE ANALYSIS

	2000		2006-2010				CHANGE 00-10			
	Owner-Occupied	Renter-Occupied	Owner-Occupied	Renter-Occupied	O.O.	R.O.				
<b>Tenure by Number of Persons in Housing Unit (Occupied Housing Units)</b>										
1 person	123	18%	96	47%	154	14%	236	49%	-4%	2%
2 persons	264	39%	58	28%	338	31%	129	27%	-8%	-2%
3 persons	97	14%	29	14%	183	17%	65	13%	2%	-1%
4 persons	115	17%	12	6%	238	22%	33	7%	5%	1%
5 persons	69	10%	6	3%	137	12%	17	4%	2%	1%
6 persons +	13	2%	3	2%	59	5%	5	1%	3%	-1%
<b>Totals</b>	<b>681</b>		<b>204</b>		<b>1,109</b>		<b>485</b>			
<b>Tenure by Age of Householder (Occupied Housing Units)</b>										
15-24 years	21	3%	34	17%	17	2%	35	7%	-2%	-10%
25-34 years	102	15%	37	18%	177	16%	111	23%	1%	-16%
35-44 years	148	22%	44	22%	305	28%	88	18%	6%	-4%
45-54 years	167	25%	31	15%	264	24%	66	14%	-1%	-2%
55-64 years	99	15%	18	9%	165	15%	54	11%	0%	2%
65-74 years	88	13%	16	8%	102	9%	27	6%	-4%	-2%
75 years +	56	8%	24	12%	79	7%	104	22%	-1%	10%
<b>Totals</b>	<b>681</b>		<b>204</b>		<b>1,109</b>		<b>485</b>			

Sources: U.S. Census Bureau, Census of Population and Housing 2000 (SF4) and ACS 2006 - 2010 5-year estimates

According to the U.S. Department of Housing and Urban Development (HUD) guidelines, housing units lacking complete plumbing or have more than one persons per room are considered substandard housing units. HUD defines a complete plumbing facility as hot and cold piped water, a bathtub or shower, and a flush toilet. When these criteria are applied to Gretna, four housing units, or 0.4% of the total units, were considered substandard in 2000. However, the reason for these units being substandard in 2000 and 2010 was entirely based upon overcrowding. Gretna's percentages have been consistent or below the State's substandard housing.

These data in Table 16 do not include housing units containing major defects requiring rehabilitation or upgrading to meet building, electrical or plumbing codes, which should also be included in an analysis of substandard housing. A comprehensive survey of the entire housing stock should be completed every five years to determine and identify the housing units that would benefit from remodeling or rehabilitation work. This process will help ensure that a community maintains a high quality of life for its residents through protecting the quality and quantity of its housing stock.

**TABLE 16  
HOUSING  
CONDITION  
COMPARISONS**

	Gretna		Nebraska	
1990 Housing Units	<b>768</b>		660,621	
1990 Occupied Housing Units	<b>753</b>	<b>98%</b>	602,363	91%
2000 Housing Units	<b>917</b>		722,668	
2000 Occupied Housing Units	<b>889</b>	<b>97%</b>	666,184	92%
2010 Housing Units	<b>1,671</b>		796,793	
2010 Occupied Housing Units	<b>1,594</b>	<b>95%</b>	721,130	91%
Total Change	<b>754</b>	<b>82%</b>	74,125	10%
Annual Change	<b>75</b>	<b>8%</b>	7,413	1%
Total Change in Occupied Units	<b>705</b>	<b>79%</b>	54,946	8%
Annual Change in Occupied Units	<b>71</b>	<b>8%</b>	5,495	1%

**Characteristics**

1990 Units Lacking Complete Plumbing Facilities	<b>0</b>	<b>0.0%</b>	5,242	0.9%
1990 Units with More Than One Person per Room	<b>9</b>	<b>1.2%</b>	10,512	1.7%
2000 Units Lacking Complete Plumbing Facilities	<b>0</b>	<b>0.0%</b>	2,408	0.4%
2000 Units with More Than One Person per Room	<b>4</b>	<b>0.4%</b>	17,963	2.7%
2010 Units Lacking Complete Plumbing Facilities	<b>0</b>	<b>0.0%</b>	2,540	0.4%
2010 Units with More Than One Person per Room	<b>29</b>	<b>1.8%</b>	12,201	1.7%

**Substandard Units**

1990 Total	<b>9</b>	<b>1%</b>	15,754	3%
2000 Total	<b>4</b>	<b>0%</b>	20,503	3%
2010 Total	<b>29</b>	<b>2%</b>	14,741	2%

Sources: U.S. Census Bureau, Census of Population and Housing 1990 (STF-3A), 2000 (DP-4), 2010 (DP04) and ACS 2006 - 2010 5-year estimates  
\* Note: Some 2010 Gretna Census Data are missing, not available

# Economic and Employment Profile

- Income Statistics
- Industry Employment
- Commuter Trends
- Sales and Fiscal Profile



# Economic and Employment Profile

Economic data are collected in order to understand area markets, changes in economic activity, employment needs and opportunities within Gretna. In this section, employment by industry, household income statistics, income by source, and basic/non-basic analyses were reviewed for Gretna, Sarpy County, other Sarpy County communities, and the State of Nebraska.

## INCOME STATISTICS

Income statistics for households are important for determining the earning power of households in the community. The data presented here show household income levels for Gretna in comparison to the State of Nebraska. These data were reviewed to determine whether households experienced income increases at a rate comparable to that of the state of Nebraska and the Consumer Price Index (CPI).

Table 17 indicates the number of households and income ranges for Gretna in 2000 and 2012. Previously, Gretna's most commonly reported household income range was the \$35,000 to \$49,999 in 1990. This income range accounted for 25.4% of all Gretna households. The most commonly reported 1990 income range for the state of Nebraska was \$15,000 to \$24,000. This difference indicated Gretna had a very strong income base compared to the State. The percent of Gretna households earning less than \$15,000, in 1990, accounted for 14.7% of the total while the same category in the State accounted for 26.6% of the total.

TABLE 17 HOUSEHOLD INCOME COMPARISONS

	2000			2008-2012			CHANGE		
	Gretna		Nebraska	Gretna		Nebraska	Gretna		Nebraska
Less than \$10,000	48	5%	8%	24	1%	6%	-24	-4%	-2%
\$10,000 to \$14,999	43	5%	7%	67	4%	5%	24	-1%	-1%
\$15,000 to \$24,999	106	12%	15%	44	2%	11%	-62	-9%	-4%
\$25,000 to \$34,999	115	13%	15%	161	9%	11%	46	-4%	-3%
\$35,000 to \$49,999	138	15%	18%	228	13%	15%	90	-2%	-4%
\$50,000 to \$75,000	228	25%	20%	357	20%	20%	129	-5%	-0%
\$75,000 and Higher	227	25%	17%	909	51%	31%	682	26%	14%
<b>Total</b>	905			1,790			885		
Median Household Income	\$50,112		\$39,250	\$77,818		\$51,381	\$27,706		55%
Number of Households	905		666,995	1,790		721,026	885		98%

Sources: U.S. Census Bureau, Census of Population and Housing 2000 (DP-3) and ACS 2008 - 2012 5-year estimates

The 2012 U.S. Census Estimates indicated an even larger discrepancy between the State and the City. In 2012, the households earning more than \$35,000 increased to 83.4% in Gretna while the State had 66.1% earning greater than \$35,000. In 2012, the State had 51.3% of its households earning more than \$50,000; while Gretna had 70.7% earning that amount. These data show Gretna has a very strong household income and has been increasing at a rate faster than the State as a whole. Part of this increase can be attributed to people living in Gretna and commuting to Omaha for work.

The median household income for Gretna was \$35,559 in 1990, which was \$9,543 greater than the state average. By 2012, the median household income increased to \$77,818 or 118% and Gretna saw the difference between the community and the State increase to \$26,437. The CPI for this period was 75.7%, which means the residents of Gretna were making more money in terms of real dollars in 2012 than they were in 1990.

Table 18 compares Gretna to the other Sarpy County communities. The table examines the median household income, as well as provides an income breakdown. The data indicate Gretna was the highest for median household income of these communities. The breakdown of income levels shows every city with relatively similar percentages except for Papillion, which had a greater number of households earning \$100,000 to \$149,999. Gretna had the highest percentage of wealthiest households, with a greater percentage of households earning more than \$50,000.

Table 19 shows owner-occupied and renter-occupied housing costs as a percentage of a Gretna's 2012 householder income. In addition, the table identifies the number of households experiencing housing cost burden. It is noted the total number of households may be different, due to the use of survey methodology. A housing cost burden, as defined by the U.S. Department of Housing and Urban Development (HUD), occurs when gross housing costs, including utility costs, exceed 30% of gross household income, based on data published by the U.S. Census Bureau. Table 18 shows that 1,331 households, or 74% of total households, paid less than 30% of their income towards housing costs. Therefore, the remaining 459 households, or 26%, experienced a housing cost burden.

Based on the 459 households experiencing housing cost burden, there were 314 owner-occupied households and 145 renter-occupied households. Owner-occupied households accounted for a housing cost burden of 24%; while, the renter-occupied households accounted for a housing cost burden of 31%. Owner-occupied households with the largest percentage (8%) having a cost burden was the \$20,000 to \$34,999 income bracket. The largest renter-occupied households with a cost burden were the \$10,000 to \$19,999 income group accounting for 13%. These statistics are similar with national trends of housing cost burden typically being felt more by renters compared to owners.

Table 20 compared Gretna's housing burden with the other communities in Sarpy County and the State of Nebraska. Gretna's overall housing cost burden was the lowest in 2000; however, the overall housing burden had increased from 17% to 26%.

## INCOME SOURCE AND PUBLIC ASSISTANCE

Table 21 shows personal income by source for Gretna, Sarpy County, and the State. The data are divided into five categories, which are: Households with earnings, Households with Social Security Income, Households with Supplemental Security Income, Households with Public Assistance, Households with Retirement Income, Median Household Income.

These data show Gretna is right in line with the County and trending below the State in terms of earnings and earnings from assistance programs. This table suggests Gretna and Sarpy County populations may be getting younger, by average, through time.

TABLE 18 SARPY COUNTY HOUSEHOLD INCOME COMPARISONS

**2000**

	Gretna		Bellevue		Papillion		La Vista		Springfield	
Less than \$10,000	48	5%	702	4%	165	3%	173	4%	32	6%
\$10,000 to \$14,999	43	5%	761	4%	141	3%	142	3%	16	3%
\$15,000 to \$24,999	106	12%	1,943	11%	433	8%	539	12%	61	12%
\$25,000 to \$34,999	115	13%	2,315	14%	485	9%	547	12%	60	11%
\$35,000 to \$49,999	138	15%	3,279	19%	689	13%	977	22%	104	20%
\$50,000 to \$74,999	228	25%	4,513	27%	1,479	27%	1,343	30%	133	25%
\$75,000 to \$99,999	139	15%	1,985	12%	1,012	19%	484	11%	82	16%
\$100,000 to \$149,999	61	7%	1,141	7%	805	15%	172	4%	29	6%
\$150,000 to \$199,999	19	2%	208	1%	161	3%	35	1%	0	0%
\$200,000 and over	8	1%	160	1%	88	2%	29	1%	6	1%
<b>Total Households</b>	905		17,007		5,458		4,441		523	
Median Household Income	<b>\$50,112</b>		\$47,201		\$63,992		\$47,280		\$48,083	

**2008-2012\***

	Gretna		Bellevue		Papillion		La Vista		Springfield	
Less than \$10,000	24	1%	821	4%	266	4%	264	4%	3	1%
\$10,000 to \$14,999	67	4%	794	4%	220	3%	118	2%	41	7%
\$15,000 to \$24,999	44	2%	1,659	9%	492	7%	456	7%	50	9%
\$25,000 to \$34,999	161	9%	1,960	10%	570	8%	600	9%	34	6%
\$35,000 to \$49,999	228	13%	2,852	15%	1,037	14%	1,392	20%	118	21%
\$50,000 to \$74,999	357	20%	4,393	23%	1,162	16%	1,647	24%	148	26%
\$75,000 to \$99,999	418	23%	3,022	16%	1,222	17%	1,135	17%	87	16%
\$100,000 to \$149,999	415	23%	2,833	15%	1,503	20%	813	12%	59	11%
\$150,000 to \$199,999	13	1%	762	4%	575	8%	224	3%	11	2%
\$200,000 and over	63	4%	338	2%	315	4%	202	3%	9	2%
<b>Total Households</b>	1,790		19,434		7,362		6,851		560	
Median Household Income	<b>\$77,818</b>		\$58,148		\$73,988		\$59,488		\$52,604	

**CHANGE 2000-(2008-2012)**

	Gretna		Bellevue		Papillion		La Vista		Springfield	
Less than \$10,000	-24	-4%	119	0%	101	1%	91	-0%	-29	-6%
\$10,000 to \$14,999	24	-1%	33	-0%	79	0%	-24	-1%	25	4%
\$15,000 to \$24,999	-62	-9%	-284	-3%	59	-1%	-83	-5%	-11	-3%
\$25,000 to \$34,999	46	-4%	-355	-4%	85	-1%	53	-4%	-26	-5%
\$35,000 to \$49,999	90	-3%	-427	-5%	348	1%	415	-2%	14	1%
\$50,000 to \$74,999	129	-5%	-120	-4%	-317	-11%	304	-6%	15	1%
\$75,000 to \$99,999	279	8%	1,037	4%	210	-2%	651	6%	5	-0%
\$100,000 to \$149,999	354	16%	1,692	8%	698	6%	641	8%	30	5%
\$150,000 to \$199,999	-6	-1%	554	3%	414	5%	189	2%	11	2%
\$200,000 and over	55	3%	178	1%	227	3%	173	2%	3	0%
<b>Total Households</b>	885		2,427		1,904		2,410		37	
Median Household Income	<b>\$27,706</b>		\$10,947		\$9,996		\$12,208		\$4,521	

Sources: U.S. Census Bureau, Census of Population and Housing 2000 (DP-3) and ACS 2008 - 2012 5-year estimates

TABLE 19 GREтна HOUSING COSTS AS A PERCENTAGE OF INCOME

	2000						2010					
	Owner-Occupied Households		Renter-Occupied Households		Total Households		Owner-Occupied Households		Renter-Occupied Households		Total Households	
<b>Less than \$10,000</b>												
Less than < 30% of income	4	1%	4	2%	8	1%	<b>Less than \$20,000</b>					
More than > 30% of income	19	3%	16	9%	35	4%	9	1%	12	3%	21	1%
<b>\$10,000 to \$19,999</b>												
< 30% of income	20	3%	15	8%	35	4%	<b>\$20,000 to \$34,999</b>					
> 30% of income	22	3%	22	12%	44	5%	40	3%	21	4%	61	3%
<b>\$20,000 to \$34,999</b>												
< 30% of income	79	12%	44	23%	123	15%	<b>\$35,000 to \$49,999</b>					
> 30% of income	27	4%	13	7%	40	5%	60	5%	85	18%	145	8%
<b>\$35,000 to \$49,999</b>												
< 30% of income	89	14%	27	14%	116	14%	<b>\$50,000 to \$74,999</b>					
> 30% of income	21	3%	0	0%	21	3%	166	13%	118	25%	284	16%
<b>\$50,000 +</b>												
< 30% of income	353	55%	47	25%	401	48%	<b>\$75,000 +</b>					
> 30% of income	8	1%	0	0%	8	1%	727	55%	93	20%	820	46%
<b>Combined Households</b>												
< 30% of income	545	85%	137	73%	682	82%	<b>Combined Households</b>					
> 30% of income	97	15%	51	27%	148	18%	1,002	76%	329	69%	1,331	74%
<b>Total</b>	<b>642</b>		<b>188</b>		<b>830</b>		<b>1,316</b>		<b>474</b>		<b>1,790</b>	

TABLE 20 SARPY COUNTY HOUSING COSTS AS A PERCENTAGE OF INCOME

	2000						2008-2012						CHANGE (PERCENTAGE POINTS)					
	Gretna	Bellevue	Papillion	La Vista	Springfield	Nebraska	Gretna	Bellevue	Papillion	La Vista	Springfield	Nebraska	Gretna	Bellevue	Papillion	La Vista	Springfield	Nebraska
<b>Owner-occupied Households</b>																		
< 30% of income	84%	85%	87%	82%	81%	84%	76%	78%	84%	78%	80%	79%	-8%	-6%	-3%	-4%	-1%	-4%
> 30% of income	15%	16%	13%	17%	19%	16%	24%	22%	16%	22%	20%	21%	9%	6%	3%	4%	0%	5%
<b>Renter-occupied Households</b>																		
< 30% of income	67%	68%	64%	72%	70%	62%	69%	58%	58%	70%	86%	57%	2%	-11%	-7%	-2%	15%	-5%
> 30% of income	25%	28%	32%	26%	19%	30%	31%	42%	42%	30%	14%	43%	6%	15%	11%	4%	-5%	13%
<b>Overall Housing Cost Burden</b>																		
< 30% of income	80%	79%	79%	75%	73%	66%	74%	70%	75%	73%	81%	70%	-6%	-8%	-3%	-2%	8%	5%
> 30% of income	17%	20%	20%	20%	18%	18%	26%	28%	24%	25%	18%	27%	8%	8%	4%	5%	1%	9%

Sources: U.S. Census Bureau, Census of Population and Housing 2000 (DP-3) and ACS 2008 - 2012 5-year estimates

TABLE 21 INCOME SOURCE COMPARISONS

	2000		2008-2012		RAW PERCENT CHANGE	RELATIVE PERCENT CHANGE	ABSOLUTE PERCENT CHANGE
<b>Gretna</b>							
Total Households	905		1,790		98%		
Households with earnings	759	83.9%	1,495	83.5%	97%	-0%	-0.4%
Households with Social Security income	245	27.1%	344	19.2%	40%	-29%	-7.9%
Households with Supplemental Security income	28	3.1%	24	1.3%	-14%	-57%	-1.8%
Households with Public Assistance income	27	3.0%	0	0.0%	-100%	-100%	-3.0%
Households with Retirement income	165	18.2%	460	25.7%	179%	41%	7.5%
Median Household Income	\$50,112.00		\$86,265.00		72%		
<b>Sarpy County</b>							
Total Households	43,495		58,876		35%		
Households with earnings	39,758	91.4%	52,034	88.4%	31%	-3%	-3.0%
Households with Social Security income	6,839	15.7%	11,359	19.3%	66%	23%	3.6%
Households with Supplemental Security income	692	1.6%	1,141	1.9%	65%	21%	0.3%
Households with Public Assistance income	745	1.7%	795	1.4%	7%	-21%	-0.3%
Households with Retirement income	7,875	18.1%	11,255	19.1%	43%	6%	1.0%
Median Household Income	\$53,804.00		\$81,109.00		51%		
<b>State of Nebraska</b>							
Total Households	666,995		721,026		8%		
Households with earnings	550,074	82.5%	591,314	82.0%	7%	-1%	-0.5%
Households with Social Security income	175,925	26.4%	196,436	27.2%	12%	3%	0.8%
Households with Supplemental Security income	19,743	3.0%	22,421	3.1%	14%	4%	0.1%
Households with Public Assistance income	18,640	2.8%	15,796	2.2%	-15%	-22%	-0.6%
Households with Retirement income	85,493	12.8%	97,902	13.6%	15%	6%	0.8%
Median Household Income	\$39,250.00		\$51,381.00		31%		

**COMPARISONS**

	2000		2008-2012		2000		2008-2012	
	Gretna	Gretna	Gretna	Gretna	Gretna	Gretna	Gretna	Gretna
	Sarpy	NE	Sarpy	NE	Sarpy	NE	Sarpy	NE
	volume				proportion			
Total Households	2.1%	0.1%	3.0%	0.2%				
Households with earnings	1.9%	0.1%	2.9%	0.3%	92%	102%	95%	102%
Households with Social Security income	3.6%	0.1%	3.0%	0.2%	173%	103%	100%	71%
Households with Supplemental Security income	4.0%	0.1%	2.1%	0.1%	194%	103%	69%	43%
Households with Public Assistance income	3.6%	0.1%	0.0%	0.0%	176%	107%	0%	0%
Households with Retirement income	2.1%	0.2%	4.1%	0.5%	101%	142%	134%	189%
Median Household Income	93.1%	127.7%	106.4%	167.9%				

Sources: U.S. Census Bureau, Census of Population and Housing 2000 (SF3) and ACS 2008 - 2012 5-year estimates

## INDUSTRY EMPLOYMENT

Analyzing employment by industry assists communities by determining the key components of their labor force. This section indicates the type of industry comprising the local economy, as well as identifying particular occupations that employ residents. Table 22 indicates employment size by industry for Gretna, Sarpy County, and the State of Nebraska for 2000 and 2012.

The data in Table 22 does not necessarily represent the number of jobs within Gretna but the type of jobs held by the residents of the community. These data indicate the residents of Gretna are employed in jobs at levels similar to Sarpy County and the State of Nebraska.

The top six employment sectors in Gretna are:

1. Educational, health and social services	37.2%
2. Professional, scientific, management, administrative, and waste	13.5%
3. Retail Trade	9.8%
4. Other Services (except Public Administration)	7.2%
5. Finance, insurance, real-estate and rental and leasing	5.8%
6. Construction	5.6%

The sector that ranked the lowest was agriculture, forestry, fishing, hunting and mining at 0.7%. It would be expected to have an increase in educational employment as new schools were built. The growth of Gretna has resulted in more employed with professional and management employment as well.

## COMMUTER TRENDS

Travel time to work is a factor used to gauge where Gretna's workforce is employed. However, the data may be skewed when comparing the travel time to the actual miles traveled to work due to potential traffic congestion during certain times of the day. Table 23 estimates how many residents of Gretna travel to work in each of the five minute intervals. The table indicates Gretna's workforce spent slightly more time traveling in 2012 compared to 2000; this is a common trend throughout the country.

The average travel time increased from 20.8 minutes in 2000 to 22.4 minutes in 2012. The largest percentage increase occurred in the 20 to 24 minutes interval. There was an increase of seven percent of people who work at home. 66% of the workforce is within 15 to 34 minutes of travel time to work that makes Gretna an ideal location to reach employment centers. A combined 70% of those commuting to work traveled more than 20 minutes in 2012 estimates, indicating the majority of people that live in Gretna work outside of Gretna.

Table 22  
**INDUSTRY  
COMPARISONS**

	2000		2008-2012		RAW % CHANGE	REL. % CHANGE	ABS.% CHANGE
<b>Gretna</b>							
Agriculture, forestry, fishing and hunting and mining	14	1%	19	0.7%	36%	-35%	0%
Construction	103	8%	142	5.6%	38%	-34%	-3%
Manufacturing	129	11%	102	4.0%	-21%	-62%	-7%
Wholesale Trade	72	6%	93	3.7%	29%	-38%	-2%
Retail Trade	200	16%	249	9.8%	25%	-40%	-7%
Transportation and warehousing, and utilities	75	6%	80	3.1%	7%	-49%	-3%
Information	50	4%	54	2.1%	8%	-48%	-2%
Finance, insurance, real estate and rental and leasing	107	9%	148	5.8%	38%	-34%	-3%
Professional, scientific, management, administrative, and waste management services	48	4%	343	13.5%	615%	243%	10%
Educational, health and social services	275	23%	946	37.2%	244%	65%	15%
Arts, entertainment, recreation, accommodation and food service	71	6%	115	4.5%	62%	-22%	-1%
Other services (except Public Administration)	47	4%	184	7.2%	291%	88%	3%
Public Administration	29	2%	68	2.7%	134%	12%	0%
<b>Totals</b>	<b>1,220</b>		<b>2,543</b>		<b>108%</b>		
<b>Sarpy County</b>							
Agriculture, forestry, fishing and hunting and mining	335	1%	637	0.7%	90%	36%	0%
Construction	3,753	6%	5,442	6.3%	45%	4%	0%
Manufacturing	4,866	8%	8,571	10.0%	76%	26%	2%
Wholesale Trade	2,421	4%	2,290	2.7%	-5%	-32%	-1%
Retail Trade	7,883	13%	9,634	11.2%	22%	-13%	-2%
Transportation and warehousing, and utilities	4,210	7%	5,183	6.0%	23%	-12%	-1%
Information	2,012	3%	1,980	2.3%	-2%	-30%	-1%
Finance, insurance, real estate and rental and leasing	7,181	12%	7,347	8.6%	2%	-27%	-3%
Professional, scientific, management, administrative, and waste management services	6,127	10%	9,337	10.9%	52%	9%	1%
Educational, health and social services	12,153	20%	19,748	23.0%	62%	16%	3%
Arts, entertainment, recreation, accommodation and food service	4,468	7%	6,483	7.5%	45%	4%	0%
Other services (except Public Administration)	2,603	4%	2,524	2.9%	-3%	-31%	-1%
Public Administration	3,335	5%	6,747	7.9%	102%	44%	2%
<b>Totals</b>	<b>61,347</b>		<b>85,923</b>				
<b>State of Nebraska</b>							
Agriculture, forestry, fishing and hunting and mining	48,942	6%	43,100	4.6%	-12%	-17%	-1%
Construction	56,794	6%	60,317	6.5%	6%	0%	0%
Manufacturing	107,439	12%	98,527	10.6%	-8%	-14%	-2%
Wholesale Trade	31,265	4%	25,081	2.7%	-20%	-24%	-1%
Retail Trade	106,303	12%	113,544	12.2%	7%	1%	0%
Transportation and warehousing, and utilities	53,922	6%	56,438	6.1%	5%	-1%	-0%
Information	21,732	2%	17,776	1.9%	-18%	-23%	-1%
Finance, insurance, real estate and rental and leasing	67,370	8%	71,292	7.7%	6%	-0%	-0%
Professional, scientific, management, administrative, and waste management services	63,663	7%	74,928	8.0%	18%	11%	1%
Educational, health and social services	181,833	21%	217,308	23.3%	20%	13%	3%
Arts, entertainment, recreation, accommodation and food service	63,635	7%	72,291	7.8%	14%	7%	1%
Other services (except Public Administration)	40,406	5%	42,496	4.6%	5%	-1%	-0%
Public Administration	33,933	4%	37,906	4.1%	12%	5%	0%
<b>Totals</b>	<b>877,237</b>		<b>931,004</b>				

Sources: U.S. Census Bureau, Census of Population and Housing 2000 (SF3) and ACS 2008 - 2012 5-year estimates

Table 23 TRAVEL TIME TO WORK

	2000		2008-2012		% CHANGE
Worked at home	24	2.0%	205	9%	7%
Less than 5 minutes	60	5.0%	148	7%	2%
5 to 9 minutes	156	13.0%	195	9%	-4%
10 to 14 minutes	99	8.3%	115	5%	-3%
15 to 19 minutes	219	18.3%	236	11%	-8%
20 to 24 minutes	243	20.3%	644	29%	9%
25 to 29 minutes	109	9.1%	231	11%	1%
30 to 34 minutes	197	16.5%	331	15%	-1%
35 to 39 minutes	29	2.4%	50	2%	-0%
40 to 44 minutes	24	2.0%	75	3%	1%
45 to 59 minutes	46	3.8%	129	6%	2%
60 to 89 minutes	4	0.3%	18	1%	0%
90+ minutes	10	0.8%	59	3%	2%
<b>Total</b>	<b>1,196</b>		<b>2,231</b>		
<b>Mean Travel Time</b>	<b>20.8</b>		<b>22.4</b>		<b>7.7%</b>

Sources: U.S. Census Bureau, Census of Population and Housing 2000 (SF3) and ACS 2008 - 2012 5-year estimates

## SALES AND FISCAL PROFILE

Retail trade is an important part of a local economy. Examining this allows Gretna to analyze the level of retail activity occurring within the City's corporate limits. Some of the most important economic activities for communities are transactions of goods and services, which take place between consumers and local businesses.

Table 24 examines the sales tax collected and the pull factor for Gretna and communities similar within the State. In addition, the table breaks down the Net Taxable Sales into a per capita number for comparison purposes.

The Pull Factor represents the ability of a community to support its local citizens versus how well the community attracts sales dollars from outside of their existing population. If a community has a Pull Factor of "1", the amount of money being spent by citizens elsewhere equals the amount being spent in the community by outside persons. If the number is greater than 1 then it is attracting additional sales from outside the community and if the number is less than 1 the community is losing sales to other areas.

Of the five communities listed in Table 24 all had pull factors less than 1 before 2009. Papillion reported a positive pull factor in 2009 and has continued doing very well with a 1.84 in 2012. Gretna began to report above "1" beginning in 2010. Gretna continues to have outside money coming into its local economy. A large metropolitan city, such as Omaha, will affect the surrounding communities and its pull factor. These statistics show that Gretna and Papillion are offering more services and options to the general public than they did in 2010.

Table 24 SALES TAX TRENDS AND PULL FACTOR COMPARISONS

	Gretna (2010 Population - 4,441 )			Bellevue (2010 Population - 50,137)			Papillion (2010 Population - 18,894)		
	Sales Tax Collected (\$mil)	Sales Tax Per Capita	Pull Factor	Sales Tax Collected (\$mil)	Sales Tax Per Capita	Pull Factor	Sales Tax Collected (\$mil)	Sales Tax Per Capita	Pull Factor
<b>2004</b>	\$2.81	\$551	0.84	\$19.83	\$420	0.64	\$7.23	\$358	0.54
<b>2005</b>	\$3.35	\$614	0.90	\$19.98	\$423	0.62	\$7.76	\$368	0.54
<b>2006</b>	\$3.25	\$548	0.78	\$20.54	\$432	0.62	\$9.81	\$448	0.64
<b>2007</b>	\$3.42	\$544	0.74	\$20.15	\$417	0.57	\$15.53	\$680	0.94
<b>2008</b>	\$3.32	\$508	0.69	\$20.23	\$407	0.56	\$17.25	\$728	0.99
<b>2009</b>	\$3.11	\$461	0.66	\$19.6	\$384	0.55	\$17.21	\$709	1.01
<b>2010</b>	\$3.37	\$758	1.07	\$20.41	\$407	0.57	\$18.6	\$984	1.41
<b>2011</b>	\$4.11	\$791	1.05	\$20.26	\$391	0.52	\$19.8	\$1,044	1.40
<b>2012</b>	\$4.67	\$866	1.13	\$21.04	\$400	0.52	\$27.84	\$1,419	1.84
<b>2013</b>	\$6.25			\$21.58			\$30.88		
<b>CHANGE 2010-2012</b>	<b>38.6%</b>	<b>14.3%</b>	<b>5.5%</b>	<b>3.1%</b>	<b>-1.8%</b>	<b>-9.1%</b>	<b>49.7%</b>	<b>44.2%</b>	<b>30.6%</b>

	La Vista (2010 Population - 15,758)			Wahoo (2010 Population - 4,508)			Nebraska (2010 Population - 1,826,341)	
	Sales Tax Collected (\$mil)	Sales Tax Per Capita	Pull Factor	Sales Tax Collected (\$mil)	Sales Tax Per Capita	Pull Factor	Sales Tax Collected (\$mil)	Sales Tax Per Capita
<b>2004</b>	\$8.79	\$601	0.92	\$1.86	\$441	0.67	\$1,146.19	\$657.91
<b>2005</b>	\$9.39	\$601	0.88	\$1.91	\$451	0.66	\$1,195.88	\$682.69
<b>2006</b>	\$8.95	\$560	0.80	\$1.91	\$452	0.65	\$1,230.73	\$699.10
<b>2007</b>	\$9.16	\$561	0.77	\$2.05	\$488	0.67	\$1,285.84	\$726.50
<b>2008</b>	\$10.13	\$610	0.83	\$2.11	\$502	0.68	\$1,314.94	\$737.92
<b>2009</b>	\$10.03	\$594	0.85	\$1.97	\$472	0.67	\$1,261.91	\$702.38
<b>2010</b>	\$9.64	\$612	0.86	\$2.09	\$464	0.65	\$1,299.18	\$711.36
<b>2011</b>	\$10.3	\$608	0.82	\$2.13	\$473	0.64	\$1,377.47	\$747.91
<b>2012</b>	\$11.06	\$637	0.83	\$2.24	\$499	0.61	\$1,429.34	\$770.39
<b>2013</b>	\$11.89			\$2.13			\$1,507.28	\$806.67
<b>CHANGE 2010-2012</b>	<b>14.6%</b>	<b>4.2%</b>	<b>-3.3%</b>	<b>7.3%</b>	<b>7.5%</b>	<b>-5.8%</b>	<b>10.0%</b>	<b>8.3%</b>

Sources: Nebraska Department of Revenue, 2004-2013

# Community Facility Profile

- Parks and Recreational Facilities
- Educational Facilities
- Fire and Police Protection
- City Facilities
- Transportation Facilities
- Communication Facilities
- Public Utilities
- Environmental Conditions
- Health Facilities
- Energy Element



# Community Facility Profile

State and local governments provide a number of services for their citizens. The people, buildings, equipment and land utilized in the process of providing these services are referred to as public facilities.

Public facilities represent a wide range of buildings, utilities, and services that are provided and maintained by the different levels of government. These facilities are provided to insure the safety, well being and enjoyment of the residents of a jurisdiction, in this case, the City of Gretna. Facilities and services provide City residents with social, cultural, educational, and recreational opportunities, as well as law enforcement and fire protection services designed to meet the public need. It is important for all levels of government to anticipate the future demand for their goods and services if they are to remain strong and vital.

The first step is to evaluate the ability of the City to meet existing and future demand while determining the level of services that will need to be provided. The analyses of existing facilities as well as the future demand for services are contained in the Facilities Plan. Alternatively, in some instances, there are a number of services not provided by the local or state governments but are provided by non-governmental, private or non-profit organizations for the community. These organizations are equally important providers of services to the community and therefore should not be overlooked.

The Community Facilities component of a Comprehensive Development Plan reviews present capacities of all public and private facilities, programs and services. This section evaluates the current demands and accepted standards to determine whether capacity is adequate, as well as determine what level of service is required to meet future demands within the planning period. Finally, recommended improvements for community facilities and services that are not adequate for present or future needs are provided.

The Community Facilities for Gretna are divided into the following categories:

- Parks and Recreational Facilities
- Educational Facilities
- Fire and Police Protection
- City Facilities
- Transportation Facilities
- Communication Facilities
- Public Utilities
- Environmental Conditions
- Health Facilities
- Energy Element

## PARKS AND RECREATIONAL FACILITIES

Below is a short inventory of existing park and recreation facilities within Gretna and its one mile jurisdiction. In the Achieve section of this Plan there is a Parks and Recreation component that will act as a Master Plan for the future parks and recreation of the City of Gretna. This portion of the plan will evaluate the existing facilities and make recommendations for future facilities.

## PARKS WITHIN GREтна AND SURROUNDING SUBDIVISIONS

There are 9 parks and outdoor recreational areas in or adjacent to the City of Gretna. These facilities are maintained by the City or a homeowners association. Table 25 lists the parks and the amenities they contain:

**TABLE 25: CITY OF GREтна PARKS INFORMATION**

Park Name	General Location	Amenities
Leo Royal Soccer Complex	Hwy 6 and Lincoln	5 Soccer Fields, Bleachers, Picnic Area with Shelter, Permanent Restrooms, Walking Trail
Lincoln Place Park	210th and Lincoln	Playstructure, Walking Paths for access, 2 horseshoe pits, large open field
Founders Memorial Park	Wallace and Aberdeen	Picnic Shelter, Greenspace
Peterson Memorial Park	Angus and Cherokee	Basketball Court, Gazebo, 2 Picnic Shelters, Picnic Tables, Grills, Swings, 2 Flower Gardens, Separated Toddler and Children’s Play Areas/Structures, Mature Trees, Swimming Pool, Walking Trail, Flag Pole Memorial
North Park	Hwy 6 and Angus	2 Fenced and Lighted Tennis Courts, 1 Lighted Basketball Court, 15 Lighted Horseshoe Pits, 1 Picnic Shelter, 1 Tee Ball Field, 3 Softball Fields, Toddler Playstructure, Swing Set, Tornado Slide, Merry-go-round, Mature Trees, Open Space, 1 Fenced and Lighted Baseball Field
Plum Creek Park	Bob White and Plum Creek	Playstructure, Softball Field, Multipurpose Field (soccer goals), Walking Trail
Chestnut Ridge Park	Highview and 217th	Playstructure, Picnic Shelter, Swings
Crystal Creek Park	Crystal Ave and Fairby	Playstructure, Picnic Facilities, Benches, Greenspace
The Fields at Gretna	210th and Capehart Road	2 Complexes with eight skinned Baseball/Softball fields, 4 Soccer/Football Fields, Concession Stand, Restrooms, Parking

Source: JEO Survey 2008, 2014

## GOLF COURSES

Gretna has seven golf courses within 10 miles of the City and their information is given below:

Golf courses	Location	Number of Holes	Public or Private
Tiburon Golf Club	Omaha, NE	27 Holes	Public
Country Drive Golf Course	Ashland, NE	9 Holes	Public
Iron Horse Golf Club	Ashland, NE	18 Holes	Public
Shadow Ridge Country Club	Omaha, NE	18 Holes	Private
Quarry Oaks Golf Club	Ashland, NE	18 Holes	Public
Pacific Springs Golf Club	Omaha, NE	18 Holes	Public
Elkhorn Ridge Golf Course	Elkhorn, NE	9 Holes	Public

Source: www.Golfink.com

## OTHER RECREATIONAL AREAS

Additional recreational areas surrounding Gretna include:

- Schram State Park
- Wehrspann Lake
- Mahoney State Park

## EDUCATIONAL FACILITIES

### PUBLIC SCHOOLS

The Gretna School District has been growing by leaps and bounds the past decade with the growth of the Omaha Metropolitan Area. Currently the district has four elementary schools, one middle school and one high school, below is a chart representing the past enrollment trends in the district. The existing high school is master planned to be expanded to 1,400 students, and there are plans for a second middle school and future elementary schools as the needs arise with rising enrollment. The site of the three new proposed school buildings is located north of 180th and Highway 370. The enrollment trends for the Gretna School District are provided in Table 26 and the current enrollment and capacity in Table 27. Figure 6 displays the school boundaries of Gretna and surrounding communities.

**TABLE 26: GRETNA SCHOOL DISTRICT ENROLLMENT TRENDS**

<b>ENROLLMENT TRENDS:</b>												
	'00-'01	'01-'02	'02-'03	'03-'04	'04-'05	'05-'06	'06-'07	'07-'08	'08-'09	'09-'10	'10-'11	'11-'12
K	90	97	112	144	194	184	219	232	241	286	287	302
1	103	90	109	124	147	199	201	224	227	254	294	301
2	103	113	101	138	142	156	217	217	240	237	262	296
3	101	113	120	115	149	162	166	229	224	256	248	269
4	107	109	121	133	132	152	178	181	232	228	268	257
5	94	118	115	133	146	148	166	183	198	235	237	268
6	121	103	127	139	153	147	161	172	191	198	239	247
7	114	130	114	141	157	158	154	165	185	205	206	238
8	127	123	141	130	145	158	173	160	173	181	210	206
9	115	133	130	158	147	158	170	179	169	180	194	215
10	133	116	134	136	171	152	162	172	187	169	188	196
11	102	130	115	135	139	171	154	165	175	186	171	188
12	108	100	132	115	141	139	177	158	178	179	189	172
CONTRACTED	5	7	7	9	11	10	10	6	13*	14*	13*	*9
<b>TOTAL</b>	<b>1423</b>	<b>1482</b>	<b>1578</b>	<b>1750</b>	<b>1974</b>	<b>2094</b>	<b>2308</b>	<b>2443</b>	<b>2633</b>	<b>2808</b>	<b>2993</b>	<b>3155</b>
<b>% INCREASE</b>		<b>4.6%</b>	<b>4.1%</b>	<b>6.5%</b>	<b>10.9%</b>	<b>12.8%</b>	<b>6.1%</b>	<b>10.2%</b>	<b>5.8%</b>	<b>7.8%</b>	<b>6.6%</b>	<b>5.4%</b>

\* Contracted students included in grade figures listed above

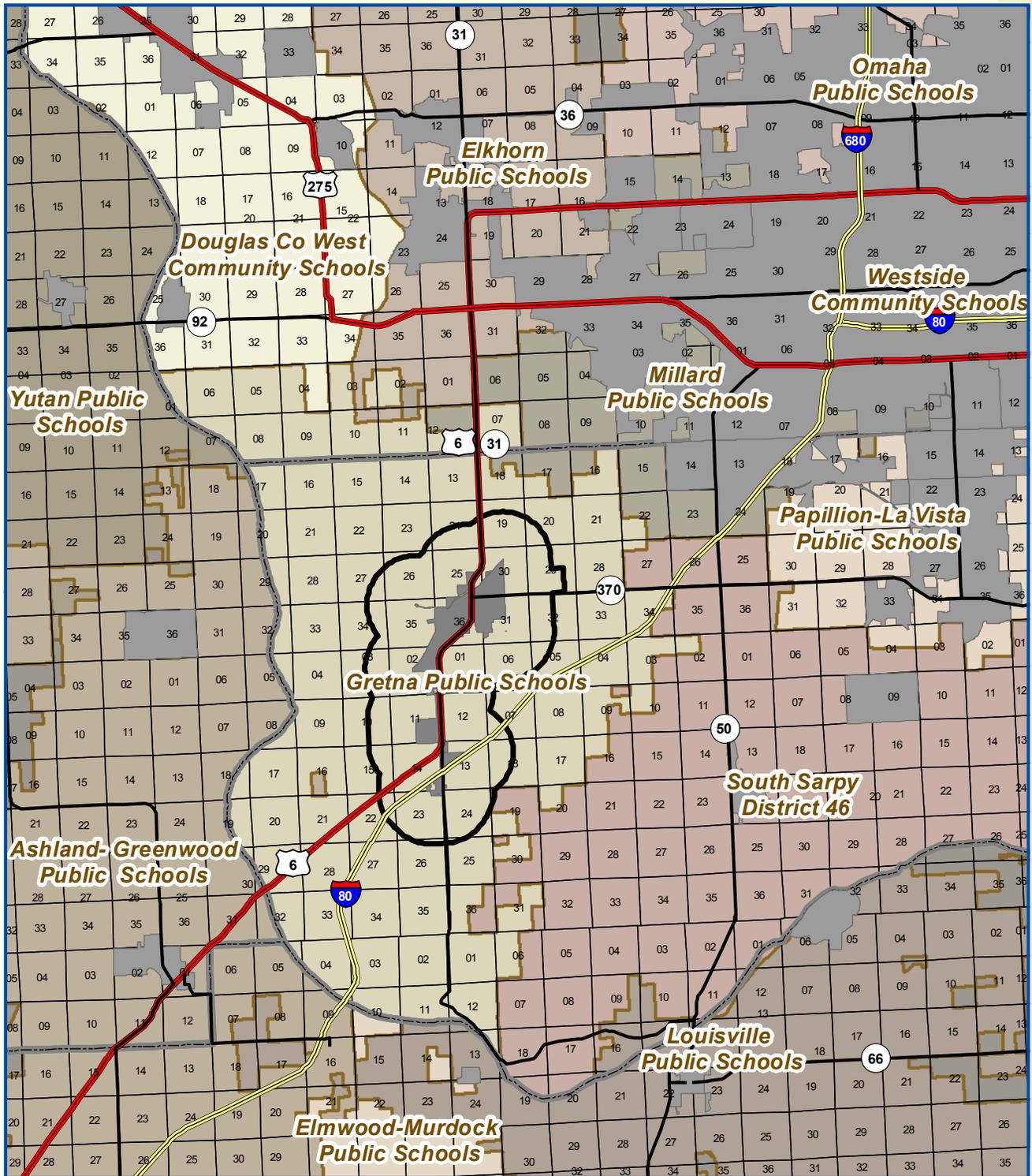
Source: Gretna Public Schools Annual Community Report, 2013

**TABLE 27: 2013 ENROLLMENT AND CAPACITY IN GRETNA SCHOOLS**

School Name	2013 Enrollment	2012 Capacity
Gretna High School	849	800
Gretna Middle School	818	852
Gretna Elementary School	478	477
Thomas Elementary School	501	552
Palisades Elementary School	584	532
Whitetail Creek	456	552
<b>Total</b>	<b>2,449</b>	<b>3,250</b>

Sources: State of Nebraska 2103-14 Report; Gretna Public Schools Annual Community Report, 2013

FIGURE 6: SCHOOL DISTRICT MAP



<b>Legend</b>		<b>City of Gretna</b>	
<b>School District</b>			
	Millard Public Schools		Interstate
	Omaha Public Schools		U.S. Highway
	Papillion-La Vista Public Schools		State Highway
	South Sarpy Dist 46		County Boundaries
	Westside Community Schools		Corporate Boundaries
	Yutan Public Schools		Gretna 1-Mile ETJ
			Sections

Created By: J. Sather  
Date: DEC 2008  
Revised: AUG 2014  
Software: ArcGIS 10.2  
File: 140271.00

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## POST SECONDARY EDUCATION

There are numerous educational opportunities in the Omaha and Lincoln areas for post secondary education in just about any field of study. A few of the closest larger enrollment institutions offering a wide variety of disciplines for their students are shown in Table 28.

**TABLE 28: COLLEGES AND UNIVERSITIES IN THE GRETNA AREA OVER 2,000 STUDENTS**

Facility	Location	2008 Enrollment	Miles from Gretna
University of Nebraska at Omaha	Omaha, NE	10,150	12
Creighton University	Omaha, NE	5,806	12
University of Nebraska Medical Center	Omaha, NE	2,448	12
Metropolitan Community College	Omaha, NE	6,175	23
Bellevue University	Bellevue, NE	2,723	24
Iowa Western Community College	Council Bluffs, IA	2,840	33
University of Nebraska at Lincoln	Lincoln, NE	19,806	38

Source: www.city-data.com

## FIRE AND POLICE PROTECTION

### FIRE AND RESCUE

Two fire halls serve the Gretna Area and 71 square miles in western Sarpy County with Fire and EMS services. The two stations have six engines, four ambulances, and three grass trucks. The Gretna Fire and Rescue Department is a joint venture between the City of Gretna and the Rural Fire District which is illustrated in Figure 7. The two entities share a fire chief and cooperate so completely that most volunteers don't know there are actually two budgets and ownership of the equipment is split by the two entities.

Station 1 Fire Hall located at 11175 South 204th St. Station 1 has four engines; a 1987 Smeal Pumper, 2006 Pierce Dash Pumper, 1993 GMC Chevy Tanker/Pumper, and 2007 Pierce Dash Rescue Pumper. There are two utility trucks (2011 and 2012), as well as two Braun ALS rescue units (2009 and 2011).

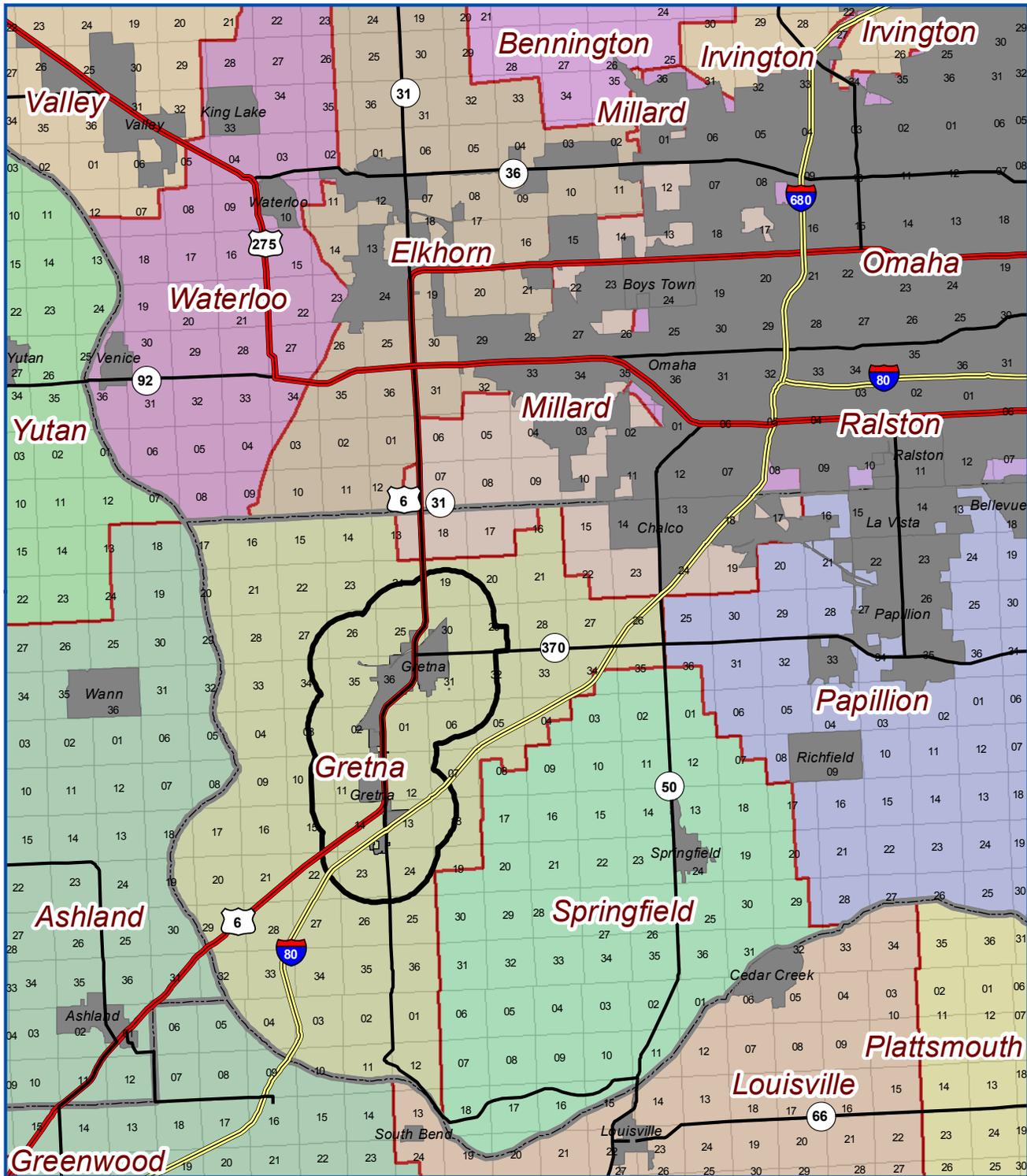
Station 2 was built on the property acquired near 168th and Cornhusker Road. This station was intended to be entirely Rural Fire District property and equipment. Station 2 has two engines, a 2000 E-One Pumper and 1986 Smeal Tanker, one 2001 utility truck, and two rescue squads. The retired 1947 antique engine is located at Station 2.

Rescue is operated as a part of the Fire Department but gets all its funding through donations and fundraisers. In 2008, Gretna had a total of 43 volunteer firefighters that can be called up by the chief and sent to either a rural or City emergency. The chief has complete control on the use of any and all equipment to respond to any emergency within either the City or the rural district.

### LAW ENFORCEMENT

The City of Gretna contracts with the Sarpy County Sheriff's Office to provide police protection for the City. Five deputies are assigned to a satellite office in the Gretna City Hall, along with a resource officer stationed at the Gretna High School during school hours. At the present time, the contract relationship provides the City with better police protection than the City could afford on its own. As the City continues to grow and the population base increases, Gretna will need to consider the possibility of adding their own police force.

FIGURE 7: FIRE DISTRICT MAP



<b>Legend</b>		
<b>Fire District</b>		
	Omaha	
	Papillion	
	Plattsmouth	
	Ralston	
	Springfield	
	Waterloo	
	Yutan	

# City of Gretna

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 Date: DEC 2008  
 Software: ArcGIS 10.2  
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## CITY FACILITIES

### CITY HALL

Gretna City Hall is located at 204 N McKenna Avenue and was constructed in 1939. Offices located at City Hall include the Mayor's Office, City Administrator, Public Works Director, City Clerk/Treasurer, Building Inspector/Zoning Administrator, Building and Zoning Department, Sarpy County Sheriff's satellite office and the Utility Department.

City Hall also houses the Gretna Council Chambers and a large community room with a kitchen and restrooms. The Council Chambers are used by various elected and appointed bodies for regular meetings and special events. The community room can be rented for a fee to different organizations and groups.

City Hall currently serves the needs of the community but its administrative needs may will increase as the City continues to grow. Renovations have been made to update the presentation area. As of 2014, no plans have been created for a new City Hall.

### CITY MAINTENANCE FACILITIES

The City Maintenance Building is located just north of City Hall on McKenna Avenue. Currently, the existing facility cannot house all of the City equipment and consideration has been given for the construction of a new public works/maintenance building facility. This potential facility is being studied for a location north of the Highway 370. The facility is expected to be designed to house offices, trucks, equipment, and have covered space for dirt, salt, sand, etc.

### PUBLIC LIBRARY

With the addition of the Children's Library, there are currently two library to serve Gretna. The original library opened in 1992 at 736 South Street. The library occupies 4,000 sq. ft. on a single level in its own building. The library is open daily for a total of 50 hours each week. The library provides service to the residents of Gretna and by contract to nonresident borrowers from rural Sarpy County and residents with cards from other Nebraska libraries. The library staff includes seven employees.

At the end of the 2008 Fiscal year, the library's collections included 21,885 books, 7,730 E-books, 1,632 media items, 100 periodical subscriptions and 900 downloadable audio books. The library has four public access computers and one public access catalog computer station. Digital services include a web site, on-line catalog access, internet access, general application software such as word processing, and a number of on-line databases/resources.

The Gretna Public Library provides a number of services directed toward special populations and interests. The Gretna Children's Library was built inside the remodeled post office located downtown at 119 North McKenna Street. Children's services include story and learning programs with 5,709 participants in 173 programs in FY 2008. Teens are offered a unique collection of reading targeted to their interests. A large print collection is supported for those with limited vision.

The Gretna Public Library is operated and supported by three separate groups that all work together to provide the community with the best possible facilities and services. There is a seven member Library Board of Trustees that governs the operation of the Library. The Gretna Public Library Foundation provides long-term planning and financial support to the library and it governed by its own Board. Lastly, the Friends of the Library raise money on an ongoing basis to purchase items not covered by the library's budget.

## CEMETERIES

**La Borde Cemetery** is owned and maintained by the City of Gretna. The cemetery is 1.77 acres in size and located in the southeast corner of 204th Street and Hwy 370. Currently there is no room for expansion, but plots are still available.

Other cemeteries in Gretna's jurisdiction include **Holy Sepulcher Cemetery** and **Pennsylvania Dutch Cemetery**. Holy Sepulcher is on Schram Road west of town and Pennsylvania Dutch Cemetery is located northeast of Gretna on 198th Street north of Hwy 370.

## TRANSPORTATION FACILITIES

The transportation facilities are critical to each resident within a community. Each resident and property owner has specific transportation needs, ranging from rail service, bus service, air transportation, as well as vehicular and pedestrian transportation. Not all of these transportation options are available directly within Gretna, but can be accessed by residents or businesses through other communities in the Omaha metropolitan area.

### TRUCK LINES

The City of Gretna is served by the interstate and intrastate carriers.

### BUS SERVICE

As of 2014, the City of Gretna has no direct bus service.

### RAILROAD

The City of Gretna has no rail access along the Burlington Northern Santa Fe line. The nearest commercial service is the Omaha Amtrak station in Downtown Omaha.

### AIRPORT

The City of Gretna has no airport in its jurisdiction. The closest public airport is the Millard Airport owned and operated by the Omaha Airport Authority. The nearest commercial airport is Eppley Airport in Northeast Omaha.

## LOCAL TRANSPORTATION NETWORK

The vehicular transportation system within Gretna's extraterritorial jurisdiction is maintained by the City of Gretna, Sarpy County, or Nebraska Department of Roads. Gretna's transportation network is addressed in more detail within the Transportation Plan of the Achieve Section.

## COMMUNICATION FACILITIES

### TELECOMMUNICATIONS

There is a competitive telecommunication market within the Omaha and Council Bluffs area. With multiple companies providing a variety of high-end communications services, Gretna is in an ideal location for telecommunications.

## RADIO STATIONS

Strongest AM and FM Stations

KFAB 1110 AM	KKAR 1290 AM	KOIL 1020 AM
KOZN 1620 AM	KYDZ 1180 AM	KOMJ 590 AM
KLNG 1560 AM	KCRO 660 AM	WHB 810 AM
KTIC 840 AM	KOSR 1490 AM	KHLP 1420 AM
KMA 960 AM	KEFM 96.1 FM	KGBI-FM 100.7 FM
KLTQ 101.9 FM	KMLV 88.1 FM	KGOR 99.9 FM
KQCH 94.1 FM	KEZO-FM 92.3 FM	KQKQ-FM 98.5 FM
KSRZ 104.5 FM	KIWR 89.7 FM	KBBX-FM 97.7 FM
KFRX 102.7 FM	KVNO 90.7 FM	KKCD 105.9 FM
KIOS-FM 91.5 FM	KVSS 88.9 FM	K299AK 107.7 FM
KRQC 93.3 FM	KIBZ 106.3 FM	KFMT-FM 105.5

## TELEVISION STATIONS

Strongest Broadcast Stations

KPTM 42 Omaha, NE	KXVO 15 Omaha, NE	KUON-TV 12 Lincoln, NE
KYNE-TV 26 Omaha, NE	KMTV 3 Omaha, NE	KETV 7 Omaha, NE
WOWT-TV 6 Omaha, NE	KVSS-LP 67 Omaha, NE	K21ES 21 Columbus, NE
KLKN 8 Lincoln, NE	K54EU 54 Columbus, NE	K61GA 61 Omaha, NE
K53EY 53 Omaha, NE		

## NEWSPAPERS

Listed below are the various newspapers serving the residents of Gretna.

- Gretna Breeze (weekly)
- Gretna Guide and News (weekly)
- Omaha World Herald (daily)
- Lincoln Journal Star (daily)

## SERVICE PROVIDERS FOR THE CITY OF GRETNA

Service Providers	Natural Gas	Electric	Cable/ Satellite Television	Telephone	Internet
OPPD		X			
Century Link			X	X	X
Cox Cable			X	X	X
Galaxy Cable			X		
Unite Private Networks					X
Black Hills Energy	X				

## PUBLIC UTILITIES

The City of Gretna currently provides the following public utility service to its residents:

- Water distribution
- Sanitary sewer collection, pumping and conveyance
- Nebraska Crossing Wastewater Treatment

## WATER SYSTEM

In 2008, the City of Gretna's water system consisted of over 48 miles of 4" to 12" diameter water mains, one 500,000 gallon elevated water storage tank and five municipal wells. The total production from the five wells was 3,010 gallon per minute (gpm) with Well #4 and Well #6 being the primary wells. In 2008, the raw water required no treatment prior to being pumped into the water system. The water distribution system is comprised primarily of cast iron and ductile iron mains with a very small portion being PVC. There were approximately 11 miles of 12" diameter ductile iron mains that provide the distribution network for the smaller 4", 6", and 8" diameter interior mains. The 2008 system served approximately 2200 customers both inside and outside the corporate limits. The elevated water tank on Highway 370 was constructed in 1977 with a high water level of only 130 feet. The City is in the process of defining its water service area with Metropolitan Utilities District (MUD) to the north and east.

Improvements to the water system include a new 2 million gallon water tower and booster pumping station on Capehart Road that provides a high water level of 167 feet. It is located within the developing regional park and became operational in October 2012. The water storage reservoir increases water pressures and provides added fire flow protection.

A new 16" diameter main is extended south of Capehart Road along Highway 6/31. This new system will allow the City of Gretna to provide additional customers as new developments occur. Extensions east along Highway 370 will be coordinated with development of this corridor east of 200th Street.

## SEWER COLLECTION AND TREATMENT

The City of Gretna's sanitary sewer collection consists of 8" diameter clay tile in the older portions of the City, and PVC in the newer areas. There are presently eight (8) sanitary sewer pumping stations that serve six (6) residential developments within the collection system. The Schram Road pumping station located in the southeast portion of the City serves a large area east of Highway 6/31, and pumps to the 18" diameter west interceptor sewer in 216th Street. The City's main pumping station is located at the old wastewater treatment plant site southwest of 216th Street and Gruenther Road. This facility has the capacity to pump over 2000 gpm via a 12" diameter force main to the City's gravity interceptor sewer located at the northwest corner of the intersection of Highway 370 and Highway 6/31.

As a result of the City entering into an agreement with the City of Omaha to treat their sewage, the City of Gretna constructed six (6) miles of 18" to 30" diameter RCP interceptor sewers to connect to the Sarpy County industrial sewer in 2001. The City owns and maintains the original interceptor sewer to 144th Street as well as the parallel sanitary sewer that was constructed in 2004. The parallel sewer provides the capacity to serve all the area within this reach of the Papio basin.

The City presently owns and operates the 35,000 GPD Wastewater Treatment Plant at the Nebraska Crossing Outlet Mall. The current capacity limits the area to be served. Planned improvements are to provide sanitary sewer service to the newly annexed areas south of Capehart Road to the I-80 interchange. This portion of the Buffalo Creek watershed will require interceptor sewers and a wastewater treatment plant to be constructed in the long-term future in the area of 192nd Street and Platteview Road. The City has been involved with the Sarpy County planning process to determine the most economical way to serve this area. The City is currently seeking an agreement with Omaha and Sarpy County to serve this area via connection to the City's sewer system in the Papio Basin, which would include the discontinued operation of the City sewer plant at the new outlet mall, plus the discontinued operation of a private sewer facility which serves the Flying J Truck Stop.

### SOLID WASTE COLLECTION (NON-PUBLIC UTILITY)

Trash service is handled by five different private haulers. These haulers include: Papillion Sanitation, Gretna Sanitation, Abe's Trash, and Waste Management. The City does not contract with these companies. Residents have the ability choose which service they would like to use.

### ENVIRONMENTAL CONDITIONS

In order to formulate a truly valid and "comprehensive" plan for the future of Gretna, it is necessary to evaluate the natural conditions that currently exist and discuss how these conditions can either promote or hinder future development. An assessment of environmental conditions allows the community to determine the impacts these factors may have on limiting the locations where various future land uses may be located within the planning jurisdiction of the City.

### WELLHEAD PROTECTION AREAS

Due to Sarpy County's use of SID's for development outside of existing City service areas, Gretna has several wellhead protection areas within their designated growth areas. While these areas do not severely impact the types of land use that can be allowed in them, care should be taken to ensure the protection of the wells from possible contamination due to development. These areas are depicted on the Environmental Conditions Map.

### 100-YEAR FLOODPLAIN

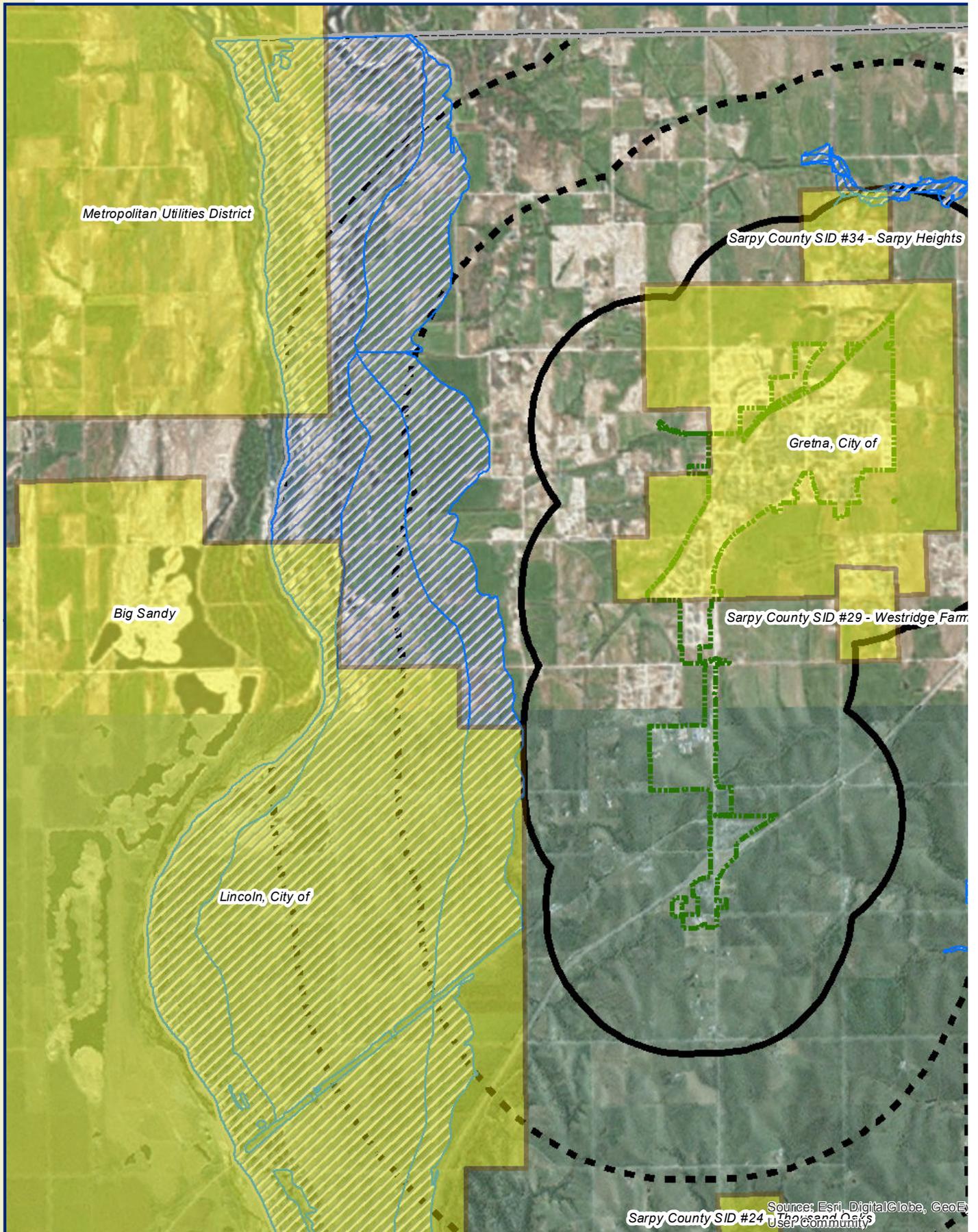
The only significant floodplain in Gretna's future growth area is on the west side of the City as you go towards the Platte River. Development within a floodplain is generally discouraged due to the difficulty of ensuring safety and the potential damage a development can have on that natural environment. Floodplain areas are generally reserved for less intensive uses such as agriculture, and recreation/open space.

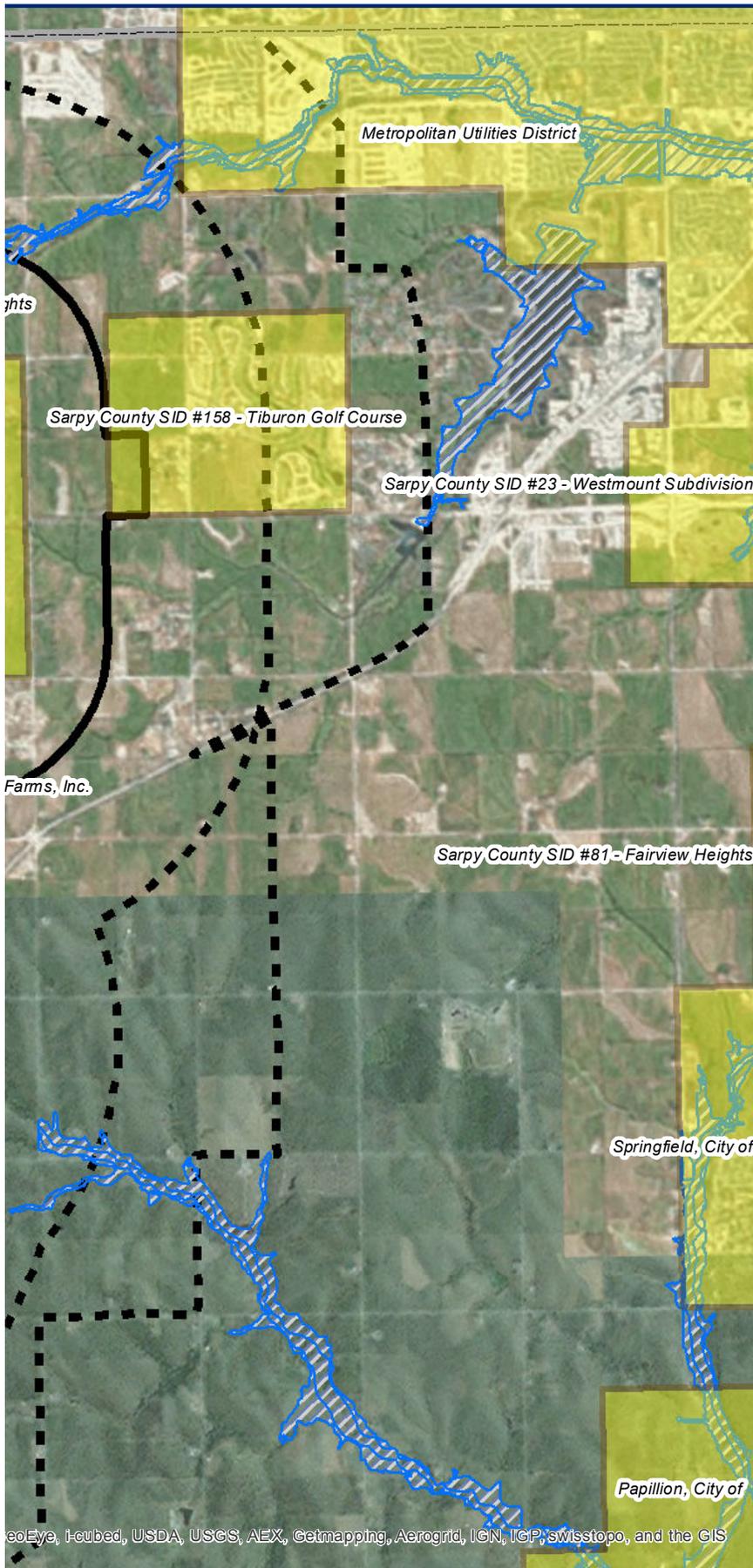
### STORM WATER RETENTION/DETENTION

All new developments within Gretna's jurisdiction should implement regional storm water retention/detention areas when possible to avoid smaller individual areas. Such retention/detention areas should be designed according to Gretna's Subdivision Regulations and Polices adopted by the City of Gretna based upon the Papillion Creek Watershed Partnership.

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FIGURE 8: ENVIRONMENTAL CONDITIONS MAP



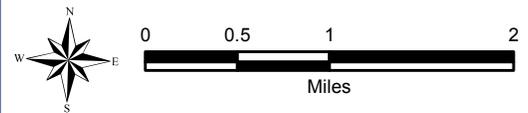


# City of Gretna

## Environmental Conditions Map

### Legend

-  Wellhead Protection Area
-  100yr Floodplain
-  Corporate Limits
-  One-Mile ETJ
-  2 Mile Growth Area
-  3 Mile Growth Area



Created By: J. Sather  
 Date: DEC 2008  
 Revised: August 20 14  
 Software: ArcGIS 10.2  
 File: 140271.00



This map was prepared using information from record drawings supplied by JEO and/or other applicable city, county, federal, or public or private entities. JEO does not guarantee the accuracy of this map or the information used to prepare this map. This is not a scaled plot.

## HEALTH FACILITIES

### HOSPITALS

#### **Alegent Health System's Lakeside Hospital**

Since opening in 2004 Lakeside Hospital has grown to include 83 licensed beds, 650 physicians on active staff, 126 full-time registered nurses, and 494 full-time employees. Lakeside is one of the most technologically integrated hospitals in the world; considered a "smart hospital" Lakeside employs wireless technology, a filmless and paperless environment and an all-digital diagnostic center. Other features include minimally invasive surgical suites and bedside registration. Located at 168th and West Center Road Lakeside Hospital is about 10 miles north and east of Gretna.

#### **Alegent Health System's Midlands Community Hospital**

The Alegent Health Facility is located at 11111 S. 84th St., Papillion and is approximately 10 miles east of Gretna on Highway 370. It is a short-term hospital with 122 beds, of which 106 are adult and pediatric and 16 are intensive care. The hospital has 350 physicians on active staff, 92 full-time RNs and 393 full-time employees. Midlands Hospital provides a full range of services, including emergency, maternity, orthopedic, cardiovascular, surgical, sleep disorders, and diagnostic services.

### ASSISTED LIVING CENTERS

#### **Silver Ridge Village**

Silver Ridge Village is located at 20332 Hackberry Drive in Gretna and provides seniors with various levels of assisted living facilities. Every resident lives in their own apartment and has access to a number of different services to provide for all their daily needs. There is a registered nurse on staff and 24 hour certified staffing for the health and safety of the residents.

#### **Gretna Community Care Center**

Located at 700 South Highway 6 in Gretna the Gretna Community Care Center offers care for individuals recovering from surgery or illness, require concentrated rehabilitation, or need ongoing long-term nursing care in one of the 63 skilled nursing facility beds.

## ENERGY ELEMENT

### INTRODUCTION

Energy plays a crucial role in nearly every aspect of our lives. It is used to grow our food, to move us from place to place, to light our homes, and to make the products we buy. The vast majority of our energy is currently supplied by fossil fuels, which will inevitably run out. Federal regulations are tightening emission rules for power plants, thus increasing the cost of using fossil fuels. By planning for energy, Gretna will save money, have a more resilient economy, help the environment, and be better prepared for the future.

### ACKNOWLEDGEMENTS

This energy element was created using data and graphics from the following:

The Nebraska Energy Office  
 The City of Gretna  
 Sarpy County Energy Element  
 National Renewable Energy Laboratories (NREL)  
 U.S. Department of Energy (DOE)  
 Omaha Public Power District (OPPD)  
 U.S. Energy Information Administration (EIA)  
 Eastern Interconnection States' Planning Council (EISPC)  
 AWS Truepower  
 American Wind Energy Association  
 U.S. Environmental Protection Agency  
 Social Explorer  
 American Community Survey

### NEBRASKA ENERGY POLICY OVERVIEW

#### **Nebraska Legislation LB 997**

In 2010, Nebraska Legislators passed LB 997 requiring all municipalities and counties, with the exception of villages, to adopt an energy element into their comprehensive plan. The following energy element is included within Gretna's Comprehensive Plan in order to fulfill the requirement of LB 997. Energy elements are required to have three components:

1. Energy infrastructure and energy use by sector
2. Utilization of renewable energy sources
3. Energy conservation measures that benefit the community

#### **Nebraska Energy Plan**

The 2011 Nebraska Energy Plan outlines 14 strategies for the State to consider in meeting the following objectives:

- Ensure access to affordable and reliable energy for Nebraskans to use responsibly
- Advance implementation and innovation of renewable energy in the State
- Reduce petroleum consumption in Nebraska's transportation sector

#### **Sarpy County Energy Element and Relationship to Gretna**

Sarpy County's Energy Element was adopted in 2012 and provided the vision, goals and strategies for the County's energy future. Although the Gretna Energy Element was created in-line with the Sarpy County Energy Element, and features similar goals, strategies, data, and graphics from that element, it is specific to the City of Gretna.

### **Energy Codes**

Under §§81-1608 to 81-1616, the State of Nebraska has adopted the International Energy Conservation Code as the Nebraska Energy Code. Any community or county may adopt and enforce the Nebraska Energy Code or an equivalent energy code. If a community or county does not adopt an energy code, the Nebraska Energy Office will enforce the Nebraska Energy Code in the jurisdiction.

The purpose of the Code, under §81-1608, is to ensure that newly built houses or buildings meet uniform energy efficiency standards. The statute finds that:

There is a need to adopt the International Energy Conservation Code in order (1) to ensure that a minimum energy efficiency standard is maintained throughout the State, (2) to harmonize and clarify energy building code statutory references, (3) to ensure compliance with the National Energy Policy Act of 1992, (4) to increase energy savings for all Nebraska consumers, especially low-income Nebraskans, (5) to reduce the cost of State programs that provide assistance to low-income Nebraskans, (6) to reduce the amount of money expended to import energy, (7) to reduce the growth of energy consumption, (8) to lessen the need for new power plants, and (9) to provide training for local code officials and residential and commercial builders who implement the International Energy Conservation Code.

The Code applies to all new buildings, as well as renovations of or additions to any existing buildings. Only those renovations that will cost more than 50 percent of the replacement cost of the building must comply with the Code. As of June 2014, the City of Gretna has adopted the 2009 International Energy Conservation Code. Updating to the latest energy code will result in energy savings for Gretna residents.

### **Nebraska Legislation LB 436 - Net Metering**

The Nebraska Legislature passed LB 436 that allows for net metering. The legislation permits citizens to generate their own energy as the credits of generated energy are purchased back from the local utility company. This procedure is found to be in the public interest because it encourages customer-owned renewable energy resources. On-site energy generation can stimulate the economic growth, encourage diversification of the energy resources used, and maintain the low-cost, reliable electric service for the State of Nebraska. By supplementing electric bill through “credits” for energy, the citizens of Gretna can save money and alleviate pressure on the utility grid.

According to the Omaha Public Power District (OPPD) website, they have offered net metering since 2009. OPPD will allow net metering for any consumer that has a qualified generator using methane, wind, solar, biomass, hydropower or geothermal energy with a total capacity of 25 kilowatts or less. OPPD had 44 qualified facilities with total generating capacity of 280 kilowatts as of December 31, 2013. The total estimated amount of energy produced by these customer-generators was 368,883 kilowatt-hours, and the net received from them was 4,436 kilowatt-hours in 2013.

### **Solar and Wind Easements and Local Option Rights Laws**

Nebraska’s solar and wind easement provisions allow property owners to create binding solar and wind easements in order to protect and maintain proper access to sunlight and wind. Counties and municipalities are allowed to develop zoning regulations, ordinances, or development plans that protect access to solar and wind energy resources. Local governing bodies may also grant zoning variances to solar and wind energy systems that would be restricted under existing regulations, so long as the variance is not substantially detrimental to the public good.

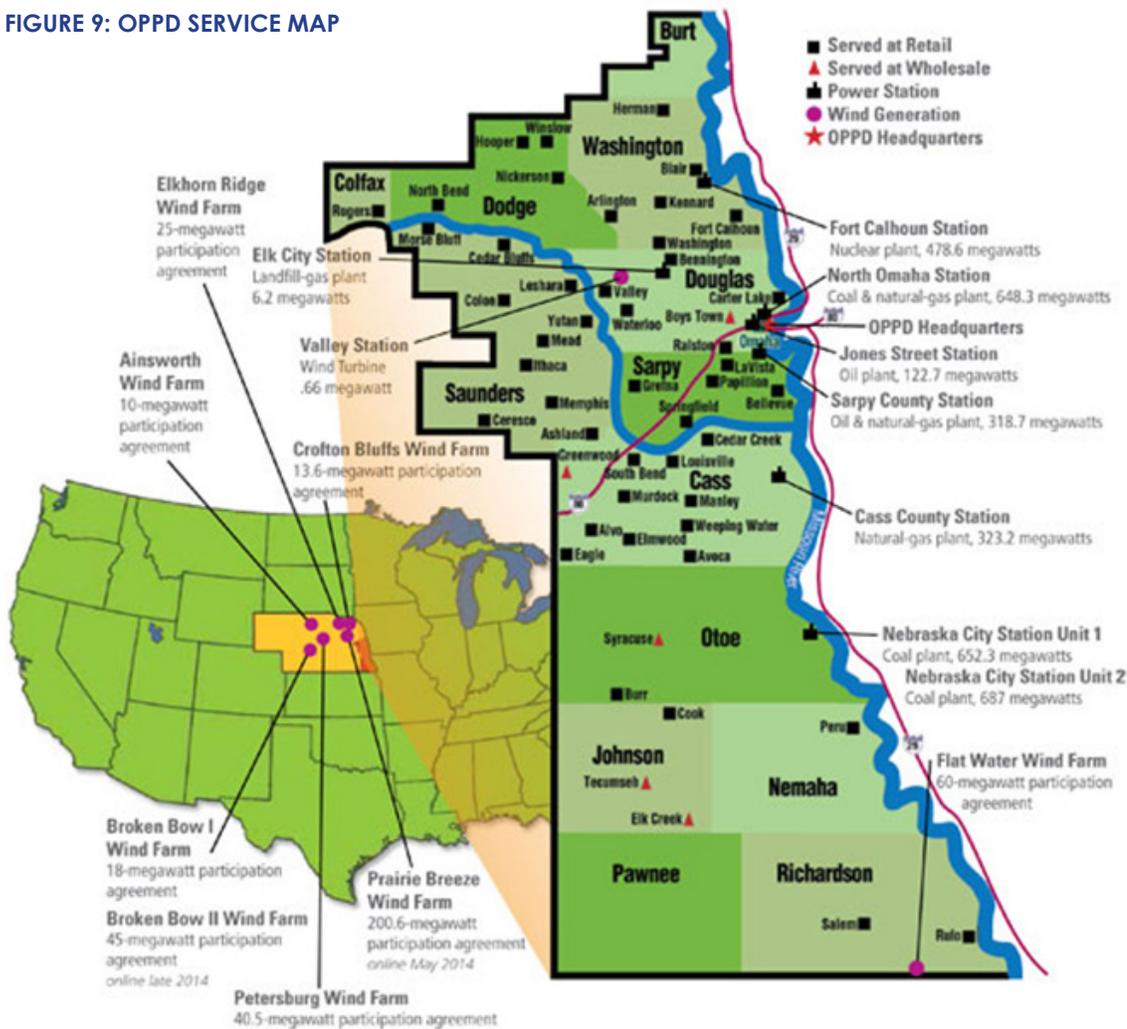
For summaries of additional programs, incentives and policies in Nebraska visit the Database of State Incentives for Renewables & Efficiency (DSIRE) website.

### LOCAL UTILITY PROVIDERS - GRETNA, NE

As seen in the OPPD Service Map (Figure 9), Gretna and the surrounding communities are serviced by OPPD. OPPD provides electricity for a 5,000 square mile area crossing 13 counties. OPPD has 15,567 miles of electric line and generating capacity of 3,237 megawatts. Black Hills Energy provides natural gas for the city.

The OPPD energy sources are represented as percentages in Figure 10. Fossil fuels (coal, natural gas, and oil) are the energy source for 72% of OPPD's electricity generation and results in emissions. Coal and natural gas will continue to be a large part of OPPD's generation mix because these energy sources are cheap, reliable, and abundant. Currently, 13.7% of OPPD's electricity is generated from renewable energy sources, most of which comes from wind.

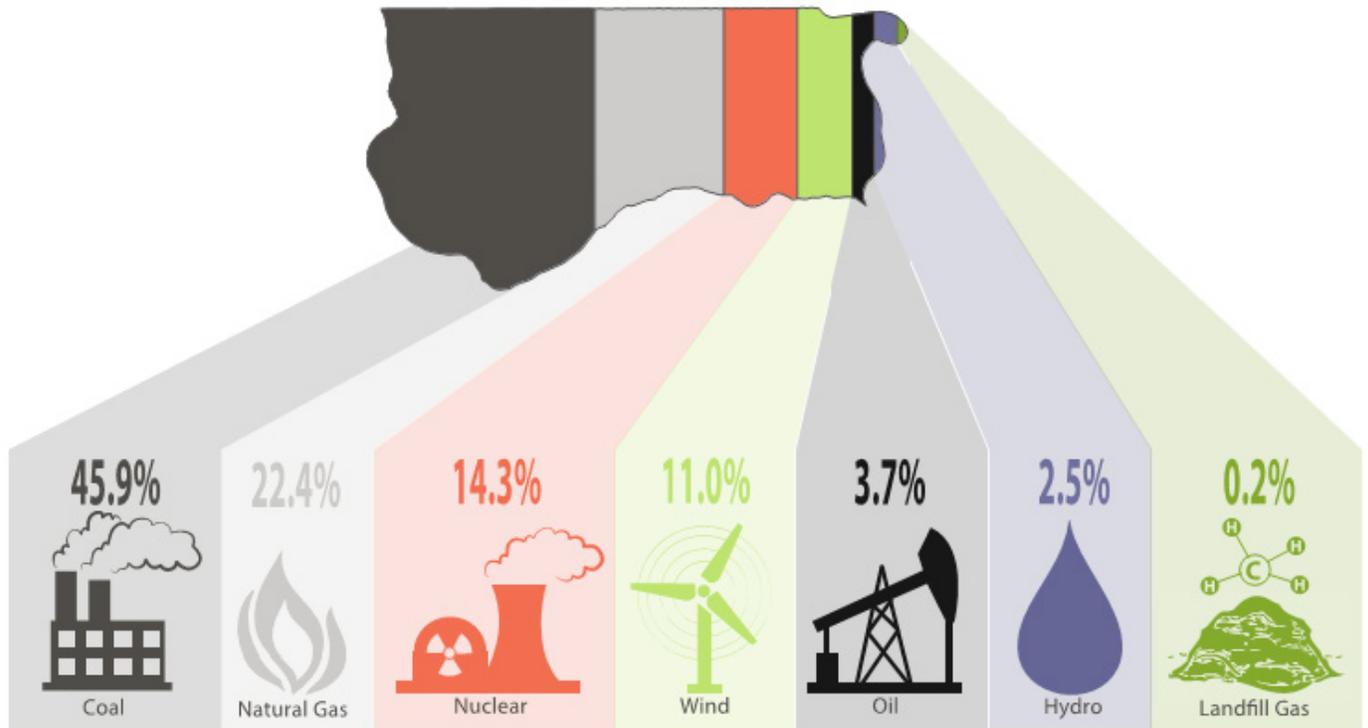
FIGURE 9: OPPD SERVICE MAP



Source: Image from OPPD website 'About US'

FIGURE 10: OPPD POWER COMPOSITION

## Energy Sources in OPPD's Electricity Mix



Source: Data from OPPD Annual Report

If OPPD plans on maintaining at least 33% of their portfolio in renewable energy beginning in 2018, they will be increasing renewable energy generation considerably. The most recent data indicates that renewable energy is responsible for 13.7% of their electricity generation mix. In order to meet that goal of 33% by 2018, OPPD will be aggressively investing in renewable energy and purchasing renewable energy elsewhere.

In 2014, OPPD created a plan to lower its carbon emissions in reaction to new proposed EPA regulations.

The OPPD board adopted a plan to:

- Retire units 1-3 of the North Omaha coal plant by 2016
- Retrofit units 4-5 of North Omaha in 2016
- Convert units 4-5 of North Omaha to natural gas by 2023
- Retrofit Nebraska City One by 2016
- Maintain at least 33% of their portfolio in renewable energy beginning in 2018
- Reduce demand by 300 MW through energy efficiency and demand side management program

This plan would reduce:

- Carbon dioxide emissions by 49%
- Mercury emissions by 85%
- NOx (nitric oxide and nitrogen dioxide) emissions by 74%
- SOx (sulfur oxide) emissions by 68%

## GRETNA ENERGY USAGE STATISTICS

As seen in Table 29, overall electricity consumption decreased between 2012 and 2013 in the residential and commercial sectors despite increasing the number of homes and businesses. The electricity used in the commercial and residential sectors is mainly for space heating and cooling, lighting and appliances. Energy strategies should focus on reducing the consumption for those needs.

**TABLE 29: GRETNA ELECTRICITY USAGE, 2012 AND 2013**

2012	SECTOR	kWh	PREMISE COUNT
	Residential	20,813,264	1,764
	Commercial	25,591,879	447
	Street Lighting	399,953	14
2013	SECTOR	kWh	PREMISE COUNT
	Residential	20,470,419	1,770
	Commercial	24,883,430	523
	Street Lighting	424,191	18

*Source: Data provided by OPPD*

Consumption data for municipal operations was not available for Gretna. However, the City of Gretna will strive to set an example for its citizens by reducing its energy consumption. Gretna can reduce its energy consumption by following the goals and strategies described later in this energy element.

Electricity consumption in Gretna is expected to increase due to rapid population growth. There will be many new homes and buildings built within Gretna in the future. This will increase overall energy consumption, but is also a great opportunity to reduce energy use per capita. Gretna can control development patterns and ensure new buildings are built within energy efficient building codes which will significantly reduce per capita electricity consumption.

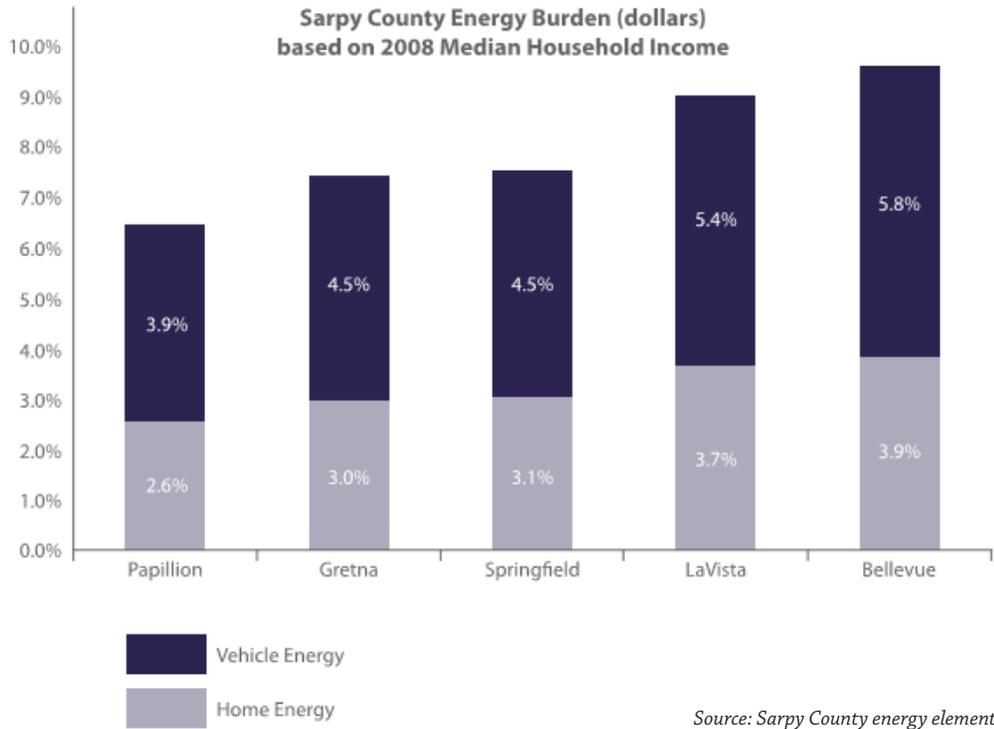
As shown in Figure 11, the Sarpy County energy element calculated that Gretna households use 7.5% of their income on energy. Conserving energy and possibly generating their own would result in Gretna residents spending less money on energy and more into the local economy.

Residents of Gretna can earn money and conserve energy by enrolling in OPPD’s Air Conditioning Management Program. The air conditioning cycling program reduces the demand during high usage times. OPPD will attach a free outdoor device to the home by the AC unit or heat pump, and credit the resident bill each year you participate in the program.

According to the American Community Survey and Social Explorer, Figure 12 illustrates the average commute time for the Gretna area is 24 minutes. Gretna residents are spending a lot of time, money and energy commuting to and from work every day. Significant energy savings can be achieved by creating strategies that reduce the average commute time.

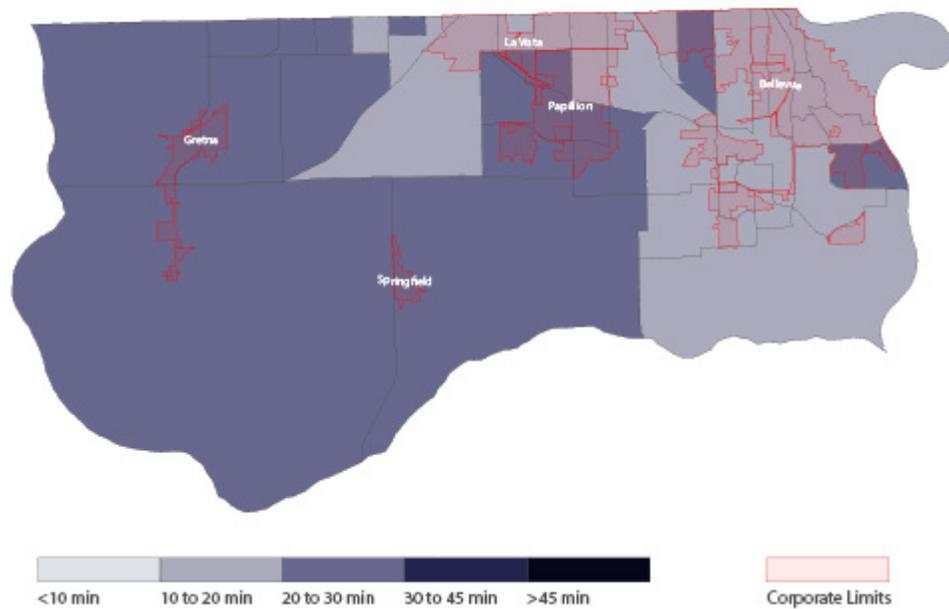
The Corporate Average Fuel Economy standards will nearly double vehicle fuel economy by 2025 to 54.5 miles per gallon. Without any action, this will lower fuel consumption per capita in Gretna. Finding other strategies to lower the commute time will result in further energy conservation.

**FIGURE 11: SARPY COUNTY ENERGY BURDEN**



**FIGURE 12: SARPY COUNTY COMMUTE TIME TO WORK BY CENSUS TRACT**

**Sarpy County Commute Time  
by Census Tract in 2012**



*Source: American Community Survey, 2008 - 2012 Five-year Estimates*

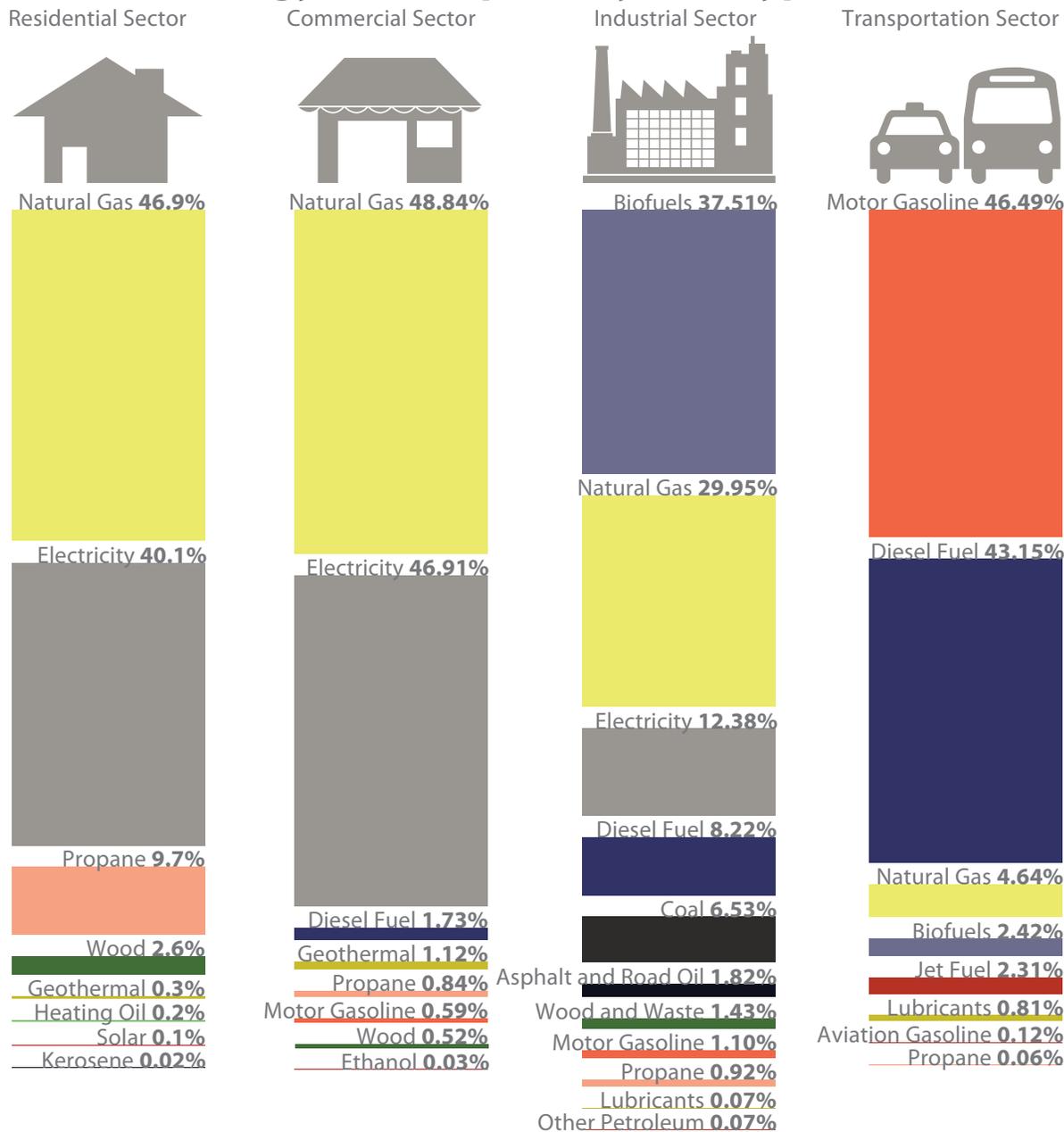
## NEBRASKA ENERGY STATISTICS

Figure 13 shows the net energy consumption by fuel type in the residential, commercial, industrial and transportation sectors. The majority of the energy spent in the residential and commercial sectors is in the form of natural gas or electricity for heating, cooling, and lighting purposes. The industrial sector relies on biofuels for 37.51% of its energy consumption.

As shown in Figure 14, Nebraskans rely on fossil fuels for an overwhelming majority of their energy needs. Energy consumption continues to increase from year to year with Nebraskans consuming 871 trillion Btus in 2011. Natural gas and renewable energy consumption are expected to increase in the future as these sources become more economical.

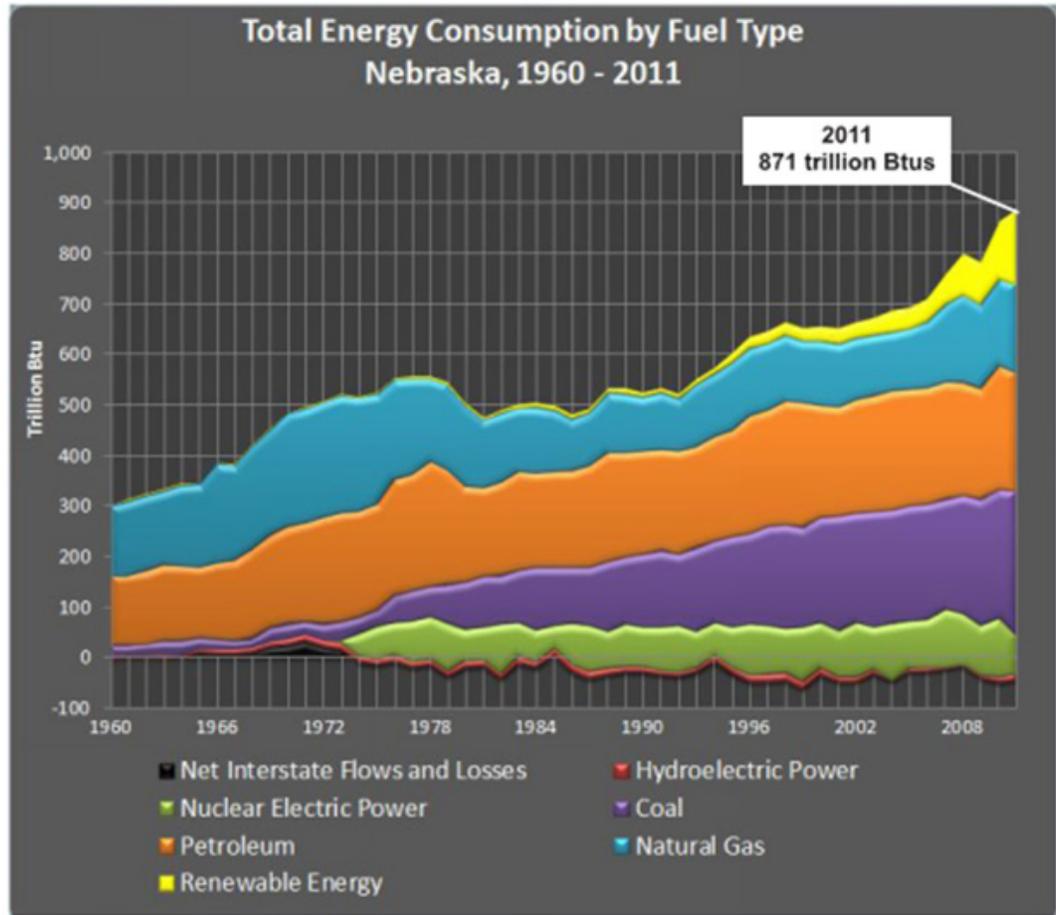
FIGURE 13: NEBRASKA ENERGY CONSUMPTION

### 2011 Net Energy Consumption by Fuel Type in Nebraska



Source: Data from Nebraska Energy Office

FIGURE 14: NEBRASKA ENERGY CONSUMPTION TRENDS



Sources: State Energy Data Report. Energy Information Administration, Washington, DC. Nebraska Energy Office, Lincoln, NE.

Table 30 and 31 show how much energy Nebraska consumed and how much money Nebraska spent on energy in 2012. Total energy consumption decreased by ten trillion Btu from 2011 to 2012, or 1%. Even though transportation consumption was just under 23% of the total in 2012, Nebraska spent more money on transportation than the combined energy uses of residential, commercial, and industrial. Gretna should look into strategies that will lower the consumption and cost of transportation because of the large expenditures and average commute time for the City.

TABLE 30: NEBRASKA ENERGY CONSUMPTION IN TRILLION BTU, 2012

Residential	Commercial	Industrial	Transportation	Total
147.0	131.9	384.8	196.9	860.6
17.1%	15.3%	44.7%	22.9%	100%

Source: Energy Information Administration (EIA)

TABLE 31: NEBRASKA ENERGY EXPENDITURES IN MILLION DOLLARS, 2012

Residential	Commercial	Industrial	Transportation	Total
1,390.3	990.5	2,289.5	5,423.0	10,093.3
13.8%	9.8%	22.7%	53.7%	100%

Source: Energy Information Administration (EIA)

## RENEWABLE ENERGY SOURCES

Nebraska is the only State in the U.S. that is 100% public power. Since they are not seeking profits, public power districts have been able to maintain some of the lowest electricity prices in the nation. The low cost of energy is one of the reasons that Nebraska has not fully taken advantage of its renewable energy potential. Unlike places such as California, where electricity prices are high, renewable energy systems have historically not been economical for Nebraska.

With new proposed federal regulations, power plants would have to lower their carbon emissions by 30% by 2030. These regulations would mean heavy carbon emitters, such as coal power plants, will require retrofits or improvements in order to meet that goal. Since a large amount of the electrical energy consumed in Gretna comes from coal, this will most likely affect the price of electricity coming from these power plants. Therefore, it would be in Gretna’s best economic interest to decrease per capita energy consumption and increase the amount of renewable energy produced in Gretna. Below is a summary of potential renewable energy options for Gretna. Although the focus of this section is on wind, hydro, solar, geothermal and biomass, all sources of renewable energy should be considered by Gretna in the future for their feasibility.

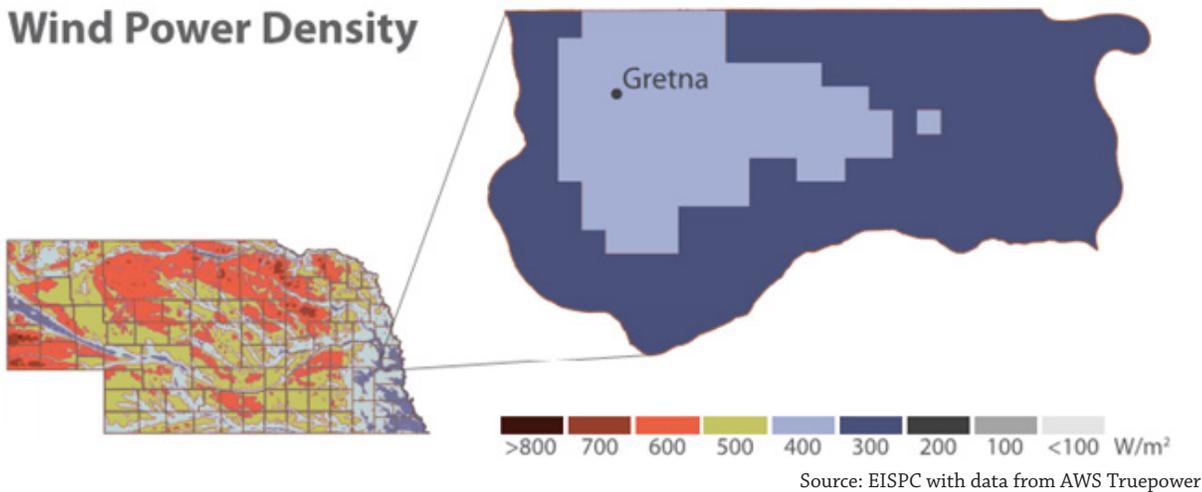
### Wind

According to the American Wind Energy Association, Nebraska has one of the best wind resources in the United States, 92% of Nebraska has the adequate wind speeds for a utility-scale wind farm. Nebraska ranks 3rd in the U.S. in gigawatt hour (GWh) wind generation potential, but has been slow in utilizing this resource compared to other states. Nebraska currently ranks 23rd in total MW installed with 534 MW. According to the National Renewable Energy Laboratory, Nebraska’s wind potential at 80 meters hub height is 917,999 MW. Wind Power is capable of meeting more than 118 times the state’s current electricity needs. Figure 15 represents the gross estimated annual average wind power density for Nebraska and Sarpy County. This data indicates how much energy is available for conversion by a wind turbine at a particular location. As the following figure indicates, Gretna and the rest of Sarpy County has some of the lowest wind energy potential in the state. Despite this data, areas around Gretna may be suitable for a wind energy operation. Electricity produced through wind power will be most cost effective on the utility/commercial scale. Small-scale wind systems for homes and businesses may not be as cost effective, but they should not be discouraged.

**Omaha Public Power District (OPPD) Wind Capacities**

The Omaha Public Power District has been aggressive by adding wind capacity in order to diversify their energy sources. According to the 2013 Annual Report, their wind capacity has been improved in each of the last few years. 124.5 MW was added in 2011, 122MW was added in 2012, and 74.8 MW was added in 2013 for a total of 321.3 MW of wind capacity since 2011.

FIGURE 15: NEBRASKA AND SARPY WIND POWER



### Hydro Power

The electricity from hydropower consumed in Nebraska comes from the 11 dams in or on the border of the State and purchases from Western Area Power Administration. The amount of electricity produced from hydropower is relatively the same every year, unless affected by drought or an offline facility. According to the Nebraska Energy Office, studies conducted in 1981 and 1997 concluded that nearly all of the potential hydro resources had been developed. Even under the most optimistic scenarios, less than 150 MW of additional power could be produced from existing or new hydro resources. There are indications that micro-hydroelectric dams would be feasible in a number of settings across the state, however not likely in Gretna.

### Biomass

Gretna may consider the feasibility of renewable energy generation from biomass because of the agriculture located in proximity of the City. Biomass (biodiesel, ethanol, landfill gas, methane, wood and wood waste) accounted for 81.7% of all renewable energy generated in Nebraska in 2011.

*Direct-fired system* - Most biomass plants that generate electricity use direct-fired systems. Simply, these plants burn biomass feedstock directly to produce steam. This steam turns a turbine, which turns a generator that converts the power into electricity. The feedstock for direct systems can be a number of things: wood and wood waste, agricultural residues, municipal solid waste, industrial waste and many others.

*Biodiesel* - The two Nebraska commercial scale plants in Arlington and Scribner have the estimated production capacity of 5.4 million gallons per year, but both have recently closed due to the price of soybeans used for feedstock.

*Ethanol* - Ethanol produced from corn and grain sorghum is a growing energy resource in Nebraska. According to the Renewable Fuels Association, Nebraska has the second largest ethanol production capacity in the nation and the second largest current operating production in the nation. Approximately 14% of the nation's ethanol capacity is in Nebraska's 27 ethanol plants. The closest local Nebraska ethanol plant is in Blair.

The Nebraska Ethanol Board estimates that 40% of the State's corn crop and 75% of the grain sorghum crop are used in the production of ethanol.

Nebraska's ethanol production:

- 91% goes to U.S. domestic markets
- 5% exported to other countries
- 4% used by Nebraskans

Ethanol consumption is mainly in the form of blended gasoline. Ethanol production and consumption is expected to continue to increase as national legislation continues to affect State policies. The Renewable Fuel Standard, established in 2005 as part of the Energy Policy Act, requires a minimum of 36 billion gallons of renewable fuel to be integrated in the nation's gasoline supply by 2022. In 2013, 87 octane fuel without ethanol began to be phased out and replaced with an ethanol-blended 87 octane gas.

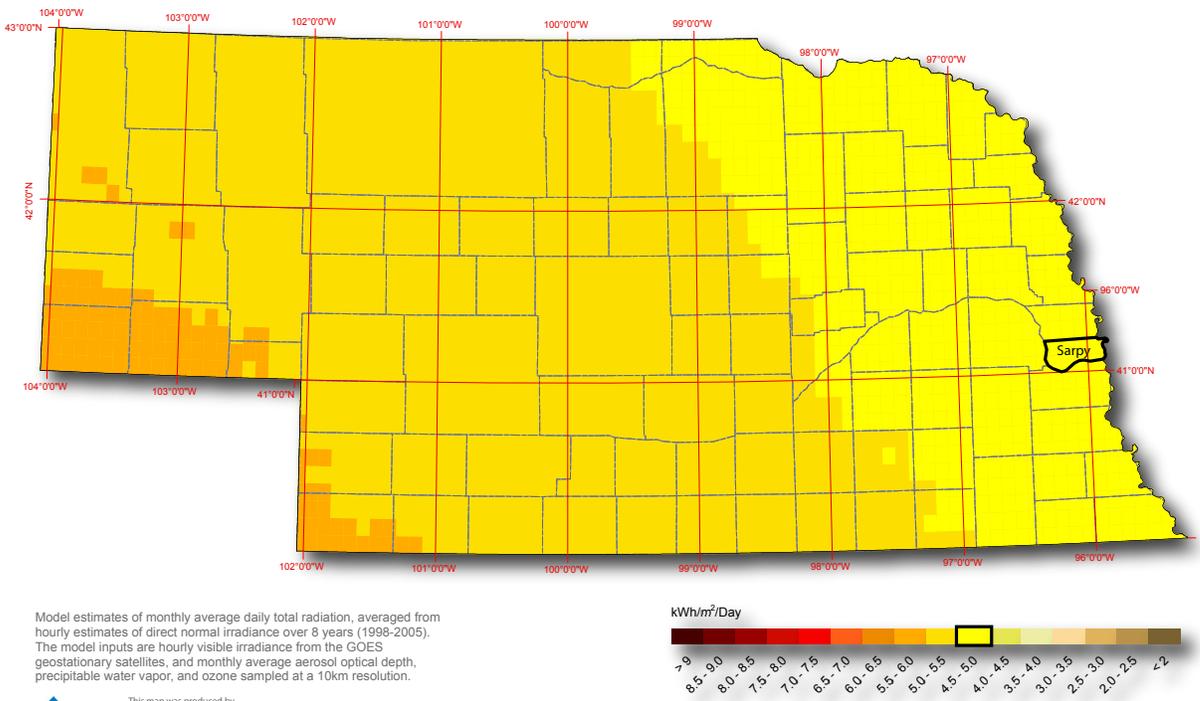
*Biogas* - Biogas is a product of the decomposition of manure, via anaerobic digestion, and typically made of 60% methane and 40% carbon dioxide. Biogas can be used to generate electricity, as boiler fuel for space or water heating, upgraded to natural gas pipeline quality, or other uses. After the production of biogas, the remaining effluent is low in odor and rich in nutrients. Byproducts of biogas production include: fertilizer, livestock bedding, soil amendments or biodegradable planting pots. Methane gas can also be extracted from wastewater treatment plants using a similar process. As the City's population continues to grow, Gretna should consider the feasibility of extracting methane from current wastewater treatment facilities.

**Solar Power**

According to the National Renewable Energy Laboratory, Nebraska is ranked 13th in solar energy potential. Currently, solar technologies are marginally used in Nebraska because it has historically been difficult for solar technologies to compete with the State’s low electric rates. As of 2013, Nebraska has solar panel facilities at three sites: NPPD Norfolk Operations Center, OPPD Elkhorn Service Center, and LES Hyde Observatory. As seen in Figure 16, Gretna has an average solar radiation of 4.5-5 kWh per square meter per day. Gretna has less solar resources than central and western parts of the State but is about average for the nation.

According to the Department of Energy, the average hardware cost of solar panels has dropped more than 60%. As the cost of solar panels continues to decrease, solar can be utilized at individual homes or businesses to help supplement electrical needs. Many utilities have incentives to help with the cost of solar, but additional steps should be taken to increase the amount of solar energy generated in Gretna.

**FIGURE 16: NEBRASKA SOLAR POTENTIAL**  
**Global Solar Radiation at Latitude Tilt - Annual**



Model estimates of monthly average daily total radiation, averaged from hourly estimates of direct normal irradiance over 8 years (1998-2005). The model inputs are hourly visible irradiance from the GOES geostationary satellites, and monthly average aerosol optical depth, precipitable water vapor, and ozone sampled at a 10km resolution.

 This map was produced by the National Renewable Energy Laboratory for the U.S. Department of Energy. September 25, 2007

*Passive solar* - Passive solar design takes advantage of a site, climate, and materials to minimize energy use. A well-designed passive solar home first reduces energy use for heating and cooling through energy-efficiency strategies and then meets the reduced need in whole or part with solar energy. In simple terms, passive solar homes collect heat as the sun shines through south-facing windows and retain it in materials that store heat, known as thermal mass.

### **Geothermal**

There are three geothermal resource applications: electricity production, direct use, and heat pumps.

*Electricity Production-* Generating electricity from geothermal resources requires drilling a well into an underground reservoir of water that can be as hot as 700 degrees Fahrenheit. The trapped steam is brought to the surface to turn a turbine that produces electricity. This resource can also be utilized on the surface of hot springs or geysers.

*Direct Use-* direct use systems have a well drilled into a geothermal reservoir to provide a steady stream of hot water. Water is brought up through the well and a system of piping, heat exchanger, and controls deliver the heat directly for its intended use. A disposal system either injects the cooled water underground or disposes of it on the surface.

Geothermal hot water has many applications. Current uses that require heat include: buildings, raising plants in greenhouses, drying crops, heating water at fish farms, and several industrial processes.

*Heat Pumps-* The type of geothermal application that is most practical and economical for the residents of Gretna is the use of geothermal heat pumps. Geothermal heat pumps are slowly becoming a popular method of heating and cooling buildings. Heat pumps use much less energy than traditional heating and cooling systems. This translates into energy and financial savings while reducing air pollution. There are many state and utility level incentives to help with the initial cost of geothermal energy.

There are two different types of heat pumps: closed loop systems and open loop systems also known as “pump and dump.” Closed loop systems move fluids through continuous pipeline loops that are buried underground at depths where the temperature does not fluctuate much. Heat picked up by the circulating fluid is delivered to the building through a traditional duct system. Geothermal heat pumps discharge waste heat into the ground in the summer months and extract heat from the ground in the winter months.

Open loop systems require an ample source of ground water. An open loop system pumps water directly from a ground water source into the building for heating and cooling purposes. The used water is either deposited on the surface or a water source such as a pond or river. Open loop systems have environmental impacts due to introducing higher temperatures and minerals into the water sources. Open loop systems may also deplete a neighbor’s well source if there is not enough groundwater. To protect the water sources which feed the City’s water system, open loop systems are prohibited in the City and its Extraterritorial Jurisdiction.

### **Education**

Gretna will not be able to achieve its energy goals without the help of its citizens. Gretna should educate the public on the benefits of energy efficiency and the most feasible renewable energy systems. In the following subsections there are resources provided that Gretna can use to raise awareness regarding energy efficiency and renewable energy systems.

### **Recycling and Composting**

Recycling and composting preserves energy by reducing the energy needed to extract raw materials. These practices also reduce the amount of solid waste, which reduces what is dumped in the landfill. Currently, recycling in Gretna is provided by private sanitation companies. These companies offer recycling as a part of their regular trash service. This is usually in the form of single stream recycling; the customer puts all acceptable recycling materials into a container much like their trash container, it is picked up by the company and is sorted later.

Composting in Gretna is also provided by the private sector. Customers can purchase an additional service or they can bring their materials to a composting site themselves. Residents also have the option of composting themselves in their backyard if they have the right materials. Recycling and composting should be encouraged within Gretna.

### **Local Food**

Food takes energy to grow, harvest, process and transport. Conditions such as the distance from where the food is grown to our table affect how much energy is used to produce our food. Supporting locally grown food reduces the energy needed for food production.

### **Goals and Strategies**

The following are some of the Sarpy County Energy Element goals and strategies suggested for Gretna.

- I. Reduce energy use per capita in Gretna
  - A. Reduce Vehicle Miles Traveled (VMTs)
    1. Increase use of multi-use trails, walking, and bicycling as alternative modes of transportation
      - a. Plan trails to connect neighborhoods and provide access to commercial areas and community facilities
      - b. Connect neighborhoods
    2. Develop realistic information regarding the impacts of new development on the cost of providing public infrastructure and services
    3. Develop and support policies that align with Gretna's Energy Element
    4. Encourage infill development
    5. Develop and implement a ride-share program for the commuting population
  - B. Ensure efficient utilization of land resources
    1. Encourage new development adjacent to existing development
    2. Interconnect new development for active and vehicular transportation options
    3. Establish development guidelines or codes so that sites and site designs are friendly to active transportation modes
    4. Increase density
    5. Promote mixed use development

6. Develop employment centers within Gretna
    - a. Reduce travel time outside of community for basic services through community-based land use plans
    - b. Utilize all available tools and programs (tax incentives, utility rebate programs) and balance jobs/housing to promote economic development
    - c. Encourage and plan for diverse housing options
  7. Encourage development of charging stations and other infrastructure for alternative fuels
- II. Increase the amount of energy generated by renewable energy systems
    - A. Inform citizens about the most practical renewable energy applications for Gretna
    - B. Examine and possibly remove unintended barriers for appropriate small-scale distributed energy generation
    - C. Review, update, and improve the City's existing regulations and policies regarding private pursuits of renewable energy generation
    - D. Evaluate the feasibility of producing energy from future or expanded publicly owned and operated wastewater treatment facilities.
  - III. Increase the amount of locally produced food that is consumed in Gretna
    - A. Support best practices for "backyard" agriculture
      1. Review existing codes regarding composting
      2. Support local food production
        - a. Support markets for local food (e.g., farmers' markets)
        - b. Leave open space in new developments for urban agriculture where appropriate.
      3. Education
        - a. Encourage community education and dialog regarding locally produced food
  - IV. Increase the amount of buildings built to LEED or equivalent standards within Gretna
    - A. Educate homeowners regarding practical energy efficiency measures, including the Energy Star system
    - B. Educate City staff on latest and progressive energy codes and systems
    - C. Support and encourage meeting current LEED standards for all new buildings and renovations in Gretna
    - D. Partner with utility companies to enhance Gretna's efforts to understand:
      1. Energy use patterns
      2. Time of use rates
      3. Incentive based rates
      4. Energy efficiency incentives
      5. Benefits of participation in utilities demand response programs
    - E. Promote best practices in energy efficient building programs
      1. Encourage residential and commercial energy upgrades
    - F. Encourage energy conservation through effective siting of buildings and landscaping

- G. Encourage increased use of green roofing systems
  - H. Promote integration of renewable energy sources into buildings
  - I. Provide information regarding available renewable energy generation
    - 1. Research incentives for best practices
- V. Reduce energy consumption within the City of Gretna's operations
- A. Incorporate energy efficiency improvements into buildings and building systems
    - 1. Conduct energy efficiency upgrades to building HVAC systems
    - 2. Automate building systems for highest efficiency and comfort settings
    - 3. Purchase or implement a system for automating building system maintenance
    - 4. Manage peak demand such as with air conditioner cycling program
    - 5. Ensure building temperature set points are within an established range that supports comfort and efficiency
    - 6. Replace indoor lighting in City buildings with energy efficient lighting
    - 7. De-lamp buildings where appropriate by removing one or more lamps from multiple-lamp fixtures
    - 8. Consider installing solar water heaters in City facilities
    - 9. Educate employees regarding convenience items that draw "phantom" loads while plugged in and not in use (e.g., space heaters, phone chargers, VCRs, TVs, coffee pots, microwaves and microfridges)
    - 10. Sleep or shut down computers every night and on the weekends
    - 11. Use EPA's ENERGY STAR program to benchmark and track building energy performance
  - B. Provide feedback to City employees on energy consumption
    - 1. Conduct building energy audits on priority City buildings to identify energy retrofit and improvement opportunities
  - C. Demonstrate a commitment to energy efficiency buildings by implementing progressively more efficient building pilot projects
  - D. Improve Gretna's renewable energy portfolio through an increase of on-site renewable energy application in appropriate City facilities and projects
  - E. Research revolving loan fund opportunities to finance energy efficiency improvements
  - F. Improve efficiency and reduce use of vehicle fleet
    - 1. Provide training to Gretna employees on efficient driving techniques
    - 2. Implement a no-idling policy and other policies to save fuel
    - 3. Create a purchasing policy to infuse more efficient vehicles into the fleet over time
    - 4. Develop a fleet maintenance/management education program
    - 5. Pilot a program using biofuels in existing fleet vehicles
    - 6. Encourage more telecommuting, trip-chaining, and trip aggregation
      - a. Develop and encourage the use of video and tele-conferencing infrastructure to reduce vehicle miles travelled for meetings

G. Educate citizens regarding energy element

1. Implement education, outreach and citizen engagement strategies to prepare staff, developers, and community for City actions
  - a. Establish a webpage where Gretna can inform citizens of its energy saving and energy efficiency efforts both prior to and in accordance with the energy element.
2. Raise Gretna's residents' awareness of the wise use of energy.
3. Recognize innovative projects that support and advance the goals and strategies of the energy element.
4. Leverage recycling education to increase the amount of recycling in Gretna and consequently reduce energy used to process raw materials and land used for landfill.

# Existing Land Use

- Existing Land Use Categories
- Existing Land Use Analysis



# Existing Land Use

This section of the Plan explains the current development patterns and land use types found in the Gretna planning area. In order for a community to plan for future land uses and land use changes, knowledge of existing land uses must be established. The purpose of this section of the Plan is to establish an inventory and evaluation of the existing land uses found within the planning area. Existing land uses are defined by how a specific parcel of land is being utilized, and does not take into account future land use or land ownership.

Land use categories that will be used later to plan for future development areas are general statements about how the underlying property is being used, and include broad labels such as residential, commercial, and industrial, as well as an identification of vacant or open spaces being used for cropland, recreational areas, and any under- or non-developed land. However, the land uses used here to identify the existing conditions will have more detailed statements in order to fully explain the variety of uses currently found. This land use inventory was completed in 2008.

Land uses and properties do not have to be arranged in a 1:1 ratio with one land use per parcel. Uses are often mingled within a development, and can be stacked on each other, such as in a Downtown building that is used for residential uses on upper floors and commercial uses on the ground floor. The number and type of land uses found in a vibrant community is constantly changing to meet the needs and desires of residents, which can produce a number of impacts that either benefit or detract from the overall sense of community and quality of life. Because of this, the success and sustainability of a community is directly influenced by the manner in which available resources are utilized given the constraints the City faces during the course of the planning period.

Typically, older Midwest communities are characterized by a fixed pattern of land use influenced by the consistency of their rural settings and relaxed pace. However, Gretna is experiencing increasing levels of growth and development pressures from Omaha, and has seen its surroundings transform from the more common rural setting found throughout Nebraska to an urbanizing extension of the Omaha Metropolitan Area. The proximity to I-80 and Highway 370, as well as its location along Highway 6/31, provide Gretna with many more opportunities than would be found in a typical town of the same size. The opportunities that result from such external forces create impacts upon the community and its residents. Development pressures drastically affect the land use around the Gretna area and will significantly determine how and where Gretna grows in the future.

## EXISTING LAND USE CATEGORIES

Land uses are generally best described in terms of specific categories that provide broad descriptions into which numerous businesses, institutions, and structures are grouped. Gretna's existing land use categories are more specific to allow for a more detailed evaluation of each use. For the purposes of PlanGretna, the following land use classifications are used:

- **Agriculture Open** – A parcel of land that is not intended for development and is currently used for low intensity agriculture uses, such as pasturing, or contains open spaces such as woodlands or flood plain.
- **Single-Family Residential** – A parcel of land with a residential structure occupied by one family, such as a traditional home on its own lot, surrounded by yards on all sides.
- **Multi-Family Residential** – A parcel of land containing a structure being utilized by three or more families within a same structure.

- **Mobile Home Residential** – A parcel of land containing a factory-built, single-family structure. These uses are Single-Family Residential in nature, but identified separately.
- **Seasonal Residential** – A parcel of land containing a Single- or Multiple-Family Residential structure, which is generally available for short-term rental or lease, usually on a seasonal basis. This land use is distinct from a typical rental property by the seasonal nature of its usage.
- **Commercial, General** – A parcel of land containing a commercial use which may sell a good, but mostly provides a service, such as automotive repair, hair salon, and includes the Downtown.
- **Commercial, Office** – A parcel of land containing a commercial use characterized by an individual or multiple office buildings, such as insurance sales or corporate headquarters.
- **Industrial** – A parcel of land containing a commercial use involved in manufacturing or packing, storage, or assembly of products, which does not have a major external effect on surrounding properties or uses.
- **Public/Quasi-Public** – A parcel of land owned or maintained by a federal, state, or a local governmental entity and open for enjoyment by public, or a parcel of land containing a use that is generally under the control of a private, religious, or non-profit entity, that provides social benefit to the community as a whole.
- **Parks and Recreation** – A parcel of land containing public or private land available for recreational, educational, cultural, or aesthetic use.
- **Vacant or Developing Residential** – A parcel of land that is currently undeveloped and not proposed for development, or may be subdivided and under preparations for residential development. This land is generally found to be open and minimally maintained.

## EXISTING LAND USE ANALYSIS

An evaluation of land uses was tabulated for the City of Gretna showing the quantity of uses found within the corporate limits as well as within the entire planning jurisdiction. The data was arranged using total acres of each classification, and is displayed in Table 32. The table has been divided into four columns of information including the total acres, percent of developed area, percentage of total area, and acres per 100 persons based upon the 2006 US Census population estimate of 5,970 persons. The persons per acre establishes a baseline from which land use numbers can be compared between communities, as well as to project future land use needs due to projected population changes.

Not surprisingly, the majority of developed land within Gretna is used for single family residential purposes. In 2008, nearly one-half of all developed property in Gretna was used for single-family dwellings. In terms of total acres, single-family uses in 2008 accounted for 351 acres. The remaining components of residential uses represented much smaller portions of the land use picture. Residential uses with two or more units accounted for 10 percent of all developed land within Gretna in 2008. Finally, manufactured home developments and seasonal residential uses were not found with Gretna at this time. However, there are a number of contiguous residential developments surrounding Gretna which give the community the potential to alter the residential land use statistics if these areas were to be annexed in the future.

Commercial land uses were categorized as general or office. Overall, commercial uses in 2008 covered just over 97 acres, or nearly 13 percent of all developed land. By far, the largest group of commercial uses was general, which accounted for about 90 percent of all commercial uses in 2008. General commercial uses occupied nearly 89

acres of land, while office commercial uses occupied slightly more than eight acres. Total acres put to industrial uses exceeded those put to commercial use by about one-third, and accounted for 17 percent of all developed land in 2008.

Public and quasi-public land uses accounted for nearly 20 percent of all developed land in 2008, second only to single-family residential. Parks and recreation acres accounted for the third smallest acreage, covering just over 23 acres in 2008.

Transportation uses, which include rights-of-way, railroads, and roadway systems, accounted for one-fifth of undeveloped land and 13 percent of total acreage in 2008. Undeveloped land, including transportation, agricultural uses, and vacant property accounted for 62 percent of all land within Gretna, compared to 38 percent for developed land.

Figure 17 shows the existing land uses within the corporate limits of Gretna.

**TABLE 32: EXISTING LAND USE WITHIN GRETNA CORPORATE BOUNDARY, 2008**

Use	Acres w/in Gretna	% Status Area	% Total Area	Acres per 100 persons
<b>RESIDENTIAL</b>	<b>387.4</b>	<b>48.2%</b>	<b>26.0%</b>	<b>6.5</b>
Single-Family	375.1	46.7%	25.2%	6.3
Multiple-Family	12.3	1.5%	0.8%	0.2
Manufactured Housing	0.0	0.0%	0.0%	0.0
Seasonal Residential	0.0	0.0%	0.0%	0.0
<b>COMMERCIAL</b>	<b>101.9</b>	<b>12.7%</b>	<b>6.8%</b>	<b>1.7</b>
General	93.8	11.7%	6.3%	1.6
Office	8.1	1.0%	0.5%	0.1
<b>INDUSTRIAL</b>	<b>127.1</b>	<b>15.8%</b>	<b>8.5%</b>	<b>2.1</b>
<b>PUBLIC/QUASI-PUBLIC</b>	<b>147.3</b>	<b>18.4%</b>	<b>9.9%</b>	<b>2.5</b>
<b>PARKS/RECREATION</b>	<b>39.2</b>	<b>4.9%</b>	<b>2.6%</b>	<b>0.7</b>
<b>DEVELOPED LAND</b>	<b>803.0</b>	<b>100.0%</b>	<b>53.9%</b>	<b>13.4</b>
VACANT/DEVELOPING RESIDENTIAL	16.0	2.3%	1.1%	0.3
AGRICULTURE	381.6	55.6%	25.6%	6.4
TRANSPORTATION	288.2	42.0%	19.4%	4.8
<b>UNDEVELOPED LAND</b>	<b>685.8</b>	<b>100.0%</b>	<b>46.1%</b>	<b>11.5</b>
<b>Total Area</b>	<b>1,488.8</b>		<b>100.0%</b>	<b>37.8</b>

Source: 2008 Comprehensive Development Plan, JEO Consulting Group, Inc.  
 Note: Acres per 100 is based upon the 2006 US Census population estimate of 5,970

Gretna currently exercises its statutory authority to enforce planning jurisdiction within one mile of the corporate boundary. An evaluation of land uses within this extraterritorial jurisdiction (ETJ) is important for future development and planning activities. The land uses found outside of the corporate limits (shown in Table 33) are mostly agriculture, agricultural residential, and single-family residential, especially to the south of the community. Due to development pressures from Omaha and within Gretna, as well as the proximity of major transportation routes, the percentage of residential uses found within the ETJ is higher than would be typical for a Midwestern community of this size. Figure 18 illustrates the different existing land uses within Gretna's ETJ.

In addition, all land uses that are found within Gretna are also found within the ETJ. This pattern is also influenced by the urban nature of the area, as well as the land use policies Gretna has held over time. The presence of all the same land use types in the ETJ will encourage greater development activity, which in turn can influence Gretna's ability to annex and grow at an increased rate over communities located in more rural settings. Figure 19 shows the existing land use outside the current ETJ and within the Gretna School District.

**TABLE 33: EXISTING LAND USE WITHIN GRETNA ETJ, 2008**

Use	Acres w/in Gretna	% Status Area	% Total Area	Acres per 100 persons
<b>RESIDENTIAL</b>	<b>2,028.7</b>	<b>78.8%</b>	<b>17.6%</b>	<b>34.0</b>
Single-Family	2,015.5	78.3%	17.4%	33.8
Multiple-Family	13.2	0.5%	0.1%	0.2
Manufactured Housing	0.0	0.0%	0.0%	0.0
Seasonal Residential	0.0	0.0%	0.0%	0.0
<b>COMMERCIAL</b>	<b>126.8</b>	<b>4.9%</b>	<b>1.1%</b>	<b>2.1</b>
General	115.4	4.5%	1.0%	1.9
Office	11.4	0.4%	0.1%	0.2
<b>INDUSTRIAL</b>	<b>192.1</b>	<b>7.5%</b>	<b>1.7%</b>	<b>3.2</b>
<b>PUBLIC/QUASI-PUBLIC</b>	<b>164.8</b>	<b>6.4%</b>	<b>1.4%</b>	<b>2.8</b>
<b>PARKS/RECREATION</b>	<b>61.6</b>	<b>2.4%</b>	<b>0.5%</b>	<b>1.0</b>
<b>DEVELOPED LAND</b>	<b>2,574.0</b>	<b>100.0%</b>	<b>22.3%</b>	<b>43.1</b>
VACANT/DEVELOPING RESIDENTIAL	291.4	3.2%	2.5%	4.9
<b>AGRICULTURE</b>	<b>7,693.1</b>	<b>85.7%</b>	<b>66.6%</b>	<b>128.9</b>
TRANSPORTATION	996.1	11.1%	8.6%	16.7
<b>UNDEVELOPED LAND</b>	<b>8,980.6</b>	<b>100.0%</b>	<b>77.7%</b>	<b>150.4</b>
<b>Total Area</b>	<b>11,554.7</b>		<b>100.0%</b>	<b>293.1</b>

Source: 2008 Comprehensive Development Plan, JEO Consulting Group, Inc.

Note: Acres per 100 is based upon the 2006 US Census population estimate of 5,970



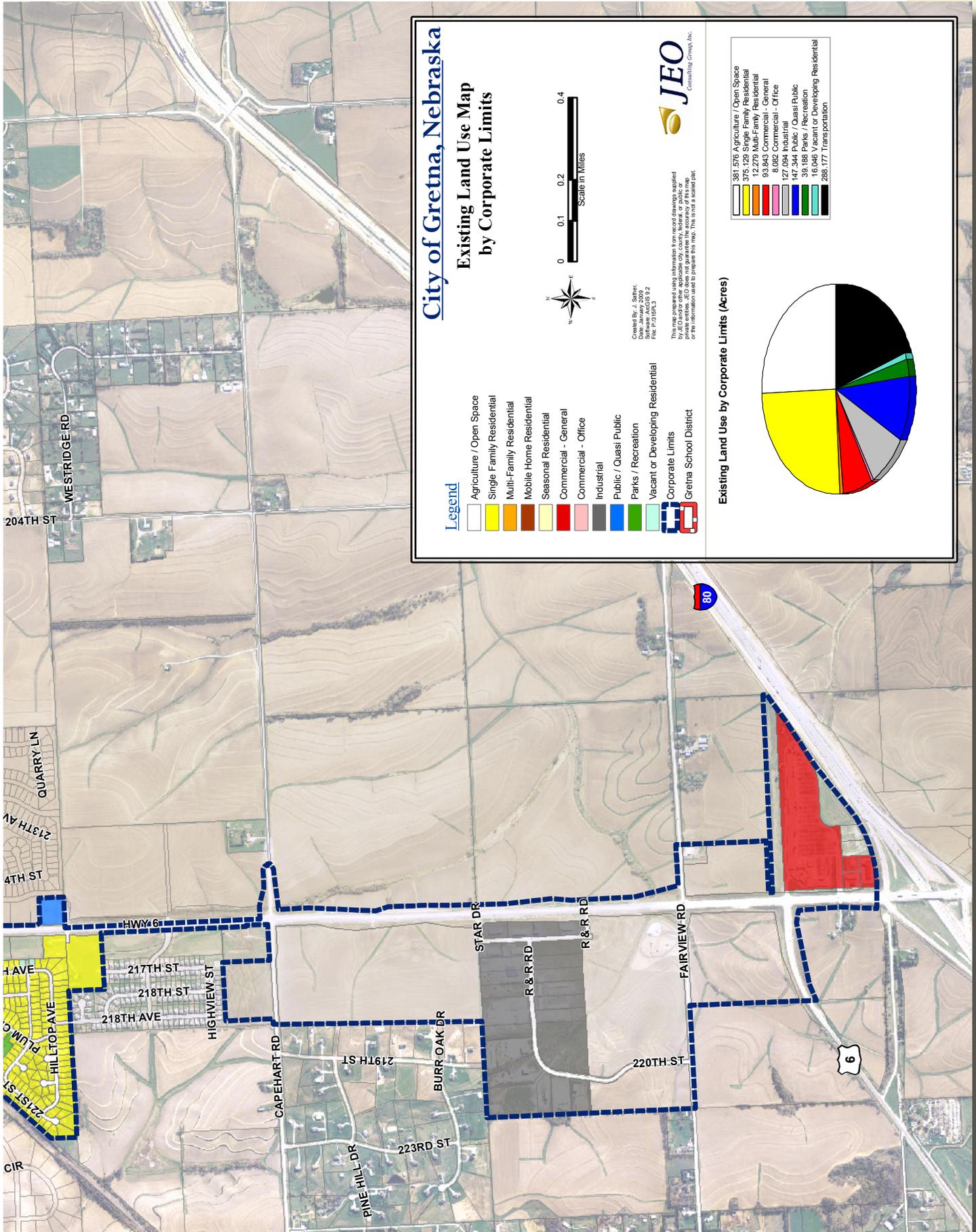
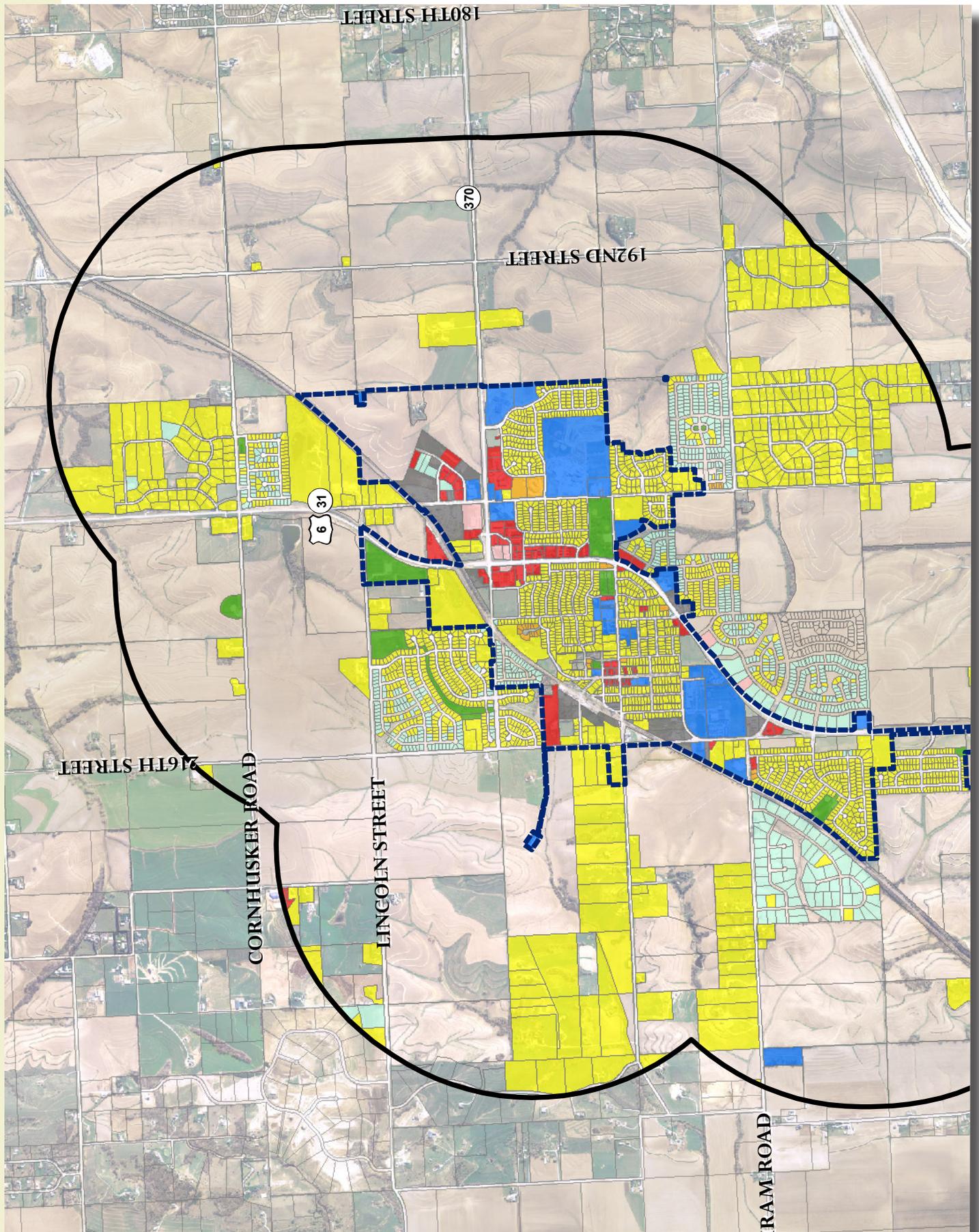
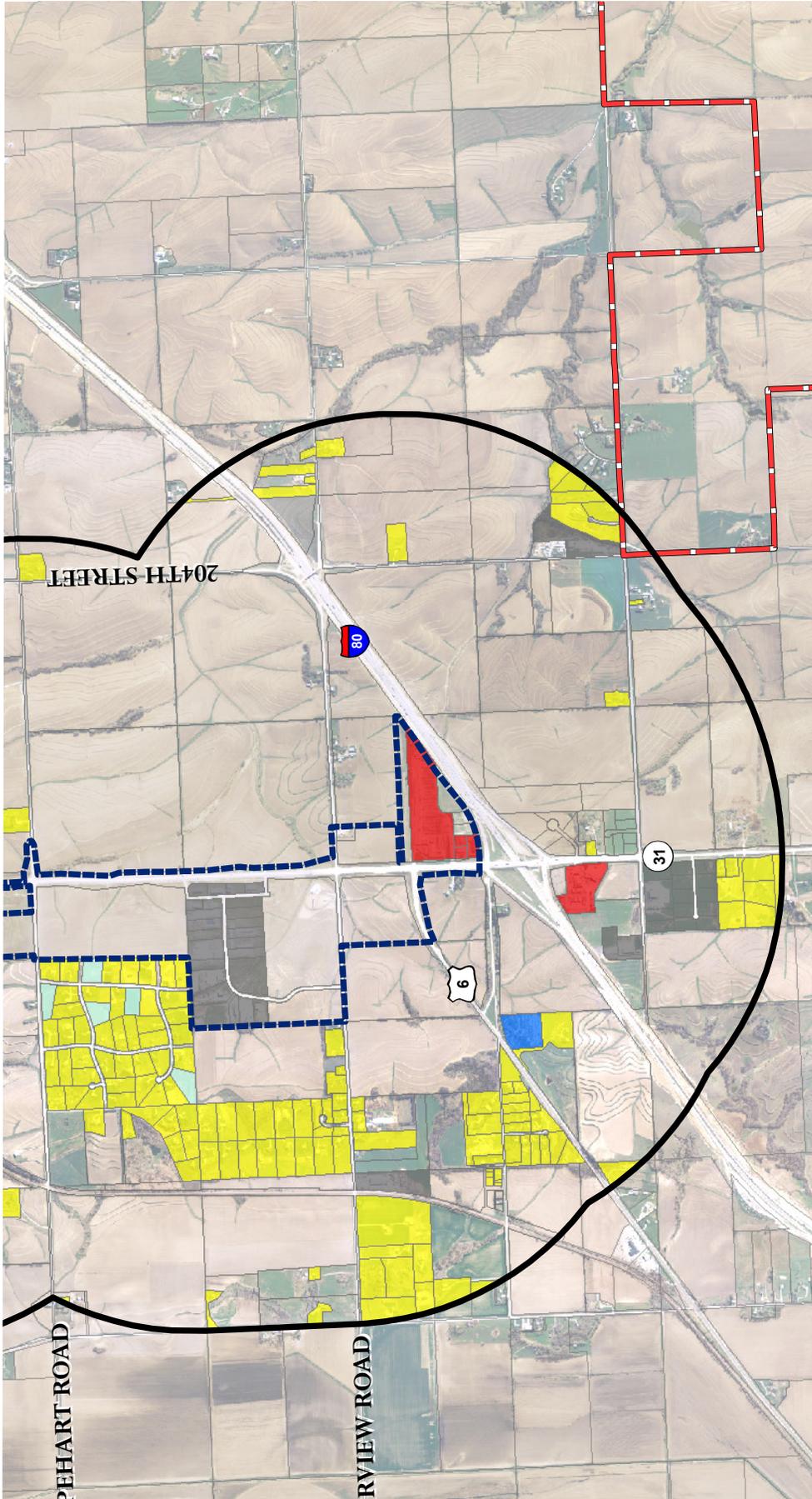


FIGURE 18: EXISTING LAND USE MAP BY EXTRATERRITORIAL JURISDICTION, 2008





# City of Gretna, Nebraska

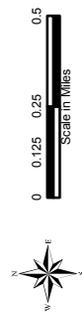
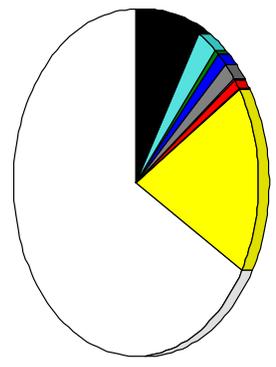
## Existing Land Use Map by Extraterritorial Jurisdiction

### Legend

- Agriculture / Open Space
- Single Family Residential
- Multi-Family Residential
- Mobile Home Residential
- Seasonal Residential
- Commercial - General
- Commercial - Office
- Industrial
- Public / Quasi Public
- Parks / Recreation
- Vacant or Developing Residential
- Corporate Limits
- Extraterritorial Jurisdiction
- Gretna School District

Acres	Land Use Type
7,693.123	Agriculture / Open Space
2,015.54	Single Family Residential
13,184	Multi-Family Residential
115,446	Commercial - General
11,38	Commercial - Office
192,127	Industrial
164,754	Public / Quasi Public
61,632	Parks / Recreation
291,449	Vacant or Developing Residential
996,056	Transportation

Existing Land Use by ETJ (Acres)

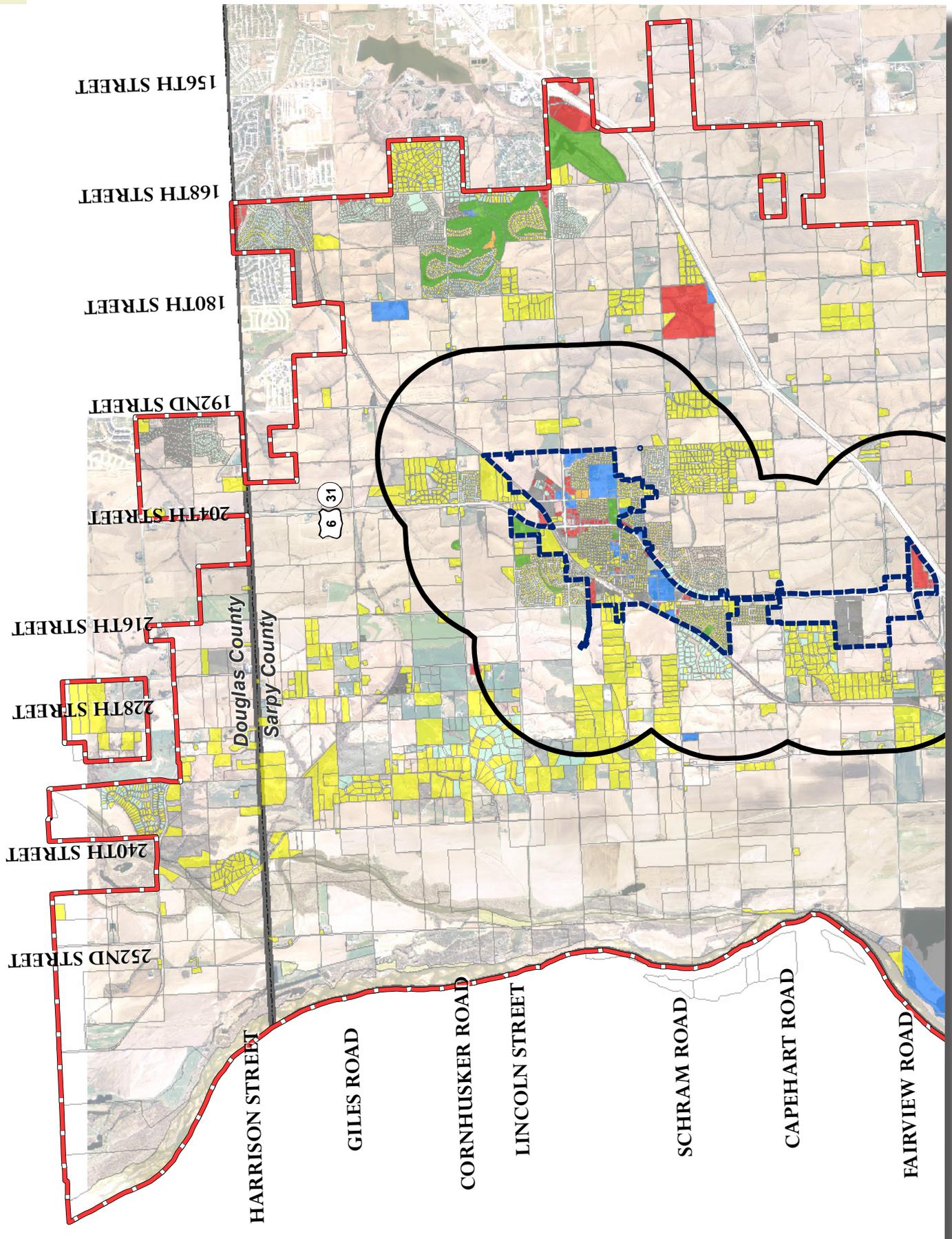


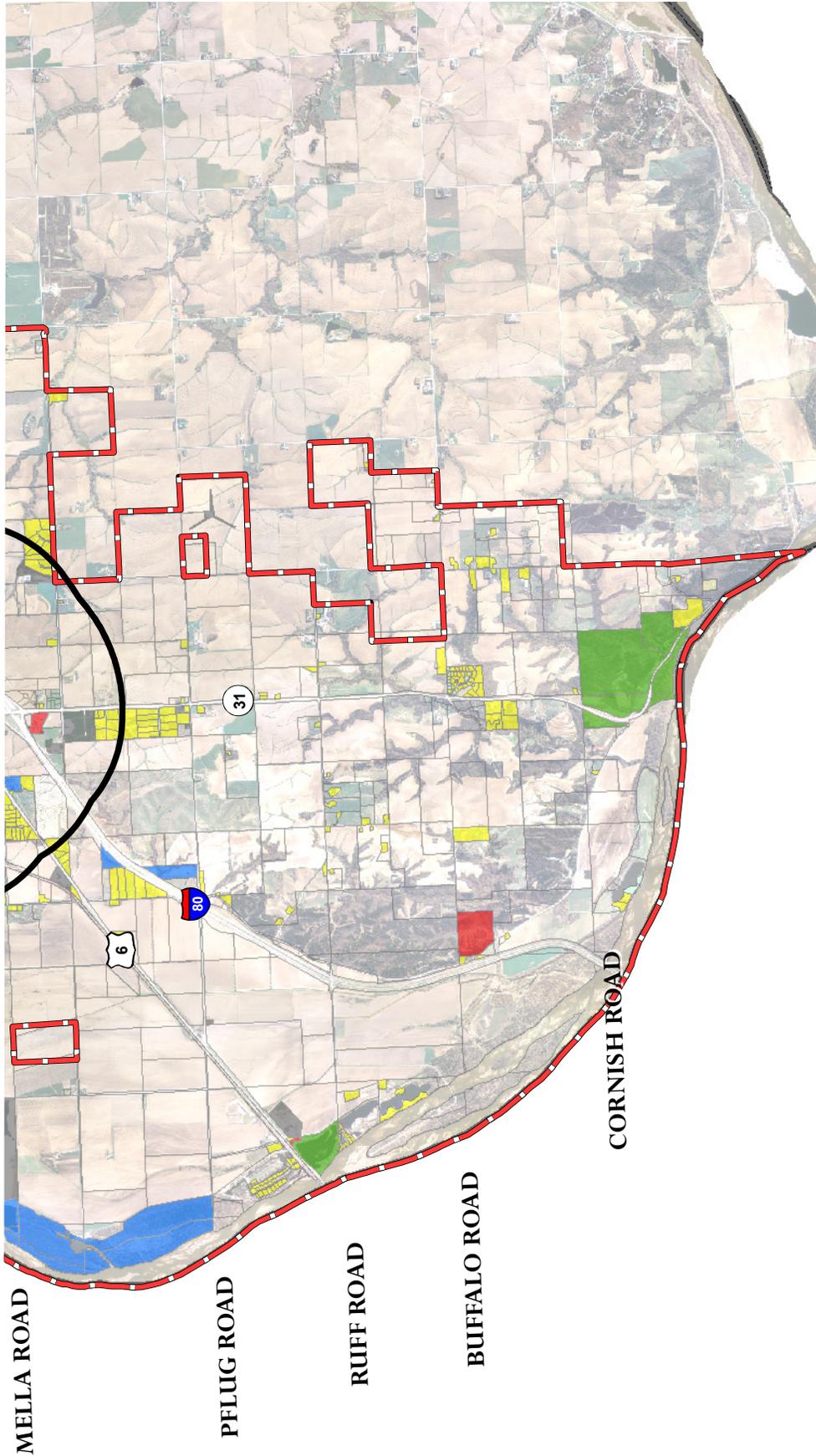
Created by: JEO  
Date: 04/20/2009  
Software: ArcGIS 9.2  
File: P1010133



This map prepared using information from record drawings supplied by JEO and/or other applicable city, county, federal, or public or private sources. JEO and its staff have not conducted a field check of the information used to prepare this map. This is not a liability.

FIGURE 19: EXISTING LAND USE MAP BY SCHOOL DISTRICT, 2008





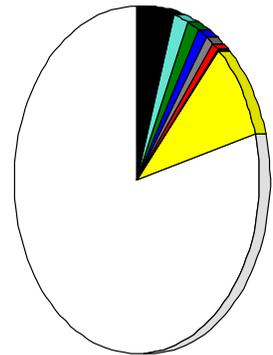
## City of Gretna, Nebraska Existing Land Use Map by School District

### Legend

- Agriculture / Open Space
- Single Family Residential
- Multi-Family Residential
- Mobile Home Residential
- Seasonal Residential
- Commercial - General
- Commercial - Office
- Industrial
- Public / Quasi Public
- Parks / Recreation
- Vacant or Developing Residential
- Corporate Limits
- Gretna School District
- Extraterritorial Jurisdiction

Color	Acres	Category
White	42,051.51	Agriculture / Open Space
Yellow	4,788.411	Single Family Residential
Orange	31,519	Multi-Family Residential
Light Blue	71,882	Seasonal Residential
Light Green	427,762	Commercial - General
Light Purple	11,36	Commercial - Office
Light Red	493,689	Industrial
Dark Grey	664,311	Public / Quasi Public
Blue	846,609	Parks / Recreation
Green	1,046,167	Vacant or Developing Residential
Light Blue	2,585.23	Transportation

Existing Land Use by School District (Acres)



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# Envision Gretna

Introduction

Public Participation Program

Visioning

Goals, Objectives, and Policies



# Envision Gretna

## INTRODUCTION

The City of Gretna committed to a process to update its Comprehensive Plan into a document that will describe and guide the future development of the City and identify land uses in the designated growth areas and the Gretna School District. The process of preparing a Comprehensive Plan and land use designations for the future of a community is an on-going process of goal setting and problem solving aimed at encouraging and enhancing better communities through a higher quality of life. Planning focuses upon ways of solving existing problems within the community, and providing a management tool enabling citizens to achieve their vision for the future.

“Envision Gretna” is the section of the Comprehensive Plan that describes how the City sees itself in the future. The envision section provides both a graphic and textual forecast of changes and improvements that community leaders and residents desire to see. In order to develop these desires, community leaders, stakeholders, and residents took part in town hall meetings, focus group meetings, and a two-day charrette and visioning process that resulted in meaningful public dialogue and the creation of preferred images of Gretna in the future.

# Public Participation Program

- Focus Groups
- Town Hall Meetings
- 2-Day Charrette



# Public Participation Program

The public participation program for PlanGretna was a way of obtaining community information and opinions and engaging the participants in exercises to develop visions for the community. This public input included several elements designed to create a partnership between community leaders, stakeholders, citizens, and the consultant. Public participation, input, and review are critical to the success of this project and support for this Comprehensive Plan. Key components of the public participation program include:

- Focus Groups
- Town Hall Meetings
- 2-Day Charrette

## FOCUS GROUPS

Focus group meetings were used to gather input from specifically identified and invited City staff, stakeholders, and residents. There was 13 focus group meetings with specifically identified and invited residents. Participants were identified because of their ability to provide specific information on a given topic of interest. These meetings were held in late December of 2007 and the first part of January 2008. The meetings generally started with a set of specific questions tailored to the particular groups, but were allowed to progress in a more conversational manner. Participants were encouraged to speak freely and answer questions completely in the hope of identify major issues facing Gretna. Since each group had specific areas of knowledge, the information gathered varied between them. The following is a summary of the issues raised in each of the focus group meetings:

### MIDDLE SCHOOL STUDENTS FOCUS GROUP (1/8/08)

Students would like to see the following:

- Improvements to the pool:
  - It's not big enough
  - Need more features (high dive)
    - Bottom of pool is rough
- Need improvements to Outlet Mall such as better stores
- Need more fast food and sit down restaurants
- Need more grocery stores
- Need a bowling alley
- Roller skating rink
- Need Skate Park
- Need amusement park
- Need amphitheater
- Larger library is needed
- Need YMCA
- Parks are outdated (old equipment)
- Need a college
- Need more sports fields and better maintenance of the existing ones.
- Need an arcade
- Need ice skating rink
- Need winter recreation such as sledding hill (currently use Chestnut Ridge)

Students like the small town and low crime rate

Community needs more sidewalks and crosswalks across streets/highways.

Where do you go?

- Dollar General
- Pool

Their parents are heard to want:

- Discount shopping such as Wal-Mart
- More retail stores such as clothing
- A sporting store
- Sit down restaurants
- Something like “Spikers” and indoor sports arena such as “the Courts”

### HIGH SCHOOL STUDENTS FOCUS GROUP (1/7/08)

- After high school students will likely attend college (in State)
- In 10 years they do not see themselves in Gretna. May want to come back but it depends.
- Many don't feel like Gretna residents because they live outside corporate limits.
- Bad: -Not a lot to do
- Good: -15 minutes from Omaha
- What brought parents to Gretna?
  - came back to area
  - jobs
  - family
- What would bring you back to Gretna?
  - to live in a city next to a big city
- Need Old Downtown re-vitalized
- Have a lot of trees – good
- Need a Village Point. More shopping but not Wal-Mart. Target ok
- Nothing to do in Gretna for youth. No where for family oriented activities
- Parks need make-over but ok
- Need more smaller pools
- Community Center would be great
- Trails good but need more of them and more sidewalks.
- Need a teen hang-out area or facility

### OTHER ELECTED OFFICIALS FOCUS GROUP (1/7/08)

- Promote economic development but not at all cost
- Development is like farming – plant the seed and let develop
- Need good quality companies
- Don't over tax infrastructure
- Learn from existing development (mistakes)
- Areas where infrastructure exists is key. Need to line in development
- Identify areas of short term investment
- Need to identify what type of areas should be developed
- Need to make plan attractive and comfortable to live and work
- Have a good school system and nice community - Need to capitalize

- Growth is happening, change is happening. Need to change with times
- Need to coordinate with other jurisdictions
- Need to look at the Sarpy County sewer study that has been completed
- Need to figure out how we can serve areas with sewer.
- Look at 180th Street interchange for possibilities
- May have to upgrade the Pflug Road interchange to tie into the proposed Beltway or will need additional interchanges. Would be nice to know where such Beltway will go.
- Business Park is warranted so quality jobs will be created.
- Gretna is this size, shape, and quality of life because this is what they want. It's a chance to get away from the big city.
- School helps with the identity to community.
- Need to attract certain kinds of people - Utilize the environment and amenities
- Location of development is always major.
- Rooftops will determine the commercial growth – commercial will follow.
- Industrial shall be along the Interstate with access
- Should avoid residential along the Interstate
- Would like to see or could see an Amtrack station in Gretna, although not really a good railroad for that now.
- Gretna needs attractive type of development with unique architecture to develop or strengthen pride in community.
- Need regional recreation/attraction. Gretna may be perfect for it.
- Need to develop common vision.
- Need to look at sewer plan and watersheds for development opportunities.

### SCHOOL ADMINISTRATION FOCUS GROUP (1/7/08)

- School will be looking at boys and girls swimming programs so need to look at swimming pool and work with City such as what was done in Elkhorn.
- Library needs is experiencing growth demands. Where should a new one locate? Library with elementary school will have security and censorship issues.
- If they have 4-5 elementary schools then will need an additional middle school.
- If 6-7 elementary schools then a second high school.
- The ridge is a problem. Once development gets past the ridge then everything could break loose.
- School district with 5-6 elementary schools, 2 middle schools and 1 high school could handle enrollment of 5,000 students.
- School Board has worked with DLR in past and will work with them again very soon to update the master plan for the district.
- Could use a recreation center. Location of such is a question. Possibly at 204th and Angus or at Quonsets site. Maybe in new town center north of Hwy 370.
- Need to install conduit under streets and highways when being constructed.
- Need to preserve sense of community.
- A new “center” (town) will be nice. Something different. Need something different than the typical strip center.
- Need to define growth in Gretna.

## ELECTED OFFICIALS FOCUS GROUP (1/7/08)

- Issues include:
  - Highway system
  - Inadequacy of highways for commuters, especially 370. 6/31 is being corrected.
  - Fire and Rescue will be a problem. Fire personnel will be an issue south of town if all the development occurs because there is not enough population to draw volunteers for a station in that area.
  - What happens with dividing the community?
  - What happens when the downtown moves? Should we create a new district with old downtown.
  - Infrastructure is an issue, including water, sewer.
- Need to attract businesses that employ 10+ workers.
- Need park plan to identify new parks and land dedication.
- Need good residential development.
- Should look at higher density residential and if it is needed.
- Identify different land uses and determine where industrial should go.
- Good points:
  - Annexation
  - Schools
  - Fire Department
  - Snow Removal
- Negative points:
  - Lack of things to do
  - Aging infrastructure
  - Streets
  - Lack of police protection
  - Looks of older structures, businesses, etc.
- Could reach 5,000 population in 5 years and 10,000 in 10-20 years.
- Need to continue to work with school district. Build buildings together, maintain agreements, develop more inter-local agreements, and look at a possible pool and community center.
- I-80 Corridor should not have residential uses. Commercial by interchanges and industrial and open space in other areas along the Interstate.

## SCHOOL BOARD FOCUS GROUP (1/12/08)

- According to Sarpy County Platteview Road will be 4-lane from I-29 to I-80
- 180th Street should be developed. Had to work with County already on widening 168th Street for Palisades Elementary School.
- Next elementary school will be at Falling Waters at 192nd and Giles.
- Cooperation with City:
  - Thomas Elementary School had land dedicated to the city for their requirement, but school has agreed to maintain it as their school ground. This works.
  - Gretna elementary schools accommodate 600. This is larger than other districts. Most other elementary schools have 7 acres, but Gretna has been typically working with 15 acres to allow for growth. Buildings are larger and they have full sized gyms.

- Gyms can be used by the public as long as in school district. Cannot use during school hours or game times.
- The beauty of a YMCA or Community Center is that public could use facilities during school hours and games.
- Community Center should probably be located in northeast quadrant. Other areas include west of 370 and 6, Quonsets, or area east of senior housing.
- Kush’s house/property could be prime for something like that beings near existing fields.
- Hwy 370 development and crossing of such is not viewed as a problem because people drive too much.
- A new library may need 6 acres. Could see a main library but also satellite libraries which would provide opportunities, meeting rooms and tie the community together.
- Amphitheater is possible north of downtown in vacant area.
- Maintenance shops (existing) should be looked at. What will happen to them?
- Need additional recreations such as community center, sledding hill, pool (indoor and outdoor). Maybe the city pool should be outdoor and the community center pool should be indoor and utilized as a school pool such as what Elkhorn did. Also if Gretna Schools developed a swim program they may have to Co-op with another school such as Elkhorn to get started.
- Things City can do:
  - Be open minded
  - Have open session such as working planning sessions.
  - School and city have quarterly meetings (great) and they need to continue. District has a written goal to keep a relationship with the city.
  - Need to keep that small town feeling and need to identify the “Heart” of the community.
  - Need a physical landmark for Gretna such as what the old water tower was (Maybe a fire shooting dragon at the high school that does something when touchdowns are scored).

### GRETNAM CHAMBER OF COMMERCE FOCUS GROUP (12/20/07)

What is Gretna’s Greatest Asset in terms of Economic Development?

- Proximity to Lincoln and Omaha
- 2 exits off I-80
- Excellent School System
- Small Town Atmosphere
- Good Reputation

How could the new Comp Plan Help?

- Sewer and Water extended south towards I-80
- Preserve the uniqueness of Gretna (avoid what happened to Millard)
- Corridors to I-80 must be developed properly

What type of Growth does Gretna want?

- Planned development, anticipate growth and control it
- Don’t just grow were its possible, grow where it should be located

#### What Barriers Exist?

- Infrastructure
- Corner of Hwy 6 and Hwy 370 (370 needs 4 lane)
- People are not fully aware of the extreme growth of the city
- Space/Location limited particularly industrial
- Large lots for Com/Ind very limited
- Topography
- Environmentally sensitive areas

#### How do you eliminate the Barriers?

- Get information out to the people (newspapers)

#### How are you Marketing Gretna?

- Chamber adopted a new slogan “Spend your green in Gretna”
- Business Fair
- Gretna Days

#### What other groups do you work with?

- All the chambers from the metro area got together once at an event in Elkhorn provided for some good networking opportunity
- Sarpy County Economic Development has some minor involvement in Gretna
- Sarpy County Economic Development has figures for the entire county and shares those with anyone
- Gretna’s new sales tax is bringing in a lot of money to the city how should it be used? (question from the mayor)
- People want a community center, new/revamped pool, expanded library, but should it be used to extend infrastructure to assist in new development?

#### What is the best area for new development?

- Along the hwy corridors

### OPTIMIST CLUB FOCUS GROUP (1/3/08)

#### Assets:

- Location
- School District
- Housing Availability
- Res. Development
- Unique small town feel
- 2 interchanges on I-80

#### Lacking:

- Industrial/Commercial sites
- Low end Housing, Multifamily complexes are full with waiting lists
- Commercial along I-80 between Interchanges

#### 10yr Vision:

- Doubling of population
- 370 developments were pushed back with the expansion to four lanes

- South along Hwy 6 will develop first
- Population could quadruple if the growth rate continues
- City needs to stick to the plan and not bend to developers
- Continual updates will be needed to keep the plan current

## Issues:

- Infrastructure limitations particularly sewer going south towards I-80, water is becoming scarce may need to tap into MUD line
- City cannot afford to run its own police dept currently the contract with Sarpy Co.
- There is a school resource officer and the State Patrol helps out as well
- Infrastructure wise it is easier to grow to the north
- Until everything settles out 370 will be a hot area for development
- There is some interest in bringing in a college possibly in NE Crossing

The Optimist Club's role in the community is to keep the small town feel of the community by working with the schools and youth. The club is leaned on pretty heavy by the community to keep people active in the community

### SCHOOL STAFF FOCUS GROUP (1/7/08)

## Issues:

- School overcrowding, build new schools prior to existing schools becoming overcrowding
- Losing small businesses
- Downtown has become a Bar Zone
- Swimming pool needs updating and expansion and maybe relocation, waterpark features
- Overpass across hwy 6 and possibly 370 in the future
- Parks system needs to expand as city grows, doesn't serve the needs of the kids currently
- Local Police Department
- Better lighting in old Gretna
- Need more Teen activities
- K-12 Rec Center that gives kids a place to go after school
- An Old Market type area in Downtown
- Kids are playing in Millard sports due to the lack of strong competition in Gretna, (too much of the everyone plays mentality)
- Limited Park space for Sports Facilities
- Few restaurants
- Outlet Mall is ineffective
- Community College in portion of Outlet Mall could be effective
- Losing the Bowling Alley took away the only entertainment opportunity Gretna had
- Need Entertainment Activities for both youth and adults
- Teen problem will continue to go unless they have some place to be
- There is a good range of housing choices except for \$300k to \$500k homes in a neighborhood setting
- North Park area is beginning to decline
- There are some rental homes in the older neighborhoods

- There is subsidized housing in one of the Apartment Complexes
- City is hesitant to putting in new apartment complexes
- There is a large interest in Theater in Gretna, Mizzula Theater comes to town every summer and is very popular
- City could use a coffee shop hangout type of business
- Would like to see tech firms and larger corporate jobs as opposed to manufacturing blue-collar jobs
- Hospital will be need as area continues to grow

### MINISTERIAL FOCUS GROUP (1/7/08)

Current and Future Issues:

- Good School District that must remain at the top as the community continues to grow
- Keeping the crime rate low
- Transition from a traditional small town into a growing community, in order to keep the small town feel the community organizations and the schools need to remain strong figures in the City
- Reaching out to areas that feel as though they are a part of Gretna, using the community organizations and schools
- Building a new High School in the Northeast
- Downtown in Failing, everyone goes to Omaha to shop and eat out
- When Gretna first expanded there was some tension between the old and new residents, that could happen again with future expansions

What do you hear from citizens?

- Need a Community/Recreation Center
- Expand Hwy 370 and growth will follow
- People feel good about living in Gretna
- Access to Omaha and Transportation Routes

What plans do your churches have for expansion?

- Every church in town has already expanded some in the past 10 years
- Have room to expand on their current sites but as the area continues to grow they will look at planting satellite congregations
- Those satellite sites could be useful in pulling people into Gretna from the surrounding area

### PLANNING COMMISSION AND BOARD OF ADJUSTMENT FOCUS GROUP (1/7/08)

Issues:

- Community Facilities are lacking and in disrepair
- Need to attract more commercial development
- Hwy 370 is tough to predict what might happen with that corridor
- Can't continue to try to pick and choose projects when they all conform to the plan/regs
- There are some inconsistencies between the planning commission and the city council

- Need to annex the triangle between hwy 370, hwy 6 and I-80
- Need to develop a plan for installing and controlling infrastructure improvements
- Vala's Pumpkin Patch and the Platte River are excellent recreation/tourism draws for Gretna
- Mulhall's is a good example business the City would like to encourage to locate in Gretna
- Looking for more service oriented businesses, preferably Mom and Pop stores with local ownership
- Encourage the development of a large commercial park along hwy 370
- Turn old downtown into a Village Point type pedestrian friendly shopping/ dining area
- The Coventry Development is an example of new urbanism that Gretna should encourage for other large developments
- Pflug road interchange will only benefit a few land owners/developers and will detract from the scenic qualities of the area
- There are new design standards in place that keep a tight reign on new buildings within the hwy corridors in particular
- In future years Infrastructure will determine where development can occur and until some of those problems are solved little development will be possible
- Another problem for development is that Gretna has no large area of flat ground for a major corporate campus, several business have looked at Gretna but not been able to find land good enough to suit their needs
- Parks in general are lacking in Gretna
- Needs to be a trail connection to the Omaha system and the Platte River Bridge
- Loss of kids to Millard sports programs has more to do with the level of competition than the lack or disrepair of fields
- Main St redevelopment will be tough due to lack of parking and age of structures
- Elkhorn's downtown was lost don't want that to happen to Gretna
- Development of a new town square is possible and could shift the center of the City closer to recent developments in the Northeast

### CITY STAFF FOCUS GROUP (1/8/08)

#### Issues:

- Gretna would like to retain the unique feel and small town atmosphere
- Residential growth is in the Northeast where infrastructure is available
- Commercial retail that fits with residential should be encouraged
- Making old Gretna a "Old Market" type area
- Industrial development should occur to the south along Hwy 6 to the Interstate
- City would like to maintain centralized city services and not spread things out
- Need a new library that has the capability for expansion in the future should the need arise

- City currently does not have enough water to supply a new major commercial development they would have to dig another well
- There is an overall lack of commercial development in Gretna
- Servicing the southern portion of Gretna with sewer will be difficult and expensive
- City needs more water storage
- Water in the south is not of sufficient quality and quantity for development
- City would like to be at 5,000 people by the 2010 census
- For the City to reach 10,000 people it will depend mostly on the market but could happen in the next 10 years with current growth rates
- To keep up with growth the City will need to look at adding a Police Dept. full-time paid fire staff, a hospital facility, a college and continued economic development
- Gretna's location is one of it's best assets with easy access to Omaha and the Interstate
- Everyone goes to Omaha for entertainment activities
- Nothing for kids to do in town particularly teenagers
- Need to encourage infill/redevelopment of open lots and old buildings already in town
- There are runoff issues with any new development
- NRD dam planned for 192nd and south of Giles
- There is potential for Gretna to share facilities with the school district such as a Pool, Library, and Rec Center
- Currently Gretna offers Baseball, Football, Soccer, Softball though volunteer organizations, there are not enough fields available for community use

## PARKS AND RECREATION FOCUS GROUP (1/12/08)

### Issues:

- Lacking Full-size Baseball Field for Adults requires roughly 3.7 acres
- Practice areas for football and soccer are difficult to find and schedule
- Baseball/softball practice fields are few
- City needs to hire a person to manage and schedule the parks fields for practices and games
- Adding lights to practice facilities could extend playing time and help alleviate some of the lack of fields
- Good competition sites could bring in tournaments which in turn brings people into Gretna
- There are some issues working with the school district for using fields particularly the HS fields
- Recreation groups work with the City to share maintenance of the facilities and are willing to do the same with the schools

## TOWN HALL MEETINGS

Two town hall meetings were held on January 8th, 2008 at the Gretna City Hall to gather public input. The purpose of the meetings was to gather input on issues (both positive and negative) facing Gretna from the residents' perspective. At the meetings, the attendees were numbered off into small groups. Each group then worked with a facilitator from JEO Consulting Group, Inc. to identify various issues and come up with some strategies to address those issues. Once all responses were recorded, participants were invited to identify the issues on aerial photography/maps.

### Opportunities Analysis

**Negatives** – What are the most negative aspects of Gretna?

**Positives** – What are the most positive aspects of Gretna?

**Future** – What is your vision for the future of Gretna?

**Actions** – What needs to be done in order to accomplish this vision?

Input was gathered through the use of a modified opportunities analysis. Participants were asked four questions about their perceptions of Gretna. The questions were presented in an open-ended style, designed to encourage multiple answers and group discussion. All responses were recorded. It should be noted that individual residents will vary as to their views about Gretna as it exists today and how it should exist in the future. By asking questions in a public format such as this, participants work to form a consensus-based list that identifies and prioritizes the most commonly held views. This process allows all views to be shared with the community, so that no view is left out.

One of the most significant issues to come from the town hall meetings was that residents like the small town feel that Gretna has. Residents recognize this and its residents as important assets and want to protect them. Residents want good polices and a plan that manages growth in such a way that it doesn't detract from the existing town and doesn't simply merge them into the Omaha metro like has occurred with other communities. Gretna wants to remain a community with its own identity. The following information summarizes the results of those small groups at the two town hall meetings:

### NEGATIVES

#### "WHAT NEEDS TO BE IMPROVED IN GRETNA?"

The participants in the Town Hall Meeting were asked to respond to this question as honestly as possible. They were told this was a brainstorming exercise, and that there was no right or wrong answer. Through brainstorming and listing every response, the participants are more likely to engage in a discussion that can lead to more responses. The reasoning behind this question is to identify what topics in the community are negative so that through the comprehensive plan these negatives can hopefully be eliminated or turned into positives.

- Larger library
- Larger swimming pool/aquatic park
- Interconnected trail system
- Economic Development
- Buffer strips in between conflicting land uses
- Design and Sign guidelines
- Make Downtown a destination
- Utilize the City owned land north of hwy 370
- Senior/Teen center
- Emergency services facility expansion
- Plan for future traffic
- City maintenance facility
- More "Green" aspects
- After school programs
- YMCA/Community Center
- Gretna welcome signage
- Infrastructure
- No Industry (large employers)
- Lack of Parks/Greenspace
- Hwy 370 traffic congestion
- No entertainment
- "Neighborhood Parks"
- Water system
- Lack of sense of identity – architecture
- Quonsets
- Uniqueness needed
- Lack of fine dining
- Tourism
- Sewer system improvements needed for growth
- Stormwater runoff

- Hwy maintenance
- Lack of business network
- Lack of knowledge of economic incentives
- Snow removal
- Services – retail
- 168th St needs improvement
- Need better access east
- Lack of downtown commerce
- No heart and soul in downtown
- Lack of identity
- Street lights at 216th and Gruenther
- Improve hwy 370 from 204th to I-80
- Outlet Mall (dead)
- Need tennis facility
- Communicate with new outlet mall owner
- Field/parking for adult baseball
- New recreational facility
- Better cooperation between City/School District on new facilities
- Control over strip malls
- Entertainment facilities
- Options for kids and teens
- Better connection to exit 426 (Mahoney SP and SAC museum)
- Revisit zoning district locations
- Currently a pass through community
- More destination options
- Cooperation with county to avoid lawsuits
- Support for local businesses
- Better location for day cares
- Need a recreation department to coordinate parks and youth activities
- More trees
- Curb appeal of the City
- Need more access points along the interstate
- Bike Paths
- Indoor pool
- Too much growth
- More paved roads
- Sidewalks
- Take more steps to retain individual culture
- Closer health care
- Depot at Rail Road area (Amtrack)
- Improve RR crossings
- Controlled Growth
- Concession stands at ballfields
- Youth activities/programs

## POSITIVES

### “WHAT DO YOU LIKE ABOUT GRETNA?”

This question was presented to the participants. The reasoning behind this question is to identify topics in the community that are positives and through the planning process these positives can be used as building blocks for improving the City.

- Good parent involvement
- Community support for schools
- VFW
- Extra curricular activities through the school
- Affordability (Keep it that way)
- Open communication
- Water quality
- Valla’s Pumpkin Patch
- Wherspann Lake
- Strong Family Values
- Population
- Rural opportunities
- Own identity
- Sarpy County
- School/City relationship
- Small town opportunity
- Different from Millard
- Good neighborhoods
- “Smart Growth”
- Good zoning policies
- Potential for a great town
- Potential to draw a magnet/destination
- Public library
- Good infrastructure
- New sewer line – increased capacity
- Access to transportation system
- Emergency services
- Friendly people

- Safe/kid friendly environment
- Strong chamber of commerce
- Stellar citizens
- Planning process
- Community leadership
- City/County employees
- Natural amenities
- Platte River
- Recreational areas
- Banks
- Parks
- City Services
- Pool
- Easy to get around
- Unique businesses (Vala's, fabric store)
- Outlet Mall
- Proximity to Lincoln and Omaha
- School District
- Access to new hospital at Lakeside
- Hwy 6 corridor to Center St and Dodge St
- Small town feel
- Drug Stores
- Medical community
- Churches
- Existing Downtown to work with
- Brick Streets
- Youth sports program
- School is the right size
- Foreign exchange students
- Volunteerism
- Gretna days
- Fast access to county roads
- Small enough to walk around
- Snow removal
- Fabric store
- Town history
- Rail access
- Exit 426
- Cabelas
- Auto repair businesses
- Good Civic Groups
- Silver Ridge Senior Facilities
- Other Nursing Home

## VISION

### “WHAT IS YOUR VISION FOR GRETNA?”

In order to respond to this question, participants were asked to think about what they want to see in Gretna in the future. This question attempts to raise issues that have been, may be, or will be topics that will affect the future of Gretna.

- Development using contour grading plans
- Trees
- Make Gretna into a destination
- More than just a bedroom community
- High-tech employment opportunities
- Economic development opportunities
- Work with universities or college to provide secondary education
- Hospital/medical development
- 30,000 to 50,000 population in 30 to 40 years
- Determine where growth will occur
- Locate City services in a central location
- Increase City staff as the population grows
- Hire an economic development coordinator
- Gretna Area Development Corporation become more active
- Higher education campus/research park
- Incentives for businesses
- Higher design standards
- Green construction
- Industrial development
- Green linkages
- Sticking to future plans
- Local owned businesses
- New urbanism developments
- Pedestrian friendly developments
- New transportation routes
- Hub for mass transit
- I-80 high-tech research parks
- Protect environmental amenities
- Downtown revitalized
- Bandstand

- “Olde Gretna”
- Bring people to Nebraska Crossing
- Nature rec facility - Platte River Valley
- Keep agricultural feel
- Trail system
- Build out of subdivisions
- Organized development along Hwy 6 and Hwy 370 corridors
- Mixed-Uses along Hwy’s 6 and 370
- More communication with MUD
- Youth activities
- Community Center
- Indoor/Outdoor swimming pool
- Outdoor sports complex with adult sized fields
- Keep library/City facilities connected to residential areas not commercial
- Traffic/pedestrian control at 370 east of Hwy 6
- Overall plan for City land near Fire Hall
- Motorsports Park at I-80
- Additional Fire Hall
- Limited access along 370 and 6 with access roads
- New library
- More national chain stores/ restaurants
- Recreation including a bowling alley
- More visible sheriff’s office
- New City administrator
- Small town feel (Mayberry)
- Smart growth
- Arterial park with open space
- Restaurants
- Business park
- Big boxes at interchanges
- Infill development
- Buffer Gretna to limit or define the community
- Open floodplain up for development with appropriate levee
- No class A schools
- Regional park
- Finish dam sites
- Developments focused on “Baby Boomers”
- Large community center
- Develop 180th St – business retail/ office
- Become a first class City
- Develop an identity
- Connectivity
- Use of City publications for promotion
- Trails network
- Attraction Multi-use area Amphitheater
- Street improvements
- “River Walk” type corridor
- Satellite college campus

## ACHIEVING THE VISION

### “WHAT NEEDS TO BE DONE TO ACCOMPLISH THE VISION?”

This question asked participants to think of any potential project that they desired to see accomplished in Gretna. This gave the participants an opportunity to dream a little and express their desires for the City.

- Maintain effective community groups and the organization of those groups
- Plan for additional Libraries
- Partner with the schools when possible
- Remove the Quonset Huts and other City eyesores
- Be pro-active when it comes to City expansion
- Keep close watch on County and State activities that could affect Gretna
- Think and act like a larger city
- Build a reputation around the metro area as a great place to live and be
- Develop the City owned land north of Hwy 370
- Restore the Historical Cemetery
- Biannual Community Development meetings-participation
- Some Forum to generate “Buzz” ie website, newsletter, etc
- Trails Plan
- Parks Plan
- Become a “Green City”

- Use Subdivision Regs to beautify the City
- Cash in lieu of dedication system for parks
- Design Standards for Multi-Family Dwellings
- Sustainable Design
- Bridge for Pedestrians across the Hwy 6 and possibly Hwy 370 in the future
- Business Incubator
- Have a Plan
- More Tax base- friendly to Gretna values
- Cooperation with other governmental entities
- Community Involvement/Buy-in
- Economic Development staff member
- Transportation- Light Rail
- More efficient government spending
- Water/Sewer Improvements
- Impact fees for SID's on City services
- Destination Entertainment/Attraction (uniqueness)
- Hospital (e.g. Bryan LGH North)
- College
- Metro Tech/Library ie La Vista
- Find financial resources/investors/donors
- Use sales tax more wisely
- Exploit the I-80 Interchanges
- User Fees
- Land incentives
- Lobby NDOR and Fed USDOT for new interchanges
- Better School tax sharing
- Make Gretna a Family Friendly City – advertise
- Improve walkability
- Provide proper location for commercial areas along the corridors
- Money, Money and more Money
- Grants/research

## 2-DAY CHARRETTE

A 2-day long charrette was held in order to work with community residents through an intense graphic exercise. The charrette was conducted at the School Administration Building, and attended by over 30 participants. Charrette participants were allowed to come and go during the process, and an open house presentation of the results was given on the final day.

### What is a Charrette?

The French word, “charrette” means “cart” and is often used to describe the final, intense work effort expended by art and architecture students to meet a project deadline. This use of the term is said to originate from the École des Beaux Arts in Paris during the 19th century, where proctors circulated a cart, or “charrette”, to collect final drawings while students frantically put finishing touches on their work.

The charrette utilized the talents of several design professionals to give life to the desires and preferences of workshop participants. The result of the charrette was series of images that represent various community improvements that participants desire to see in Gretna. The images created during the charrette process can be found throughout the Character Area Section of this Plan.

# Visioning

- Vision Statement



# Visioning

PlanGretna’s visioning process took what was learned in the public participation component and combined it with what we analyzed through demographics and existing conditions to develop a vision, goals, objectives, and policies for the City of Gretna. This visioning process helps establish land use policies and aids in the decision making process as development occurs in and around Gretna.

**Visioning** is a process of evaluating present conditions, identifying problem areas, and bringing about consensus on how to overcome those concerns and existing problems and manage growth and change. By determining Gretna’s strengths and weaknesses, the community can decide what it wants to become and develop a “road map” that will guide future decisions and ultimately fulfill the vision of Gretna and its residents.

Change and growth are continuous, therefore Gretna must identify specific criteria that will be used to judge and manage change. Instead of reacting to development pressures after the fact, the City, using its adopted vision, can better reinforce the desired changes and discourage the negative impacts that may undermine the vision. A shared vision allows Gretna to develop the goals and policies that will provide the support and direction necessary to bring the vision into reality.

Key components to Gretna’s Comprehensive Plan are the goals, objectives and policies. These will be developed based upon citizen input that defines a vision of what the City would like to become. The issues and concerns of the citizens are developed into a shared vision, or vision statement. The vision statement can then be further delineated and translated into broad based goals and objectives that will be used to guide, direct, and base future decisions on growth and development within Gretna and its designated future growth area. These goals and objectives will also be further developed into action statements (policies), which aim to provide the kind of “day-to-day” direction and monitoring that will be useful in implementing this PlanGretna and realizing the vision. Consensus on “what is good land use?” and “how to manage change in order to provide the greatest benefit to the City and its residents?” is formed. Gretna’s goals, objectives and policies attempt to address various issues, regarding the question of “how” to plan for Gretna’s future.

**Goals** are the desires and projected state of affairs which the community intends to achieve. Goals are the most general statement of future preferences and outcomes, and set a broad framework for objectives and policies. Goals should be established in a manner that is clearly understood and allows them to be accomplished. It is noted that the goals may need to be modified or changed from time to time to continue to reflect community preferences.

**Objectives** are the intermediate steps that guide the community from the present to the future. Objectives provide direction and benchmarks that can be used to monitor progress. Objectives also help maintain support and interest in Plan implementation by providing tangible actions that provide perceptible results. By actually observing and participating in Plan activities and their results, community residents are allowed to involve themselves in the evolution of their community, which enhances their support for the Plan and instills excitement for continued involvement.

**Policies** are more detailed than objectives, and describe the type of action that should be used to achieve the related goal. Policies are part of the value system linking goals with action. Policies represent plans of action that guide decisions in order to achieve rational outcomes, and are concerned with defining and implementing the goals and objectives of PlanGretna. The adopted policies synthesize the information

from the existing growth condition and responses from the public input process in order to develop solutions or actions that will achieve the various goals. Policies are a means to achieving the goals established by the community and they imply a clear commitment to the City's future development.

The development of goals, objectives, and policies should include the development of several specific elements that will enable the community to understand and utilize their potential. One process that explains these elements is the S.M.A.R.T.<sup>1</sup> process. This process provides insight into the way in which goal-setting can occur in a way that makes implementation more likely, and provides a framework for the creation of Gretna's goals, objectives, and policies.

Goals, objectives, and policies ensure that the Comprehensive Plan accomplishes the desires of the residents of Gretna. When these goals, objectives and policies are followed, development proposals in the community will be evaluated as to their relationship with the citizens' comments and shared desires. Therefore, goals, objectives and policies should be referred to as diligently as the Future Land Use Map or any other part of the Comprehensive Plan when reviewing and/or making recommendations on planning issues.

It is important for goals to be written in a manner that allows for both long-term and short-term accomplishments. Utilizing both long-term and short-term goals provides continuity throughout the implementation process, allowing the community to monitor Plan progress. Long term goals keep the community focused on their future vision and provide general direction towards achieving that vision. Short-term goals provide the kind of tangible results that can be used to preserve community support. Short-term goals are important for several reasons as they:

- Allow for immediate feedback and success, which fuels the desire to achieve additional goals and improved policies.
- Allow for the distribution of resources over time thus assuring a balanced use of public investment.
- Establish certain policies that need to be followed before the long-term goals can be accomplished.

### The SMART process:

**Specific:** objectives should be precisely defined rather than tolerating diffuseness or nebulosity

**Measurable:** objectives should define a method of measuring progress

**Agreed-To/Achievable:** objectives should be agreeable to all parties, and should be described in a way that makes them achievable

**Realistic/Rewarding/Relevant:** objectives should be realistic and describe accomplishments that make sense

**Time-related:** objectives should include a time-scale for completion

<sup>1</sup> George T. Doran, *There's a S. M. A. R. T. Way to Write Management Goals and Objectives*, *Management Review (AMA Forum)*, November 1981, pps. 35-36

## VISION STATEMENT

A vision statement for Gretna was created based upon various meetings, charrette process, and interactions throughout the planning process of updating the Comprehensive Plan. Such vision statement and a series of guiding principles lays the framework for developing the goals, objective, and polices; and the development of the Future Land Use Plan. Early on in the planning process, it was agreed upon by the Steering Committee that Gretna is a diverse, safe, and family oriented community bound together by shared, small town values and a commitment to excellence. The residents of Gretna value their small-town feel while enjoying the opportunities found in the metropolitan area and offering their residents high-quality education. Gretna always welcomes new residents and businesses to experience the community's quality of life. The vision statement for Gretna is the following:

**Vision Statement:**

The citizens of Gretna will continue to promote our strong sense of community pride and caring by working to improve our quality of life through creative planning and providing excellent services to support future growth and opportunities.

# Goals, Objectives and Policies

- Community Image
- Environmental Preservation
- Parks and Recreation
- Built Environment
- Residential
- Downtown
- Commercial and Office
- Industrial
- Transportation
- Community Facilities and Services
- Economic Development
- Citizen Participation
- Plan Implementation



# Goals, Objectives, and Policies

The goals, objectives, and policies that have been generated for PlanGretna 2009 are organized into general categories. The categories are broad enough to allow multiple issues to fall within them, but narrow enough to allow a fairly clear distinction and separation. These categories are used for a logical organization of the goals and policies, and are not presented in any particular order.

## COMMUNITY IMAGE

**Goal:** The unique community identity and sense of place that is recognized by the residents of Gretna and characterized by its setting and history will be respected by policy decision makers.

### Objectives and Policies:

- CI.1 The qualities that make Gretna unique, including the important qualities of its established neighborhoods and Downtown, should be identified and protected.
  - CI.1.1 Major entryways into Gretna will be identified, protected and enhanced in order to emphasize and preserve the natural setting and appearance of the community. Such entryways should incorporate decorative lighting, street medians, and additional landscaping where feasible and appropriate to create a visible invitation into the community.
  - CI.1.2 New developments should reinforce and compliment existing community aesthetics.
  - CI.1.3 New developments should provide continuity and interconnectivity between developments through the use of pedestrian trails and shared access.
  - CI.1.4 Local events that celebrate Gretna and attract visitors should be encouraged to improve civic pride and increase community recognition.
- CI.2 Public art is an important means by which the community can strengthen a sense of place and promote a positive image, and should be incorporated into public projects when appropriate.
  - CI.2.1 When public projects are being considered, a portion of the project budget should be allocated to a public art element to be incorporated into the project, when appropriate.
- CI.3 The design of individual urban and rural areas should be improved through innovative development practices.
  - CI.3.1 Developments along the corporate limits of Gretna should provide clear urban boundaries.
  - CI.3.2 Development proposals that include multiple land uses or are located in areas characterized by multiple land uses should provide open land buffers to separate conflicting uses.
  - CI.3.3 Large-scale residential developments should provide vital activity centers for residents.
  - CI.3.4 Developments proposed in areas having environmental assets should include the preservation of critical natural areas and vistas.
  - CI.3.5 Commercial developments, neighborhoods, and public open spaces should be linked by appropriate connections with the transportation system.
  - CI.3.6 Arterial and collector streets should be designed as efficient travel corridors.
  - CI.3.7 Development across the community should provide a balanced distribution of housing and job opportunities.

- CI.3.8 Residents of Gretna should be provided with a broad range of diverse housing types, sizes, and price ranges.
- CI.3.9 Developments that require the extension of public infrastructure should be designed to maximize the conservation of physical and social resources.

## ENVIRONMENTAL PRESERVATION

**Goal:** Gretna will encourage the conservation and protection of natural resource areas and open spaces through reasonable, controlled growth and development that incorporates environmental amenities. Natural resources in and around Gretna will be protected and managed to insure long term quality, availability, and sustainability for the current and future residents.

### Objectives and Policies:

- EP.1 Areas that include significant native ecosystems and environmentally sensitive areas should be conserved, protected and/or restored through appropriate land use planning techniques.
  - EP.1.1 Wetland areas should be preserved or mitigated when the filling of wetlands cannot be avoided.
  - EP.1.2 Areas that possess a potential risk to the health, safety, or welfare of the public should be developed only when guidelines exist that promote the safe and reasonable development of those areas.
  - EP.1.3 Areas with particular topographic features that present substantial barriers to development should be maintained in a natural or passively developed condition.
  - EP.1.4 Healthy natural vegetation should be protected and preserved through creative development design.
  - EP.1.5 Trees are encouraged throughout the community in public areas and street rights-of-way in order to improve the appearance of Gretna's public spaces. The City should create a set of standards to promote the planting of recommended tree species and develop a plan for removal of those posing a hazard.
  - EP.1.6 Protect all water supplies and aquifers from development activities that may affect the quality and/or quantity of water. Development shall demonstrate a positive or, at least, a neutral impact on surface and ground water supplies.
  - EP.1.7 In making land use decisions relative to industrial or other uses likely to pose a threat to air quality, the City will consider proximity for the proposed use to residential areas and meteorological factors such as prevailing wind direction and velocity.
- EP.2 Gretna will collaborate with other local, county, regional, and state agencies to preserve the 100-year floodplain, areas prone to inundation by stormwater, and natural drainageways, in order to protect the community's ability to appropriately disperse stormwater runoff.
  - EP.2.1 Natural drainage areas, floodplains, and wooded areas should be utilized as greenways that define and connect neighborhoods within the community.
  - EP.2.2 Sources of pollution, both point and non-point, should be controlled through the promotion of best management practices.
  - EP.2.3 Federal standards for air and water quality should be followed strictly, and strengthened when appropriate.
  - EP.2.4 Energy conservation and sustainable development practices should be encouraged through education and site design and building orientation techniques.
  - EP.2.5 Incorporation of adopted stormwater policies into every new development.

- EP.2.6 Zoning and design standards should be created or enhanced to protect the natural resources of Gretna through the encouragement of preservation and conservations practices, as well as regional storm water detention.
- EP.2.7 As Gretna continues to grow, the City will evaluate the benefits of participation in the FEMA National Flood Insurance Program to prevent flood-caused loss of life and property, by applying identified mapped areas showing the floodplain and floodway and regulating development in those areas. Development within floodplains and areas subject to stormwater inundation should be discouraged, unless accepted and required safety measures are enforced to protect human safety.

## PARKS AND RECREATION

**Goal 1:** Gretna will support the development of recreational opportunities that enhance our environmental amenities and provide citizens with opportunities to interact with the natural environment.

**Objectives and Policies:**

- P/OS.1 Natural open spaces, drainageways, and green corridors should be preserved to delineate a continuous pedestrian transportation system that interconnects the community and provides access to regional public access areas.
  - P/OS.1.1 Creative development patterns should be encouraged that incorporate natural open spaces and greenways in a way that maintains their visibility and accessibility.
  - P/OS.1.2 Floodplain areas along creeks and drainage areas should be used to provide park and recreation opportunities within passive open spaces, land use buffers, and stormwater detention and drainage facilities. Preserve the natural attributes of both the floodplain and floodway to avoid loss of life and property while providing open space.
  - P/OS.1.3 Parks, open spaces, greenways, and recreation areas should be used to buffer incompatible land uses from one another.
  - P/OS.1.4 Park and recreation facilities should be designed to accommodate the particular needs and interests of area residents while protecting, preserving, and conserving the environmental character and quality of the area.

**Goal 2:** Gretna will provide appropriate parkland, recreation, and open space facilities that meet the diverse desires of its citizens and visitors and the needs created by additional growth and expanding development. These facilities should be a combination of the expansion/updating of existing facilities and the establishment of new facilities.

**Objectives and Policies:**

- P/OS.2 Local park land and amenities should be maintained and improved to reflect a high community appearance standard and project citizen pride.
  - P/OS.2.1 Funding mechanisms for park purposes, such as maintenance, improvements, and land acquisition should be created to facilitate the further development of passive and active recreational area.
  - P/OS.2.2 The location level of service provided for active and passive recreational areas, fields, and other amenities should enable pedestrian access within a five-minute walk from all residences.
  - P/OS.2.3 The quantity level of service provided for active and passive recreational areas, fields, and other amenities should be a rate of approximately one developed park acre per 14 zoned residential acres.
  - P/OS.2.4 Park land shall be dedicated in each development according to park and trails map or park fees shall be paid in lieu of land dedication.

P/OS.2.5 Existing parks should be upgraded to their level of service and amenities according to the according to the approved park and recreation section of this plan.

P/OS.2.6 Provide parks and recreational facilities that are reasonably accessible and have the necessary amenities including open spaces/sports fields for the residents and visitors of Gretna.

P/OS.3 New recreational facilities and programs should be encouraged and developed to respond to the particular needs and desires of residents of all ages.

P/OS.3.1 The potential to create, enlarge or rehabilitate outdoor recreation facilities and/or develop an indoor multi-purpose facility should be explored, and pursued if reasonable. The City of Gretna will need to explore the development and expansion of recreational opportunities in the future including, but not limited to: an aquatic center or splash park, additional soccer fields, and updating all the city parks equipment.

P/OS.3.2 The feasibility of providing a system of trails in conjunction with City utility easements and City streets should be reviewed, and implemented if reasonable.

P/OS.3.3 Encourage private developers to actively contribute to the City's park, recreation and open space system and encourage the development of private recreational facilities to supplement those provided by the City.

P/OS.3.4 Establish new set of standards requiring or promoting dedication of parks and open space and fees in lieu of such dedication.

P/OS.3.5 New residential developments should provide adequate public recreational facilities to serve their anticipated residents as well as the community as a whole. Such recreational development should be based upon the adopted Parks and Trails Map.

P/OS.3.6 New park and open space sites should be identified within established growth areas, and constructed prior to development and before acquisition becomes too expensive.

P/OS.3.7 Usable park and open spaces should be located in highly visible areas accessible by pedestrian and vehicle, rather than located behind development or on remnant tracts of land.

P/OS.3.8 Public acquisition and development of additional park sites should be limited to those with substantial recreation potential.

P/OS.3.9 Location of new schools should generally coincide with location of proposed neighborhood parks according to the adopted Parks and Recreation Map based upon future land use and development. Such locations would eliminate duplicate amenities or inefficient construction.

P/OS.3.10 Maintain and improve recreational amenities offering year round use.

P/OS.3.11 Promote recreation as a means of economic development for Gretna. Regional parks or recreational facilities should be explored, including flood control structures with recreational amenities and/or a regional recreational facility near Interstate 80. Locating, development and promotion of these opportunities should be performed jointly with Papio-Missouri NRD, Nebraska Tourism, Nebraska Department of Economic Development, etc.

## BUILT ENVIRONMENT

**Goal 1:** Development sites in Gretna will adhere to appropriate design standards that ensure compatibility with their proposed and surrounding uses and particular locations.

**Objectives and Policies:**

BE.1 The community should ensure an appropriate balance of uses by establishing thresholds for use types based upon historic development trends, market influences, and public desires.

BE.1.1 Long term growth areas for the City should be preserved in order to facilitate future urban development.

BE.1.2 Acreage developments should be directed to areas outside of future urban growth areas in order to minimize conflicts between urban and acreage uses and so the city may provide urban services as efficiently as possible. Urban development should occur in areas immediately abutting the City that reflect a logical and timely extension of urban infrastructure.

BE.1.3 Any lighting used to illuminate an off-street parking area, sign or other structure should be arranged so as to deflect light away from any adjoining property or from public streets and minimize light escape from excessive uplighting, through fixture type, height and placement.

BE.1.4 The use of existing land features, vegetation, and stream corridors should be promoted as natural buffers.

BE.2 The quality of existing neighborhoods and defined districts within the community should be strengthened by enhancing neighborhood identities through revitalization efforts.

BE.2.1 The historical, cultural, and institutional resources of the community should be preserved, protected, and promoted.

BE.2.2 As Gretna grows, new development should distinguish itself from the cities of Omaha, Papillion, and La Vista.

**Goal 2:** Gretna will grow and develop in a balanced manner that respects private rights and promotes public benefit.

**Objectives and Policies:**

BE.3 Development should take place in an orderly fashion, take advantage of existing urban services, and avoid, to the extent possible, patterns of leapfrog, noncontiguous, and scattered development.

BE.3.1 Contiguous and infill development should be encouraged as preferred practices, and sprawling development patterns should be discouraged.

BE.3.2 Growth limit boundaries should be implemented that are based upon the efficient provision of adequate infrastructure to guide new growth to areas most able to support new development.

BE.3.3 Infill growth and development should be strongly encouraged in areas served by existing and available infrastructure in order to maximize infrastructure system efficiency.

BE.3.4 Higher intensity and/or less compatible commercial uses should be located in areas not adjacent or in close proximity to residential neighborhoods.

BE.3.5 Higher density residential development should be used as a transitional land use between commercial developments surrounding low-density residential neighborhoods; compatible low-intensity office development should be encouraged as an alternative transitional land use into low-density residential neighborhoods.

BE.3.6 Annexation decisions should be based on the ability to provide services and should ensure fiscally responsible boundary expansions.

BE.3.7 Subarea studies should be pursued with other agencies when there are common issues that need to be addressed.

- BE.4 Opportunities should be sought for the City to cooperate with Sarpy County and adjacent communities to develop and coordinate complimentary growth patterns.
- BE.4.1 Regional policies and programs should be sought within Sarpy County that provide and protect defensible edges between rural and urbanizing land.
- BE.4.2 The area recognized as Gretna's Future Growth Area should be continually reviewed by city and county staff and officials to assure compatible and sound uses that will be adequately serviced.
- BE.4.3 Within Sarpy County, development within an area defined as a future growth limit that may hinder the appropriate and logical expansion of urbanizing communities should be minimized.
- BE.4.4 Interlocal agreements shall be achieved with each adjacent jurisdiction in order to recognize suitable growth jurisdictions so that land may be developed efficiently and appropriately. Such agreements should be based upon topography; infrastructure and service capabilities; and watershed, fire and school districts.

## RESIDENTIAL

**Goal 1:** Gretna will enable the development of a mix of housing types and residential densities to provide housing opportunities for, and meet the needs of, its various socioeconomic groups.

### Objectives and Policies:

- R.1 Land development regulations should be developed and implemented to guide high quality new residential environments.
- R.1.1 Many activities of daily living should occur within walking distance. Neighborhoods should include homes, stores, workplaces, open space, schools and parks and recreation opportunities.
- R.1.2 New housing development proposals should protect and incorporate existing environmental features.
- R.1.3 Residential areas should be protected from incompatible land uses that may create adverse impacts. New and existing residential development should be separated from more intensive uses, such as heavy agriculture and industrial development, by the use of setbacks, buffer zones, or impact easements.
- R.1.4 Adequate screening and buffering should be provided between residential areas and adjacent commercial and industrial development, including highways, streets, parking and service areas.
- R.1.5 The housing needs of the elderly and disabled should be addressed in a way that allows aging residents to remain in Gretna. Such housing opportunities shall be located where compatible with the residential neighborhood.
- R.1.6 Multi-family and elderly housing should be encouraged and located nearest to commercial areas.
- R.1.7 Residential areas should be designed with a pedestrian orientation, including interior trails that connect to the public sidewalk system. Such trails and sidewalks should be designed to encourage walking and bicycling and provide multiple connections within and between neighborhoods and to commercial areas. Sidewalks should be provided on both sides of streets, or in alternative locations as allowed through design standards.
- R.1.8 Public uses such as elementary schools and churches should be located near the center of neighborhoods.
- R.1.9 Parks and open space should be within walking distance of all residences.
- R.1.10 The City shall encourage sustainable development policies that utilize natural features and drainage to minimize the impact of development upon the watershed and nearby waterways.

- R.1.11 The City shall encourage energy/water efficient development in all housing units; this includes the use of energy/water efficient appliances, proper insulation, and retention of all old growth trees when possible for shading purposes.
- R.2 New housing development should be served by public infrastructure at the least cost.
  - R.2.1 The efficient use of infrastructure should be promoted by focusing well-designed new and redeveloped housing on vacant, infill, or under-developed land.
  - R.2.2 Develop subdivision regulations that provide for a quality living environment while avoiding inefficient and expensive public infrastructure expansions, such as implementing cluster developments.
- R.3 Neighborhood revitalization programs should be developed to promote successful neighborhoods.
  - R.3.1 The City should explore providing a city-sponsored organization that utilizes grant funds and volunteers to assist with the repair, removal, replacement, or winterization of homes of low income families, disabled residents or senior citizens on limited incomes.
  - R.3.2 The aesthetic viability and quality of existing residential neighborhoods should be encouraged to influence and be reflected by new developments.
  - R.3.3 Promote the preservation, maintenance and renovation of existing housing and neighborhoods throughout the City. The appearance and condition of existing housing should be preserved to prevent the negative impact of poorly maintained property upon the viability of neighborhoods.
  - R.3.4 Assess, enhance and maintain infrastructure and services in existing neighborhoods.

**Goal 2:** New neighborhoods will be designed and developed with a sense of community that reflects Gretna’s history, culture, and individual character.

**Objectives and Policies:**

- R.4 New neighborhoods should provide a sense of place with a mix of compatible uses that serve residential needs.
  - R.4.1 Streets should interconnect neighborhoods with limited dead-end and cul-de-sac streets, and encourage walking, reducing the number of automobile trips, and conserving energy by reducing the length of automobile trips.
  - R.4.2 Proposed street layouts should respond to local topography, water courses and greenways.
  - R.4.3 Neighborhoods should be defined with a center that serves as a gathering space for residents.
  - R.4.4 Higher building densities and higher intensity residential land uses should be encouraged to separate single-family and low density residences from commercial uses.
  - R.4.5 New housing developments should provide an aesthetic character that represents traditional “neighborhood” design.
  - R.4.6 The City of Gretna will review and consider, whenever possible, any new or alternative development concepts or proposals, provided such concepts or proposals are consistent with and do not compromise in any way the established disposition of the adopted land use and goals, objectives and policies of this plan.
  - R.4.7 The city’s development regulations should be flexible enough to allow for unique housing products and creative development design, while limiting the mass produced form of past development trends, and encourage neighborhoods to maintain the small town feel that is important to the City’s residents.

## DOWNTOWN

**Goal:** Gretna should establish its current Downtown as a mixed use arts and culture district.

### Objectives and Policies:

- D.1 The Downtown area should be supported by an aggressive attempt to address and abate derelict and nuisance properties in order to help Downtown grow in its appeal and preserve its historic and aesthetic character.
  - D.1.1 The use of development incentives tailored to attract uses to properties within Downtown should be explored as a means to encourage appropriate reinvestment in the area.
  - D.1.2 Infrastructure improvements in the Downtown area should be designed to enhance the appearance of Downtown, including streets, parking, utilities, lighting, signage and streetscaping.
  - D.1.3 Downtown development should promote pedestrian access through the use of sidewalks and trails that connect the core to the rest of the community.
- D.2 The Downtown should be enhanced with an Arts and Cultural District designed to celebrate the arts as well as Gretna's heritage.
  - D.2.1 A Downtown Master Plan should be developed and adopted that will help guide the re-development/transition of the area.
  - D.2.2 The display of public art should be incorporated into public projects.
  - D.2.3 The display of private art should be encouraged through display methods and locations that enhance the experience of visiting the area while remaining compatible with Gretna's values and aesthetic character.

## COMMERCIAL AND OFFICE

**Goal:** Gretna will maintain and expand the variety of retail and commercial service activities and densities available to residents as well as visitors to the community.

### Objectives and Policies:

- C/O.1 New office, retail and commercial development should be provided in locations within the existing city boundaries that conveniently serve existing neighborhoods, or in areas designated on the Future Land Use Map such as along Highway 370, Highway 6/31 and the Interstate.
  - C/O.1.1 New or established commercial uses should not encroach upon, or expand into, existing residential neighborhoods, unless otherwise designated as mixed use.
  - C/O.1.2 Strip commercial development should not be developed in a linear strip along a roadway nor be completely auto oriented, but rather locate parking to the inside and create more pedestrian orientation. In areas where linear commercial development occurs due to existing parcels, topography constraints, or other factors, such developments be encouraged to build according to the established guidelines of the area or corridor. Creativity in construction of such developments is encouraged while meeting minimum guidelines.
- C/O.2 Site design for new commercial areas should incorporate elements that promote high quality developments.
  - C/O.2.1 The impact of commercial activities on neighboring land use areas, particularly residential ones, should be minimized through site design strategies, appropriate building orientation, and buffering.
  - C/O.2.2 Appropriate transitional methods should be considered at all

locations where the development or expansion of commercial land use abuts existing or planned residential development.

C/O.2.3 Signage used within and around commercial areas should be designed to compliment the materials and scale of surrounding development.

C/O.2.4 Commercial development along arterial roads shall be strategically located so as not to compromise the character of the area. Development percentages shall be established for commercial land uses at major intersections.

C/O.2.5 Commercial districts should be located:

- i. where urban services and infrastructure are available or planned for in the near future;
- ii. in sites supported by adequate road capacity – commercial development should be linked to the implementation of the transportation plan;
- iii. so that they enhance entryways or public way corridors, when developing adjacent to these corridors; and
- iv. in a manner that supports the creation and maintenance of green space.

C/O.3 Commercial and Office areas should be supported by an attempt to promote quality retail commercial and business park uses within Gretna’s jurisdiction.

C/O.3.1 The use of development incentives tailored to attract uses to properties should be explored as a means to encourage appropriate investment and reinvestment in certain areas.

C/O.3.2 Infrastructure improvements should be designed to enhance the potential of commercial growth, including water, sewer, and streets.

C/O.3.3 Small business development and employment growth should be supported throughout the community.

C/O.3.4 Future commercial retail and business park development should be coordinated and encouraged southeast of town along the Interstate, south side of Gretna along Highway 6/31, and east of Gretna in areas designated along Highway 370.

## INDUSTRIAL

**Goal:** Gretna will continue to encourage business and industrial development as an important source of revenue and employment for the community.

### Objectives and Policies:

I.1 Site design for new industrial areas should incorporate elements that promote high quality developments.

I.1.1 The impact of industrial activities on neighboring land use areas, particularly residential ones, should be minimized through site design strategies, appropriate building orientation, and buffering.

I.1.2 Appropriate transitional methods should be considered at all locations where the development or expansion of industrial land use abuts existing or planned residential development.

I.1.3 Signage used within and around industrial areas should be designed to compliment the materials and scale of surrounding development.

I.1.4 Industrial districts should be located:

- i. where urban services and infrastructure are available or planned for in the near future;
- ii. in sites supported by adequate road capacity – industrial development should be linked to the implementation of the transportation plan;
- iii. so that they enhance entryways or public way corridors, when developing adjacent to these corridors; and
- iv. in a manner that supports the creation and maintenance of green space.

- I.2 The impact of industrial activities, including increased traffic, noise, and pollution on neighboring land uses should be minimized through appropriate site design measures, and on should be mitigated to lessen environmental hazards.
  - I.2.1 New industrial development should be located in areas of similar or compatible use.
  - I.2.2 New industrial development should be located in areas where adequate public services, facilities and infrastructure already exists or can be provided in an efficient manner.
- I.3 Industrial areas should be supported by an aggressive attempt to promote quality light industrial type uses within Gretna's jurisdiction.
  - I.3.1 The use of development incentives tailored to attract uses to properties should be explored as a means to encourage appropriate investment and reinvestment in certain areas.
  - I.3.2 Infrastructure improvements should be designed to enhance the potential of industrial growth, including water, sewer, and streets.
  - I.3.3 Small business development and employment growth should be supported throughout the community.
  - I.3.4 Future light industrial and employment development should be coordinated and encouraged south of town in areas northwest of Exit 432 north of Highway 6 and in areas south of Exit 432 east and west of Highway 31.
  - I.3.5 Heavy industrial uses, if developed, should be directed south of town in areas northwest of Exit 432 north of Highway 6 where visibility is less from the Interstate, Interchange, and Highways 6 and 31.

## TRANSPORTATION

**Goal:** The transportation network for Gretna will provide interconnected access between neighborhoods and commercial areas, a balance of opportunities for vehicular, pedestrian, and other forms of transportation, and levels of service that respond to and influence land use needs.

### Objectives and Policies:

- T.1 A variety of funding mechanisms and sources should be identified to fund street improvements.
  - T.1.1 The City's development fees should be reviewed on an annual basis to ensure the appropriate relation between actual costs and revenue is maintained.
  - T.1.2 The City should continue to look at joint project funding with the NRD, county, State and Feds for various transportation and trail projects. Research and consideration of new funding programs should be exhausted.
- T.2 Street intersections should be designed or improved to minimize vehicular/pedestrian conflicts and eliminate dangerous driving conditions.
  - T.2.1 Efficient pedestrian movement throughout the City should be encouraged through site design, development patterns, and land use strategies.
  - T.2.2 Safe and well-maintained curbs and sidewalks should be provided throughout the City.
  - T.2.3 Pedestrian access through sidewalks and trails should be incorporated into the design of City streets when appropriate.
- T.3 Transportation-related decisions should be made in consideration of land use impacts including, but not limited to, adjacent land use patterns, both existing and planned, and their designated uses and densities.
  - T.3.1 Commercial and wayfinding signage (both permanent and temporary) along major arterials and other streets should be kept to a minimum and be low profile in order to minimize distraction of motorists, and

- meet the aesthetic guidelines of the corridor.
- T.3.2 Adequate vehicular circulation and interconnectivity within commercial developments should allow access among adjacent commercial developments without the need to drive on the public streets.
- T.3.3 Traffic within residential neighborhoods should not include substantial pass-through trips that originate and end outside the neighborhood.
- T.3.4 Possible development of a bypass south of I-80 at Platteview Road or Pflug Road is recognized. Continued research on such development should be done and careful consideration of land use decisions should be made as development progresses.
- T.4 Alternative transportation opportunities such as light rail should be considered in Gretna’s future as the area between Lincoln and Omaha continues to grow, the I-80 Corridor develops, and energy costs rise.
  - T.4.1 Light rail development in and near Gretna should be encouraged. Park and ride stations near Interstate 80 and/or near Highways 6/370 interchange should be considered in any re-development plans.

## COMMUNITY FACILITIES AND SERVICES

**Goal 1:** Gretna will provide and maintain a high level of governmental and civic services and facilities that sustain and enhance the quality of life for all residents and businesses.

### Objectives and Policies:

- CF/S.1 Public buildings and structures should be well built, functional, and designed to blend attractively within the context of surrounding development and to serve as a guide for future development or redevelopment.
  - CF/S.1.1 Public facilities and services should be provided in a timely, orderly and efficient arrangement that support existing and planned land use patterns and densities.
  - CF/S.1.2 Public personnel and equipment should be located throughout the community to provide timely and effective response and service.
  - CF/S.1.3 All public sites, buildings, and landscaping should be kept attractive and well-maintained.
  - CF/S.1.4 Sustainability development programs should be implemented within public building projects.
  - CF/S.1.5 Facilities should be provided that offer emergency services, meals, lodging, social activities and special events for the special needs and elderly residents in the community.
  - CF/S.1.6 Conservation programs and energy-efficiency practices and programs should be encouraged that reduce operating costs for energy, sewer, and water usage.
  - CF/S.1.7 A Facilities Management Study/Plan should be completed to assess public facility needs.
- CF/S.2 Work to repair/update the public infrastructure so as to create an inviting environment for attracting new residents and businesses into Gretna. All improved properties should be connected to the public sewer and public water systems. The extension of City services will be controlled so as not to over extend the system while not hindering appropriate development.
  - CF/S.2.1 The provision of additional public services should be utilized to encourage future land development patterns that promote efficiency.
  - CF/S.2.2 Existing infrastructure systems should be maintained and improved to enhance the economic value of existing neighborhoods and other development areas.
  - CF/S.2.3 Work to replace badly deteriorating streets as opposed to patching them.

**Goal 2:** Gretna will provide new infrastructure and community facilities, and conduct improvements and upgrades, in a fiscally responsible manner.

**Objectives and Policies:**

- CF/S.3 Infrastructure projects should be planned, funded, and constructed in a manner that maximizes the public benefit and minimizes the public cost.
- CF/S.3.1 Infrastructure should be built to a size and capacity that will serve the full development potential of a particular area in order to minimize the need to replace or improve facilities too soon after construction.
- CF/S.3.2 Infrastructure should be maintained in a safe and operational condition through regular inspection and preventative maintenance.
- CF/S.3.3 The infrastructure necessary to support future development should be provided concurrently with that development.
- CF/S.3.4 The costs of off-site infrastructure impacts and improvements should be financed by developers rather than the community as a whole.
- CF/S.3.5 Cooperative relationships with the development community should be sought to identify and implement creative methods of financing for infrastructure improvements.
- CF/S.3.6 Site designs should encourage compatibility with the natural characteristics of a site, minimize grading and impervious cover, and preserve site hydrology to the maximum extent possible.
- CF/S.3.7 Annexation requests should address the various impacts upon public safety and community facilities prior to final consideration.
- CF/S.3.8 Annexation policies should encourage contiguous development.
- CF/S.3.9 Opportunities to co-locate community facilities should be sought to maximize efficiencies in service provision and reduce capital and operating costs.
- CF/S.3.10 Water and sewer should be provided to areas south of town in a way that is financially responsible to allow for further development along Highway 6/31.
- CF/S.3.11 Evaluate all possible alternatives to continue to provide sufficient services to current and future residents, including tapping into MUD for water service.
- CF/S.3.12 Identify all funding mechanisms and sources to fund new infrastructure or improve existing services. Research and consideration of new funding programs should be exhausted.

## ECONOMIC DEVELOPMENT

**Goal:** Gretna’s focus for economic development will combine retention and expansion of existing businesses and the pro-active attraction of high impact businesses. Gretna will promote and welcome a broad variety of businesses; develop its entrepreneurial capacity through public and private partnerships, while leveraging its unique strengths in quality of life, location and education. The City shall strive to promote balanced growth bringing in, residential , commercial and industrial growth to create a self-sustaining community.

### Objectives and Policies:

- ED.1 Economic development partnerships between local entities, Sarpy County, and private companies should be encouraged and sought in order to assist existing and expanding business enterprises. The City of Gretna shall work with the Gretna and Sarpy County Chambers of Commerce and other economic development groups to encourage and promote economic development within the city and the surrounding jurisdiction.
  - ED.1.1 An identity for Gretna should be developed that can be utilized as a marketing tool for new business and residents.
  - ED.1.2 The recreational and entertainment opportunities within Gretna should be expanded so they may be promoted as quality-of-life and lifestyle amenities to new residents.
  - ED.1.3 A regional recreational development should be identified and considered to compliment the recreational opportunities near Mahoney State Park and to serve a growing population in the Omaha-Lincoln Metro Area.
  - ED.1.4 A new interchange at 192nd Street and I-80 should be promoted to regional and state officials as a way to support additional economic growth and accessibility.
  - ED.1.5 Gretna’s location between Omaha and Lincoln with access to I-80 should be continually promoted regionally and nationally.
  - ED.1.6 The youth of Gretna should be encouraged to remain in Gretna or return to the City after completion of their post-secondary education. The youth of the community should be involved in the identification and development of city projects.
  - ED.1.7 Encourage, promote and develop economic development partnerships between local entities and private companies to assist existing and expanding business enterprises.
  - ED.1.8 The City needs to be involved in all economic development ventures.
  - ED.1.9 Encourage and promote the development of home-based businesses and telecommuting based upon high technology communication infrastructure.
  - ED.1.10 Develop an office park in the City and market it to firms that might consider setting up a regional office in Gretna.
  - ED.1.11 The City should develop zoning and subdivision regulations that will provide for quality design and aesthetics for new commercial, industrial and even residential developments particularly at the entrances to the community.

## CITIZEN PARTICIPATION

**Goal:** The citizens and residents of Gretna will be informed about the planning process, included in policy guidance activities, and encouraged to become involved in the creation of the future of Gretna described in this Plan.

### Objectives and Policies:

- CP.1 Citizen volunteers should be encouraged to participate in ongoing plan implementation activities.
  - CP.1.1 The City should utilize multiple methods and utilize technology to provide education about the planning process to its citizens, including the city's website and surveys.
  - CP.1.2 Citizen committees and focus groups should be used when appropriate to address specific issues as needs and desires continue to change.
  - CP.1.3 Ongoing citizen involvement programs should be created that provide residents the opportunity to be involved in all phases of the planning process.

## PLAN IMPLEMENTATION

**Goal:** Gretna will implement this Plan through a joint effort of the citizens and leaders of the community. This Plan will be reviewed and updated as necessary to reflect changing public preferences and needs.

### Objectives and Policies:

- PI.1 This Plan should be reviewed on an annual basis for necessary updates, and should be reviewed in detail every five to ten years for potential significant updates.
  - PI.1.1 When major new, innovative development opportunities present themselves and have the potential to impact several elements of the Plan and which are determined to be of importance, an amendment to the Plan should be considered.
- PI.2 The policies and vision of this Plan should be supported by additional planning projects during the planning period.
  - PI.2.1 The City should utilize subarea planning studies to address the particular land use, infrastructure, or other needs of a defined area.
  - PI.2.2 Zoning, subdivision, and other regulations and development guidelines should be developed/amended and implemented that support and further the policies of this Plan.
- PI.3 Public resources are scarce and should be conserved and used efficiently, meaning infrastructure should not be built or developed if it is not needed.
  - PI.3.1 Public infrastructure, including transportation facilities, water, sewer, parks, schools, and libraries, are essential to the health, safety, and welfare of the community; as the community grows, these systems and facilities should be developed concurrently.
  - PI.3.2 Private capital is a significant part of the funding used to construct infrastructure in new developments and the City should pursue partnerships with private developers, investors, and other private funding sources to offset the costs of new growth.

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# Achieve Gretna

Introduction

Population Projections

Future Land Development Analysis

Future Land Use

Land Use Designations

City Beautification

Special Character Areas

Future Growth Area

Transportation Plan

Parks and Trails Plan



# Achieve Gretna

## INTRODUCTION

The Achieve Section of PlanGretna builds upon the previous sections by describing the various plan elements that will be used to manage future growth and development. Using the vision, goals, objectives and policies previously established, this section will help readers understand how the landscape of Gretna will be reshaped to result in the community’s future vision. Managing and guiding future change can be accomplished in many ways, with varying degrees of public and private influence. This section aims to develop an understanding that provides appropriate public guidance that encourages and facilitates intelligent and sustainable growth patterns while allowing enough flexibility to allow the market to fluctuate and respond to its changing demands and influences.

The process of achieving the shared vision for the community depends upon the realization of several components. Each component, by itself, represents an identified improvement or change that characterizes Gretna 20 years from now. The various components can be separated based upon the following ideas:

- Population Projections
- Future Land Development Analysis
- Future Land Use
- City Beautification
- Special Character Areas
- Transportation Plan
- Parks and Trails Plan

## POPULATION PROJECTIONS

For a complete analysis of population projections please refer back to the Profile Section of this plan. The following population projections were selected as the most probable and indicates the different scenarios that may be encountered by Gretna through the year 2030.

The population projections for Gretna are as follows:

Year	Low Series	Medium Series	High Series
2015	5,278 persons	5,522 persons	5,597 persons
2025	6,022 persons	7,835 persons	7,332 persons
2035	6,766 persons	10,584 persons	11,687 persons

## FUTURE LAND DEVELOPMENT ANALYSIS

A critical component of future community growth is the ability of Gretna to accommodate the anticipated population change. The development needs generated by future populations cannot necessarily be estimated based upon past trends. Recent trends nationwide suggest development patterns have been getting more compact, and will continue to do so as fiscal constraints limit a community’s ability to extend and provide additional infrastructure. In addition, market trends point towards different development types and ratios of built area to open space.

In order to accommodate future growth, a community needs to provide adequate developable area for that growth. Gretna is in a unique position in that it is located in between Lincoln and Omaha on Interstate 80 and is in close proximity to the Platte River. This roughly means that Gretna will be under development pressure for expanded commercial and residential areas for many years to come. To avoid growth

**Community Character** is important to the residents of Gretna, and tends to be associated with low to medium residential densities, a strong school district, and a City with community pride not afraid to grow.

that is inefficient and unsustainable over the long term, and does not maintain or improve the overall health of the community, Gretna needs to guide that development in a responsible manner.

## EXISTING AND FUTURE LAND USE ANALYSIS

Currently, development in Gretna's corporate limits is characterized primarily by single-family residential uses, and a large amount of agriculture/open space due to the recent annexation south along Hwy 6/31 to Interstate 80. Land use policies that have been expressed in PlanGretna seek to change the mix of uses throughout town and in the surrounding jurisdiction. Residential uses will contain a mix of unit densities, ranging from estate lots to medium/high density multiple-family uses. Commercial uses will expand significantly to include mixed use developments, business parks, and flexible light industry/commercial areas all targeted at the regional market as well as the travelling public moving between the Omaha and Lincoln Metropolitan Areas. Industrial uses will expand in some areas encouraging additional light industry. The land development needs of Gretna will be based on a different mix of uses rather than what currently exists due to the increasing population and development pressure and therefore, the increase in demand for commercial and industrial services.

The Future Land Use and Transportation Map (Figure 20), presented in more detail later, utilizes different land use categories than are depicted in the existing land use analysis. The most significant change is that planning for the future does not include allocating land to agriculture or undeveloped status within the one-mile extraterritorial jurisdiction. While a certain amount of undeveloped land is essential to allow the market to work efficiently and control land values, it is not a land use category that is mapped. Land located in the designated future growth areas of Gretna near the Platte River may have an agricultural designation due to natural and development restrictions. These areas are identified on the Growth Area Future Land Use Map (Figure 23). The other changes are due to the need of more specific categories within the future land use so as to properly direct and control future growth. Table 34 compares the existing land uses with the determined future land use categories and the change percentage of those overall categories.

Table 34 shows the development potential that exists within the current corporate limits of Gretna. For example, the Gretna's commercial area can triple its current acreages and the amount of residential area can double in size. These numbers show us that Gretna has had an aggressive annexation policy that has allowed the City to expand and keep up with market demand for vacant/developable ground.

It should be noted that the decrease shown in the Other Developed Land (public/quasi-public, parks/recreation, and transportation) category is due to the fact that a future land use map does not get into the specifics that an existing land use map does when it comes to individual parcels, for instance if there is a small park or church within a residential neighborhood, the existing land use map will identify it as such but the future land use map will merely consider it part of the residential category. The reasoning for this is that we do not try to predict specific parcels of land for these services to occur in the future but allow them to occur within any category. Parks and Recreation, Public, and Quasi-public uses are typically allowed outright or by conditional use in varying residential and commercial zoning districts. Large parcels of land with planned public or recreational uses may be identified as such on the Future Land Use Map (Figure 20) for future development and/or protection.

**TABLE 34: LAND USE COMPARISON – CORPORATE LIMITS, EXISTING 2008 AND FUTURE 2014**

Existing Land Use Category		Acres	Future Land Use		Category	Acres
Residential		387.40	Residential			<b>790.2</b>
<i>Single-family</i>		375.10		<i>Low Density</i>		142.5
<i>Multi-family</i>		12.30		<i>Medium to Low Density</i>		50
				<i>Medium to High Density</i>		597.6
<hr/>			<hr/>			
Commercial		101.88	Commercial			<b>377.3</b>
<i>Office</i>		8.08		<i>Business Park Commercial</i>		
<i>General</i>		93.80		<i>Central Business District</i>		7.81
				<i>General Commercial</i>		147.5
				<i>Highway Commercial</i>		195.2
				<i>Mixed Use Commercial</i>		26.8
<hr/>			<hr/>			
Industrial		127.09	Industrial			<b>258.4</b>
				<i>Flex Space</i>		96.4
				<i>Light Industrial</i>		162
<hr/>			<hr/>			
Other Developed Land		186.50	Other Developed Land			<b>186.5</b>
<b>Total Developed Land</b>		<b>802.88</b>				
Agriculture/Undeveloped		685.80	Agriculture/Undeveloped			<b>0</b>
<b>Total Area</b>		<b>1,488.68</b>	<b>Total Acres</b>			<b>1,612</b>

Source: JEO Consulting Group, Inc, 2008 and 2014

# Future Land Use

- AG Agricultural
- TA Transitional Agriculture
- RE Residential Estate
- LDR Low Density Residential
- MLDR Medium-Low Density Residential
- MHDR Medium-High Density Residential
- P/QP Public/Quasi-Public
- PRO Parks, Recreation, and Open Space
- MU Mixed Use
- CBD Central Business District
- GC General Commercial
- CR Commercial Retail
- HC Highway Commercial
- BP Business Park
- FS Flex Space
- LI Light Industrial
- HI Heavy Industrial



# Future Land Use

The purpose of this Future Land Use section is to guide public and private decisions regarding the use of land in the City of Gretna as it evolves over time. The Future Land Use Plan assists the community in determining the type, direction, and timing of future growth. The criteria established in this Plan reflect the following:

- The current use of land within and around the community.
- The desired types of growth, including location of growth.
- Growth pressures from outside the community.
- Physical characteristic, and strengths and constraints to future growth.
- Current population and economic trends affecting the community.

To be useful as a day-to-day decision-making guide with a long-range focus, this Future Land Use Plan must be flexible enough to balance the seemingly opposite goals of being both adaptable to unanticipated changes and specific to current conditions and issues in the community. During the next 20 years, conditions in the community will invariably change. This change is to be expected and is due to numerous factors including expected growth pressures from the Omaha Metro as well as unexpected economic, social and technological changes. In some cases, these factors may change dramatically from present conditions. For these reasons, and in order that it remains a useful decision-making guide, this Land Use Plan should be monitored closely and updated as necessary.

The City of Gretna has experienced steady population growth over the last decade, and this growth is projected to continue. This Future Land Use Plan addresses how the City will plan for its continued participation in the growth and development of the metropolitan area. Growth related issues include: determining how much of the projected growth the City of Gretna wants and is able to accommodate, identifying suitable locations for additional business and residential development, and guiding future growth to ensure that it enhances the character and quality of the city as a great place to live, work and play.

As Gretna continues to change and grow, many choices and decisions will need to be considered regarding:

- The extent to which the City will enforce the Comprehensive Plan recommendations as requirements rather than just suggestions.
- The implementation tools that will be most meaningful and useful to ensure that Comprehensive Plan recommendations are followed to the extent required.
- Determining how much future employment and residential growth the City wants to attract and accommodate, and the amount and type of land needed.
- Determining the public costs of additional growth and creating ways to pay for them.
- Deciding how to establish a defined, identifiable City edge that maintains Gretna's identity.
- Identifying and recommending locations that are suitable for future redevelopment with different uses and/or densities.
- Determining the proper balance between preserving the character and scale of existing developed areas and accommodating selective infill and redevelopment with different uses and/or higher densities.
- Determining the tools and techniques for encouraging and facilitating the recommended types of redevelopment.

## LAND USE DESIGNATIONS

Based upon the following land use categories and designation on the Future Land Use and Transportation Map (Figure 20), an analysis can be made of the amount of land for each major land use within Gretna's one-mile jurisdiction. This analysis is completed only to take a quick view of the entire planning jurisdiction of Gretna and how the uses are divided up. Although there is no scientific measure or ideal percentages, some comparison can be made to the area of land and uses within Gretna's corporate limits. The following calculations are based on the future land use designation for Residential, Commercial, and Industrial uses.

- Residential – 8,586 acres (includes Rural Estates)
- Commercial – 2,978 acres (includes Flex Space and Mixed Use categories)
- Industrial – 591 acres
- Other - 321 acres

There are 17 general land use categories used to define different use types, characteristics, and densities. These categories have been chosen to reflect the basic use and intensity to which land in the City and its jurisdiction is proposed to be developed. While the categories define land uses, they are intended to do so in a very general way; these land use categories are the basis for the zoning districts, but they are not the same as zoning districts. Any of a number of zoning districts may be appropriate in a single land use category.

## AG AGRICULTURE

The Agriculture land use area is intended to accommodate continued agriculture uses. These areas are generally located west along the Platte River Valley and other areas where development is restricted and unlikely. There is no Agriculture land use designation within Gretna's one-mile jurisdiction but is designated on land in the community's two- and three-mile growth areas. Characteristics of the AG category include:

- Location in areas outside the corporate limits where City services (water and sanitary sewer) will be difficult and/or costly to provide.
- Location in areas outside the corporate limits determined to have unique or sensitive natural areas, including stream corridors, tree stands, floodplain, wetlands, and natural habitat areas.
- Accessory buildings are allowed at an agricultural scale.
- Uses within this area include agricultural uses (except livestock feeding operations), wineries, single-family residential, parks/recreation/open space, and associated accessory uses.

### TA TRANSITIONAL AGRICULTURE

The Transitional Agriculture land use area is intended to accommodate continued agriculture uses while allowing for residential development. These areas are located outside the corporate limits where transition is appropriate between existing agriculture uses and expanding more dense development. There is no Transitional Agriculture land use designation within Gretna’s two- and three-mile growth areas. Characteristics of the TA category include:

- Location in areas outside the corporate limits where City services (water and sanitary sewer) will be difficult and/or costly to provide.
- Location in areas outside the corporate limits determined to have unique or sensitive natural areas, including stream corridors, tree stands, floodplain, wetlands, and natural habitat areas.
- Accessory buildings are at a scale between typical suburban development and farm buildings.
- Uses within this area include agricultural uses (except livestock feeding operations), wineries, single-family residential, churches, parks/recreation/open space, and associated accessory uses.

### RE RESIDENTIAL ESTATE

The Residential Estate land use area is intended to accommodate large and very large lot residential development. These areas generally are located along the edges of the community or in the one-mile zoning jurisdiction where land use type is less dense and intense, sharing similar qualities with neighboring rural land uses. Characteristics of the RE category include:

- Location in areas where City services (water and sanitary sewer) will be difficult and/or costly to provide.
- Location in areas determined to have unique or sensitive natural areas, including stream corridors, tree stands, wetlands, and natural habitat areas. The larger lots allow for greater potential to preserve nature through the use of conservation easements and common open space.
- Accessory buildings may be allowed at a scale between typical suburban development and farm buildings.
- The area will accommodate residential development at a density of one-half to two units per acre. Density is very dependent upon public utilities vs. private systems and the specific soil conditions of the properties.
- Uses within this area include single-family residential, churches, parks/recreation/open space, and associated accessory uses.

## LDR LOW DENSITY RESIDENTIAL

The Low Density Residential land use area is intended for typical suburban scale residential development densities. This category represents one of the most common residential land use type, and is located throughout town and in the one-mile zoning jurisdiction. Characteristics of the LDR category include:

- Locations throughout town to provide convenient access to transportation routes, commercial areas, jobs, schools, parks and recreation areas, and public services.
- Accessory structures should be limited in size to reinforce the pedestrian/human scale character of the neighborhood.
- Pedestrian connectivity will be important; the public sidewalk and trail system should provide adequate opportunities for residents to walk to destinations or for enjoyment.
- The area will include densities ranging from one to four dwelling units per acre.
- Uses within this area include single- and two-family residential dwellings, public and quasi-public uses, parks, group homes, and home occupations.

## MLDR MEDIUM-LOW DENSITY RESIDENTIAL

The Medium-Low Density Residential land use area is intended to provide higher residential densities than LDR, but still commonly found within urban neighborhoods. This area will also have a significant role as a transitional use between most commercial areas and lower density residential development. Characteristics of the MLDR category include:

- Locations throughout town where uses can serve as transitions that buffer and/or screen lower density residential uses from commercial uses and major streets.
- All areas should provide a mixture of housing styles, types, and occupancy levels in order to meet the housing needs and socioeconomic abilities of all residents.
- Neighborhood parks and open spaces should be included in all new developments and provided with access to the City's Trail System.
- Pedestrian connectivity will be important; the public sidewalk system should provide adequate opportunities for residents to walk to destinations or for enjoyment.
- The area will include densities ranging from three to 10 dwelling units per acre.
- Uses within this area include single- and two-family residential dwellings, public and quasi-public uses, group homes, and home occupations.

## MHDR MEDIUM-HIGH DENSITY RESIDENTIAL

The Medium-High Density Residential land use area is intended to accommodate denser residential development. This area would support apartment complex-types development, or a mixture of townhomes and apartments. The location of this area is intended to act as a buffer between more intensive commercial uses and lower density residential uses. Characteristics of the MHDR category include:

- Location where uses can serve as a transition between lower density residential areas and commercial uses.
- Location in areas adequately served by transportation facilities and near abundant employment opportunities.
- Opportunities for outdoor recreation and open space will be an important design element.
- Pedestrian connectivity with and between developments shall be required through use of the public sidewalk and trail systems, such pedestrian opportunities will compensate for the density of development.
- The area will include densities ranging from nine to 15 dwelling units per acre.
- Residential alternatives should be allowed, including units with varying numbers of bedrooms, and live/work units.
- Uses within the area include single-, two-, and multiple-family dwellings, with a focus on group homes, multiple-family and multiple-story structures.

## P/QP PUBLIC/QUASI-PUBLIC

The Public/Quasi-Public land use areas are intended to provide easy, convenient access for residents the common activities of daily life. However, the areas identified on the map tend to be already developed with uses specific to this category. The reason for this is that speculation with respect to future public and quasi-public uses can artificially inflate the underlying land value to the detriment of the City finances and community residents. In addition, not all existing or proposed public and/or quasi-public land uses are identified by way of Public/Quasi-Public Land Use designation since these uses are typically allowed outright or by conditional use in varying residential and commercial zoning districts. Characteristics of the P/QP category include:

- Locations dispersed throughout town, near activity centers and major streets.
- Locations that provide an opportunity to share facilities between uses, such as library and park or community center and post office.
- Uses within this area include public facilities, municipal properties, and schools.
- Structures should model appropriate architectural design elements, high-quality construction techniques, and appropriate materials and finishes.

## PRO PARKS, RECREATION, AND OPEN SPACE

The Parks, Recreation, and Open Space land use area accommodates those undeveloped properties that are intended to benefit the public by remaining undeveloped as open space or parks. However, many of the areas identified tend to be already developed with uses specific to this category. The reason for this is that speculation with respect to future public and quasi-public uses can artificially inflate the underlying land value to the detriment of the City finances and community residents. In addition, not all existing or proposed parks, recreation, and open space land uses are identified by way of Parks, Recreation, and Open Space Land Use designation since these uses are typically allowed outright or by conditional use in varying residential and commercial zoning districts. Characteristics of the PRO category include:

- Locations that are dispersed throughout the community for easy access, or are important and appropriate to the function served.
- Uses within this area include parks, passive and active recreation areas, ball fields, trails, and natural areas, as well as drainage and flood control structures such as detention or retention facilities, drainage swales, and floodplain areas.

## MU MIXED USE

The Mixed Use land use areas encompass all retail, office, service uses, business park, educational, and medium to high density residential uses. Commercial uses may vary widely in their intensity of use and impact, varying from low intensity offices to medium intensive uses such as convenience stores, restaurants, copy centers, fitness center, medical center, and educational campus. Characteristics of the MU category include:

- Located throughout town and in the one-mile zoning jurisdiction along arterial and collector transportation routes.
- Location where uses can serve as a transition between lower density residential areas and more intense commercial uses.
- Areas are developed as an overall site plan where interaction of uses is appropriate.
- Neighborhoods should be served by small-scale commercial developments, providing uses that serve the convenience and daily needs of nearby residents.
- Consideration should be given to diversity of uses at intersections, so competition of uses and redundancy is eliminated.
- Pedestrian scale and orientation will be an important design consideration for commercial and residential projects of all sizes.
- Pedestrian connectivity with and between developments shall be required through the use of the public sidewalk and trail systems. Such pedestrian opportunities will compensate for the density of development.
- The design and exterior surface treatments should reinforce existing development patterns; in newly developing areas, design themes should strengthen the overall image of the development consistent with the character of Gretna.
- Large-scale commercial developments should provide a mix of use types, including residential uses above the first floor, where appropriate.
- Landscaping, berms, fences, and setbacks should be used to screen and buffer commercial uses and parking lots from residential uses and transportation corridors; the scale of which should be appropriate to the relationship between the uses.
- Buildings shall be oriented along corridors so that parking and loading docks are directed away from public right-of-ways.
- Opportunities for outdoor recreation and open space will be an important design element, and public/quasi-public uses will be allowed.

## CBD CENTRAL BUSINESS DISTRICT

The Central Business District land use includes the area of Gretna considered the community's Original Downtown where businesses were generally located, including the existing municipal offices. The lots are usually small and the area offers higher pedestrian access. This land use area will encompass small, office, restaurant, and service uses. Characteristics of the CBD category include:

- Located in the original downtown, the intensity of particular uses suited to the character of the surrounding area.
- Neighborhood should be served by small-scale commercial developments, providing uses that serve the convenience and daily needs of nearby residents, while offering a destination cultural flare.
- Pedestrian scale and orientation will be an important design consideration for commercial businesses of all types. Pedestrian linkage of this area to other neighborhoods shall be incorporated through sidewalk and trail connections.
- The design and exterior surface treatments should reinforce existing development patterns consistent with the character of the area and of Gretna.
- Landscaping, fences, and walkways should be used to screen and buffer commercial uses from residential uses; the scale of which should be appropriate to the relationship between the uses.
- Uses within this area do not include those generally associated with big box stores, large open parking lots, or industrial uses, such as warehousing/distribution, manufacturing and production, etc.

## GC GENERAL COMMERCIAL

The General Commercial land use areas encompass all retail, office and service uses. General Commercial uses may vary widely in their intensity of use and impact, varying from low intensity offices, to more intensive uses such as gas stations, restaurants, grocery stores or automobile repair. Parking lots are usually shared by adjacent uses. Each area designated as general commercial in the land use plan may not be appropriate for every commercial zoning district. The appropriateness of a commercial district for a particular piece of property will depend on a review of all the elements of the Comprehensive Plan. Characteristics of the GC category include:

- Located throughout town, the intensity of particular uses suited to the character of the surrounding area.
- Larger, more intense commercial developments located nearer to major streets.
- Neighborhoods should be served by small-scale commercial developments, providing uses that serve the convenience and daily needs of nearby residents.
- Pedestrian scale and orientation will be an important design consideration for commercial projects of all sizes. Commercial areas shall be connected by residential neighborhoods through sidewalks and/or community trails.
- The design and exterior surface treatments should reinforce existing development patterns; in newly developing areas design themes should strengthen the overall image of the development consistent with the character of Gretna.
- Landscaping, berms, fences, and setbacks should be used to screen and buffer commercial uses from residential uses; the scale of which should be appropriate to the relationship between the uses.
- Uses within this area do not include those generally associated with big box commercial uses or industrial uses, such as storage, warehousing/distribution, manufacturing and production, etc.

## CR COMMERCIAL RETAIL

The Commercial Retail land use designation is intended for large-scale retail uses that serve a regional trade area and generate a significant amount of visitors. Due to the size of these developments, site design is very important. New development proposals should account for the following design considerations:

- Location on a highway and near Interstate due to high vehicle trip generation.
- Sites that are generally larger than 10 acres in size.
- Generally not compatible with low density residential neighborhoods.
- The design and exterior surface treatments should reinforce a common theme for the development, one that strengthens the overall image of the development consistent with the character of Gretna.
- Pedestrian scale and orientation will be an important design consideration, maximizing pedestrian access throughout the site.
- Uses within this CR land use include large retail establishments, unique and one-of-a-kind establishments, entertainment venues, and other uses that generate large volumes of traffic. Business parks, large recreational uses, and higher density residential uses are also appropriate as adjacent uses to these developments, as well as some lodging, service commercial uses and smaller retail stores.

## HC HIGHWAY COMMERCIAL

The Highway Commercial land use areas encompass all retail, office and service uses. Commercial uses may vary widely in their intensity of use and impact, varying from low intensity offices, specialty shops, and indoor storage to more intensive uses such as gas stations, restaurants, grocery stores, sales and service, or automobile repair. Each area designated as highway commercial in the land use plan may not be appropriate for every commercial zoning district. The appropriateness of a commercial district for a particular piece of property will depend on a review of all the elements of the Comprehensive Plan. Characteristics of the HC category include:

- Located throughout town along major corridors, the intensity of particular uses suited to the character of the surrounding area.
- Larger, more intense commercial developments located nearer to major streets.
- Neighborhoods should be served by small-scale commercial developments where appropriate, providing uses that serve the convenience and daily needs of nearby residents.
- Pedestrian scale and orientation will be an important design consideration for commercial projects of all sizes. Commercial areas should be connected to other neighborhoods where possible through sidewalks and/or community trails.
- The design and exterior surface treatments should reinforce existing development patterns; in newly developing areas design themes should strengthen the overall image of the development consistent with the character of Gretna.
- Landscaping, berms, fences, and setbacks should be used to screen and buffer commercial uses from residential uses; the scale of which should be appropriate to the relationship between the uses.
- Uses within this area do not include those generally associated with industrial uses, such as warehousing/distribution, manufacturing and production, etc.

## BP BUSINESS PARK

The Business Park land use designation refers to a development that will cater to large-scale employers, provide numerous office buildings and flexible configurations, and attract high-paying, professional jobs to Gretna. These areas are generally located along the edges of the community or in the one-mile zoning jurisdiction where the land use type is less dense and intense. Areas along Interstate 80 and Highway 6/31 will provide ideal Business Park access and visibility. The Business Park designation includes such developments as office parks, corporate campuses, and research and development parks.

- Office Park – Office Parks include small offices and service use enclaves which have similar but lower level intensity uses than a Corporate Campus. These uses generally do not require a high level of visibility or access.
- Corporate Campus – Corporate Campuses feature a mix of office types, including multi-tenant mid-rise offices, single-tenant corporate offices, medical and post-secondary research facilities, and office support services such as retail, restaurants, and hotels. A Corporate Campus is designed around an integrated master plan. Visibility and access are very important.
- Research and Development Park – Research and Development Parks provide for research and development facilities, light manufacturing, and office/warehouse uses that form the backbone of corporate campus uses. Visibility is important, but excellent access is necessary, especially for large truck traffic. Large distribution centers, warehouses, truck terminals, and outdoor storage are inappropriate in a Research and Development Park.

Characteristics of the BP category include:

- Located on the edge of town and in the one-mile zoning jurisdiction, the intensity of particular uses suited to the character of the surrounding area.
- Larger, more intense commercial developments located nearer to major streets or the Interstate.
- Pedestrian attributes, including trails, will be an important design consideration for projects to tie into the adjacent properties and the community.
- The design and exterior surface treatments should reinforce existing development patterns; in newly developing areas design themes should strengthen the overall image of the development consistent with the character of Gretna.
- Large-scale commercial developments should provide a mix of use types, including residential uses above the first floor, where appropriate.
- Landscaping, berms, fences, and setbacks should be used to screen and buffer commercial uses from residential uses; the scale of which should be appropriate to the relationship between the uses.
- Uses within this area do not include those generally associated with industrial use, such as warehousing/distribution, manufacturing and production, etc.

## FS FLEX SPACE

The Flex Space land use area is intended to accommodate commercial uses and smaller, less intensive industrial uses. Such uses may vary from agriculture retail, service uses, and self storage, warehousing, and employment centers. Location is important, as proximity to major streets can help ensure heavy traffic will avoid residential areas and prominent pedestrian activity centers. Characteristics of the FS category include:

- Locations that cater to the specific needs of the user, providing a level of water, sewer, and electrical capacity, closeness to major transportation routes, and lot sizes necessary to accommodate initial development and potential future expansions. Visibility for flex space uses may be more important than industrial land use designations.
- Uses shall emit a minimal amount of noise, odor, waste, and other operational by-products.
- Significant landscaping and buffering should be used to screen Flex Space uses from view of nearby residential areas, other conflicting land uses and important view corridors.
- The design and exterior surface treatments should reinforce existing development patterns; in newly developing areas design themes should strengthen the overall image of the development consistent with the character of Gretna.
- Uses within this area include agriculture retail, service areas, equipment sales and service, automobile repair, warehousing, distribution, manufacturing, and production companies as well as office parks and employment centers. Offices, restaurants, grocery stores, motels and similar uses are not permitted in the Flex Space Land Use District.
- Uses shall have fully enclosed facilities without any outside storage.

## LI LIGHT INDUSTRIAL

The Light Industrial land use area is intended to accommodate less intensive or lighter industrial uses. Location is important, as proximity to major streets and railroad can help ensure heavy traffic avoids residential areas and prominent pedestrian activity centers. Characteristics of the LI category include:

- Locations that cater to the specific needs of the user, providing a level of water, sewer, and electrical capacity, closeness to major transportation routes, and lot sizes necessary to accommodate initial development and potential future expansions.
- Uses shall emit a minimal amount of noise, odor, waste, and other operational by-products.
- Significant landscaping and buffering should be used to screen Light Industrial uses from view of nearby residential areas, other conflicting land uses and important view corridors.

## LI LIGHT INDUSTRIAL (CONT.)

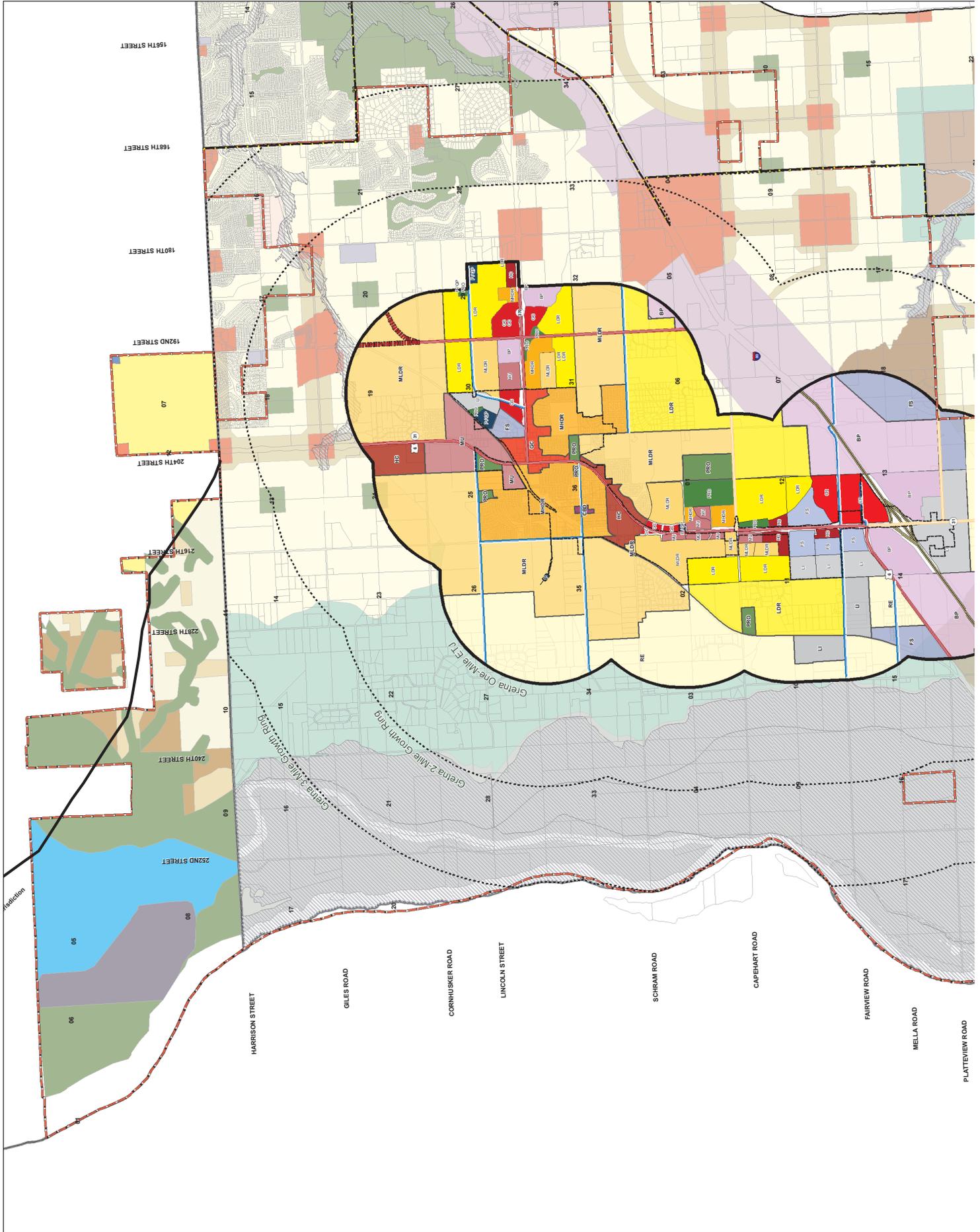
- The design and exterior surface treatments should reinforce existing development patterns; in newly developing areas design themes should strengthen the overall image of the development consistent with the character of Gretna.
- Uses within this area include warehousing, distribution, light manufacturing, production companies, and employment centers.
- Outside storage may be allowed and controlled in the Light Industrial Districts through use of conditional use permits.

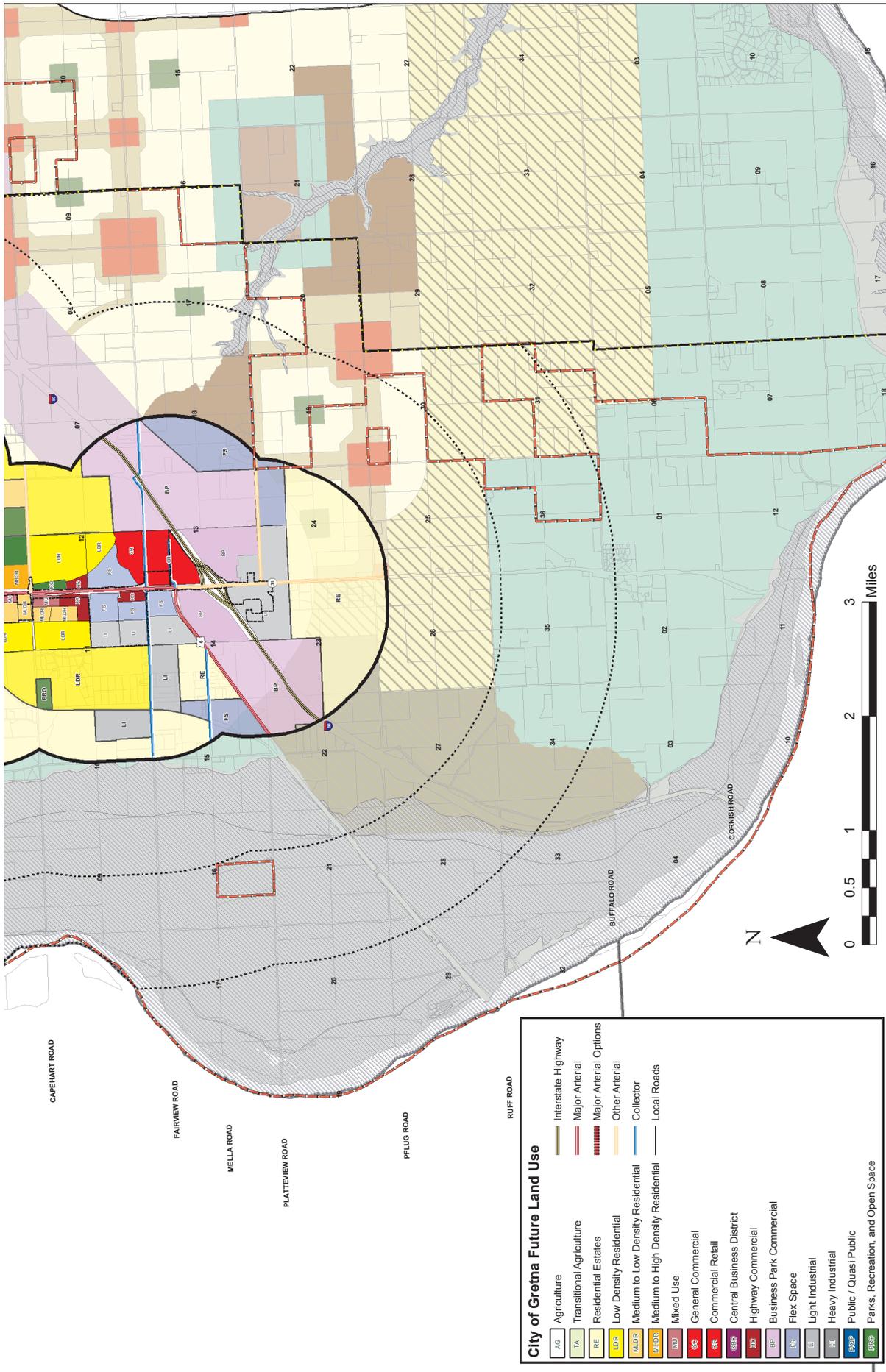
## HI HEAVY INDUSTRIAL

The Heavy Industrial land use area is intended to accommodate larger, more intensive industrial uses. Location is important, as proximity to major streets can help ensure heavy traffic avoids residential areas and prominent pedestrian activity centers. Careful consideration shall be given before designation of any heavy industrial uses so as not to encroach upon or conflict with less intrusive uses or destroy important view corridors. Characteristics of the HI category include:

- Locations that cater to the specific needs of the user, providing a level of water, sewer, and electrical capacity, closeness to major transportation routes, and lot sizes necessary to accommodate initial development and potential future expansions.
- Uses shall emit a minimal amount of noise, odor, waste, and other operational by-products or take measures to contain such impacts in-site.
- Significant landscaping and buffering should be used to screen Heavy Industrial uses from view of nearby residential areas, other conflicting land uses, important view corridors, major streets, and pedestrian activity centers; certain use components should be screened from view off-site, such as delivery and pick-up areas, outdoor storage, and trash receptacles; fences should not be used alone to provide screening.
- The design and exterior surface treatments should reinforce existing development patterns; in newly developing areas design themes should strengthen the overall image of the development consistent with the character of Gretna.
- Uses within this area include warehousing, distribution, manufacturing, and production companies.
- Outside storage is permitted and maximum height requirements for structures may be significantly higher than other land use districts.

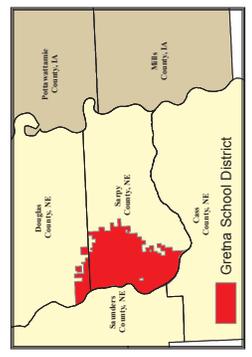
FIGURE 20: GRETTA FUTURE LAND USE AND TRANSPORTATION MAP





### City of Gretna Future Land Use

AG	Agriculture	Interstate Highway
TA	Transitional Agriculture	Major Arterial
RE	Residential Estates	Major Arterial Options
LDR	Low Density Residential	Other Arterial
MDR	Medium to Low Density Residential	Collector
MDR	Medium to High Density Residential	Local Roads
MDR	Mixed Use	
GC	General Commercial	
CR	Commercial Retail	
CB	Central Business District	
HC	Highway Commercial	
BP	Business Park Commercial	
FS	Flex Space	
LI	Light Industrial	
HI	Heavy Industrial	
QP	Public / Quasi Public	
RS	Parks, Recreation, and Open Space	



### Legend

- Existing Interlocal Agreement
- Gretna Public Schools - District Boundary
- One-Mile Extraterritorial Jurisdiction
- Two / Three-Mile Growth Areas
- Gretna Corporate Limits
- Parcel Boundaries
- Existing Floodplain

### Douglas County / Omaha Current Future Land Use

LD	Low Density Residential
UR	Urban Residential
ED	Existing Residential Development
EC	Environmentally Constrained
CM	Community Mixed Use Area
NM	Neighborhood Mixed Use Area
CH	Community Center
PR	Preserve
RE	Resource Extraction

### Sarpy County Current Future Land Use

CR	Conservation Residential
ER	Estate Residential
UR	Urban Residential
OR	Other Residential II
LR	Long Term Residential Growth
BP	Business Park
MI	Mixed Use
MC	Mixed Use Center
PI	Plug In/Exchange Development
LI	Light Industrial/Storage
IN	Industrial
CH	Community Center
PS	Park School Site
GR	Greenway

### City of Gretna Sarpy County Nebraska Future Land Use and Transportation Map Including School District 77-0037

Created by J. Sahar, Planning Director, City of Gretna, 12/14/14  
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# City Beautification

- City Beautification Components
- Special Character Areas



# City Beautification

City beautification refers to more than making sure the community is aesthetically appealing; the City should also be inviting, functional, and enjoyable. City beautification is an effort to reinforce the City's identity and character by maximizing the impact of visual enhancements and developing a meaningful, unified approach to development and growth throughout Gretna. The different components that may be combined to produce a thematic, cohesive and attractive Gretna include:

- **Forms, colors, and materials** – A common design vocabulary will be important to developing a strong connection between community elements. This design vocabulary should reinforce a connection to Gretna through the use of patterns, textures, and visual elements derived from the community's identity, history, location, and vision.
- **Signage** – Signage is one of the most prominent elements in a city landscape. A comprehensive sign program can establish visual unity, hierarchy, and interest. The image created at the community's entrance points should be carried through the community. Banners along entrance corridors or within designated commercial areas will unify a community under a common theme. Signage types should be reviewed in order to ensure an appropriate scale, quantity, and style of signage in key community locations, such as entrance corridors, public squares, and important view sheds.
- **Streetscape** – Streetscape elements offer another method of creating interest in different areas of town by changing various characteristics. For example, street medians, planters, decorative light poles, crosswalks, fences and varying paving textures and colors can be used to delineate different neighborhoods, pedestrian pathways, trails, and development sites. Creativity should be encouraged within a common form, color, and material palette to maintain a common community image.
- **Landscape** – Gretna's setting within the Omaha Metropolitan Area is unique due to its closeness to agricultural land, open, natural spaces, and the Platte River. These genuine, ecological areas provide a landscape setting that differs from communities north and east of Gretna. A public Right-of-Way landscape plan could reinforce the natural area by utilizing common plant species for street trees and median plantings, community entrances, special seasonal interest plantings, and reinforcing scenic views.
- **Public Art** – Gretna can introduce a community theme by incorporating art into public projects or at designated community locations. Such Public art can be incorporated in various ways, including sculpture installations, painting hard surfaces in abstract patterns, and utilizing tile or metal inlay patterns in concrete surfaces.

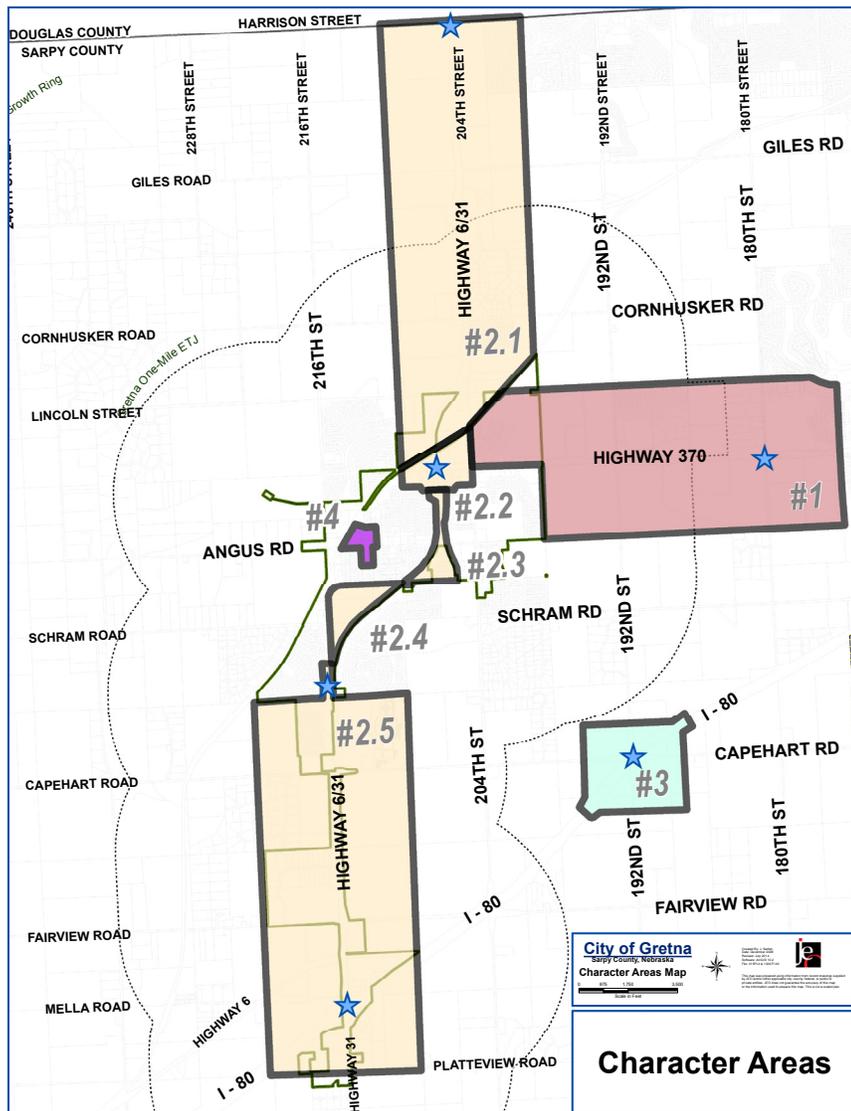
Community beautification programs for Gretna should be developed consistent with the Special Character Areas. Through developing a comprehensive visual vocabulary, Gretna will be able to identify and assess those characteristics that mean the most its residents. Within these Special Character Areas, the base design vocabulary can be modified to represent the particular area characteristics, when appropriate.

## SPECIAL CHARACTER AREAS

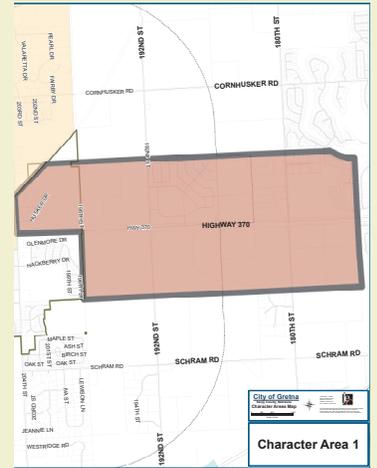
Character Areas are areas within Gretna that have a unique and recognizable character that is different from other places in the community. The character differences may be the result of topography, age and style of structures, relationships among the built environment, land use patterns, landscaping, street patterns, open space, or streetscapes. This Plan will present several different areas within Gretna that do or could have distinctive characteristics that give them their own particular quality. Planning for character can provide a way to maintain the community quality that is so important to the residents of Gretna; it is one more tool that can be used to plan for a future Gretna that is just as special as the current Gretna.

Character generally defines the look or feel of a place – a look or feel that sets it apart from other areas. The area characteristics identified here are intended to define, maintain, or enhance a desired character for the area. These characteristics go beyond mere design and involve more than looking at just the physical layout of development or the amount of open space provided. Character areas define geographical areas of different size, each reinforced through a common setting, land use pattern or character of development. The Character Areas defined in this Plan include:

- **Character Area 1: Highway 370 Corridor**
- **Character Area 2: Highway 6/31 Corridor**
  - Character Area 2.1
  - Character Area 2.2
  - Character Area 2.3
  - Character Area 2.4
  - Character Area 2.5
- **Character Area 3: Interstate 80 Corridor**
- **Character Area 4: Original/Downtown**
- **Character Area 5: Community Entrances**  
(Blue Stars located around Gretna)



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# Character Area 1: Highway 370 Corridor

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## Highway 370 Corridor

### AREA DEFINITION:

Character Area 1 is generally defined as the one-mile wide Highway 370 Corridor extending east from 204th Street to 180th Street.

### INTENT:

Highway 370 is a major thoroughfare connecting the cities of Gretna, Papillion, and Bellevue. As a result of increased use and congestion, the highway was extended to four lanes in 2013 and will eventually be expanded to six lanes. The additional lanes will accommodate higher traffic flows and will improve the connectivity in the area. This expansion will also foster concerns for aesthetics and pedestrian movement.

The highway expansion has provided infinite opportunities for development. The Area is primarily comprised of undeveloped land that will be in high demand as the Omaha Metro area grows. Figure 21 provides for the Highway 370 corridor master plan. The plan should direct what type of development should occur and where. The quality and character of the new development in this area should be directed by the following recommendations.

### COMPATIBLE USES:

#### Commercial

- Retail
  - Locally Owned businesses
  - National and Regional Chains
- Restaurants
- Office
  - Professionals (Attorneys, physicians, dentists, architects/planners/engineers, accountants)
  - General
- Commercial Parks
- Business Parks, including medical facilities

#### Residential

- Single-family detached
- Townhomes
- Condominiums
- Apartments

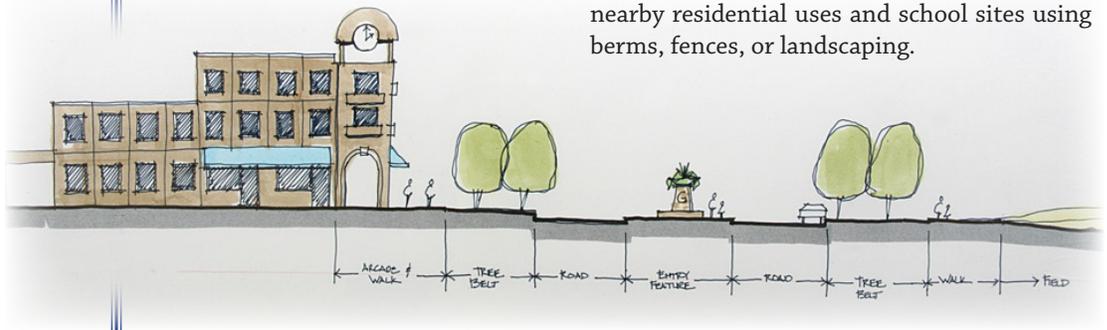
#### Mixed Uses

- Commercial on main level
- Office/Residential on second level
- Residential on additional levels

#### Public/Civic

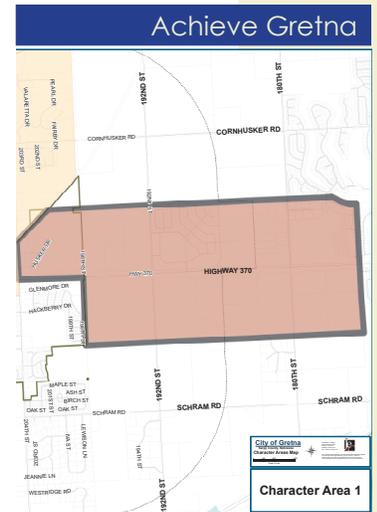
- Governmental Facilities
- Libraries
- Recreation/Community Centers
- Parks/Trails

Commercial uses should be **screened** from nearby residential uses and school sites using berms, fences, or landscaping.



## POLICIES:

1. Developments within this Character Area should contain a mixture of uses including commercial, office, and medium- to high-density residential.
  - Any development of 160 acres or more shall consist of a mix of commercial/retail/office uses (of approximately 50%) and residential developments approved by the City of Gretna and shall include sufficient landscaping along both the highway frontage and interior of the development.
  - The total land coverage for commercial/retail/office area may be increased when residential is a part of a multi-story structure.
  - All developments in this character area, regardless of size, should consider mixed use in their design and avoid repetitive uses on adjacent properties.
  
2. All residential uses within this Character Area should have greater setbacks from any highway/arterial unless a high-density development is proposed. For access purposes, higher density developments should be located closer to the highway/arterial. The intent of this Character Area regarding Medium to High Density Residential is to vary such developments (apartment, townhomes, condominiums) in different locations along the corridor.
  - Exception: Mixed-use, multi-story developments in which residential is integrated into commercial/office development.
  
3. Green Space requirements:
  - A ten-foot landscape buffer along any highway or major arterial in addition to any required yard setbacks. Such buffer should be designated adjacent to the right-of-way.
  - An additional 8% of the entire development site should be allocated for green space in any non-residential development. This green space can include:
    - Required green space within parking areas
    - Internal trails systems
    - Natural drainageways
    - Internal boulevard systems
    - Green space at the intersection of highways and major arterials
  - Parking requirements for commercial/office/industrial:
    - No more than two drive lanes should be allowed without an intervening green/landscape median/island. Three or four drive lanes shall require such median/island of at least five feet in width for adequate vegetative growth.
  
4. All developments shall follow storm water policies adopted by the City of Gretna.
  - Use of sustainable storm water management practices and maintenance of natural drainage patterns, incorporating water courses into the design of neighborhoods and business park features.
  
5. Developments along and adjacent to 192nd corridor shall allow for such arterial designation and designation of public right-of-way and increased setbacks shall be made part of the platting process.





**Landscaping** should be provided in naturalistic groupings rather than in a linear manner.



Developments on the north and south sides of Highway 370 should provide **walkable environments**, including sidewalks, crosswalks, and bridges, or underpasses in conjunction with drainage ways to provide for pedestrian accessibility.

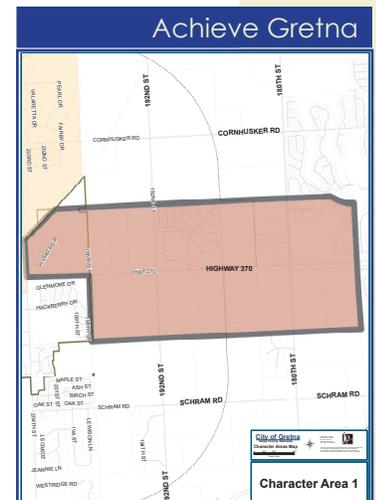
All site design elements should be integrated and relate to one another as well as to the overall character of Gretna. Site and building design should **enhance the natural environment** and integrate with natural features, such as topography, trees, view sheds, etc.

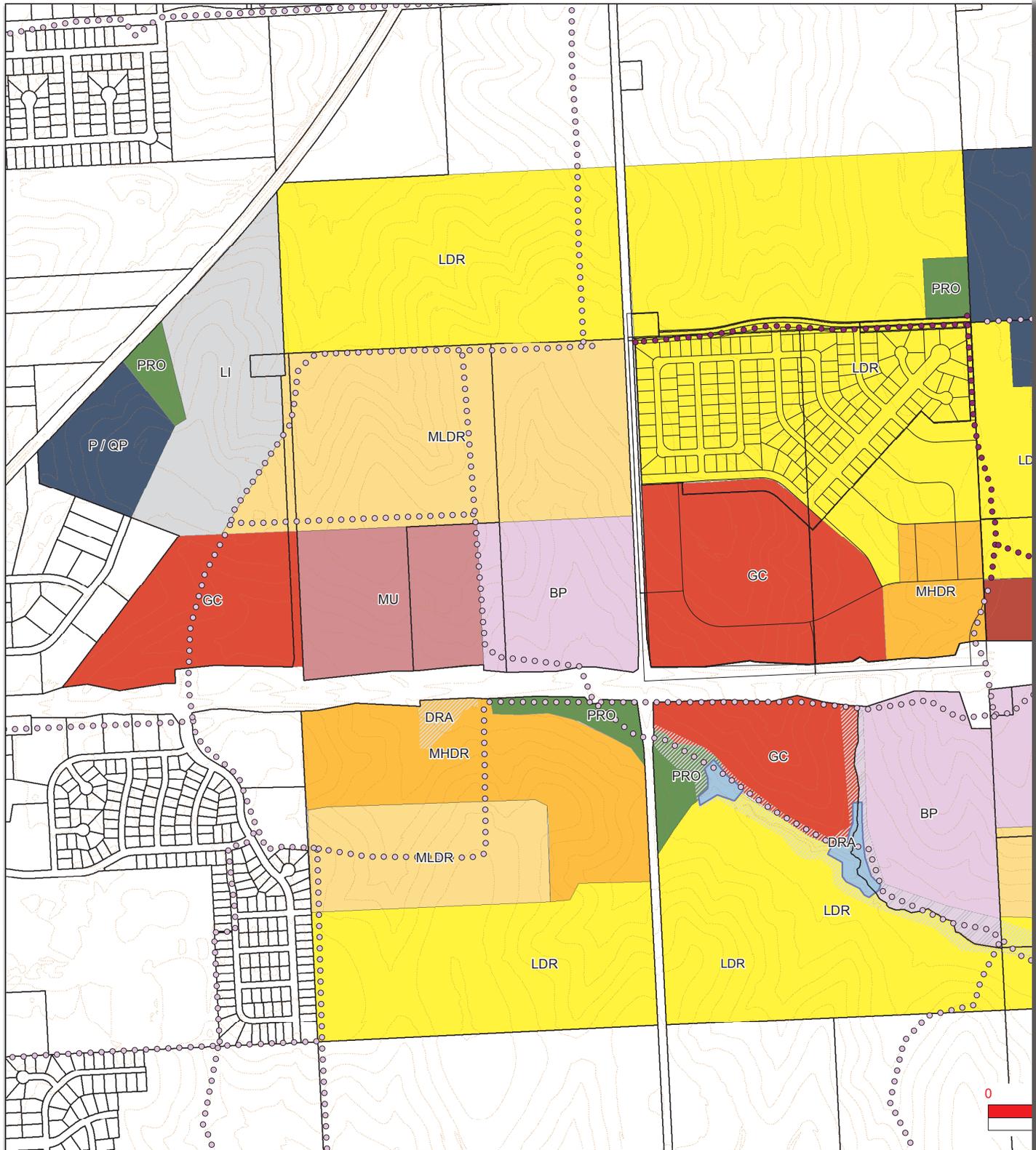
Stormwater facilities should appear **natural** through the use of appropriate landscaping and naturalistic shapes.



## DESIGN STANDARDS:

1. Brick, masonry, and tile materials should be the primary building material palette of choice
  - Integration of materials is encouraged (stucco, ironwork, glass, etc.)
  - Metal and pre-fabricated buildings are strongly discouraged unless designed to meet the character of the area
2. Façades shall consider views of buildings from all public right-of-ways.
3. Developments should be designed to a human scale
  - The street-facing façade should be penetrable. It should include windows and openings to allow a connection between the building interior and the sidewalk.
  - Shopping and dining areas should be oriented toward and friendly to the pedestrian rather than the automobile.
  - Awnings should be placed on the main level to provide shelter and protection for pedestrians on the sidewalk.
  - Pedestrian and bicycle networks should take precedence over the automobile.
4. Mixed-Use and New Urbanist Developments are encouraged. Large strip commercial development should be avoided unless innovative in design.
5. Public spaces should be incorporated into all developments. These spaces can include plazas, pedestrian corridors, seating areas, and more.
6. Parking areas should be designed to include natural landscaping and softer surfaces that break the expanse of hard paving. They should be located away from the street right-of-way or behind buildings and should be well-screened.
7. Landscaping designs should include trees and vegetation that will enhance the Area. Proper plant selection and placement can create buffers between uses, visual screens, and pedestrian protection
8. A variety of paving materials and textures should be utilized to designate significant paved areas. (i.e. crosswalks, sidewalks, etc.)
9. Lighting should be decorative in nature and reflect the character of the Area.
10. Wayfinding Signage should be designed to reflect the character of the Area and be strategically located to avoid interference with traffic safety.
11. Signage should be consolidated and limited to those necessary to identify tenants within the development and direct traffic. Such signage should be low profile/monument style signs.
12. Earthen berms and/or plantings shall be incorporated into site plans to increase aesthetics and reduce negative visual impacts of parking lots and structures from public right-of-ways and residential areas.
13. Lighting from commercial and retail developments shall not unreasonably emanate into residential areas so as to create a nuisance.



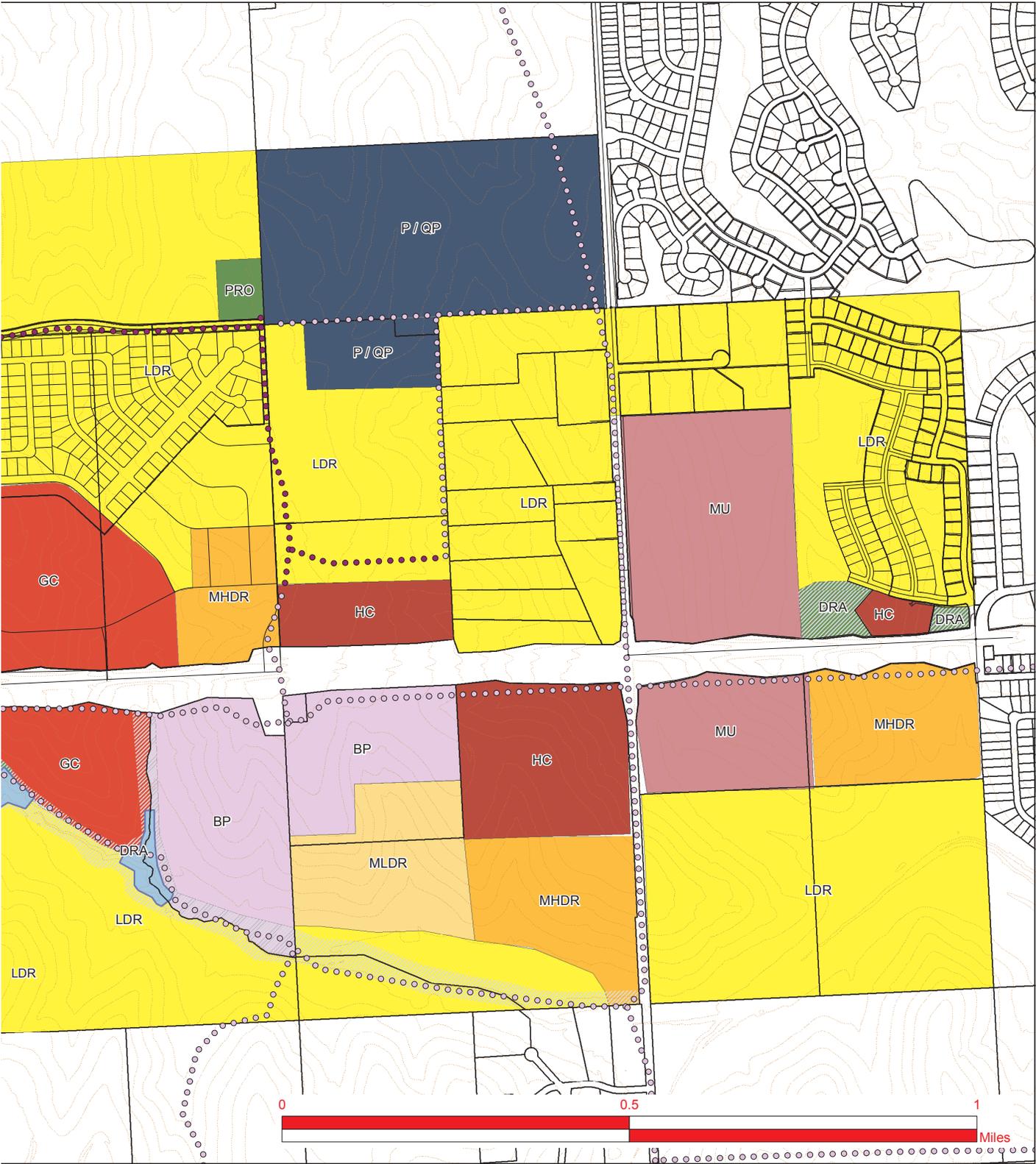


Legend			Trail Type
<span style="color: red;">■</span> General Commercial (GC)	<span style="color: blue;">■</span> Flex Space (FS)	<span style="color: green;">■</span> Parks, Recreation, and Open Space (PRO)	●●● Existing Trail
<span style="color: brown;">■</span> Highway Commercial (HC)	<span style="color: lightgrey;">■</span> Light Industrial (LI)	<span style="color: yellow;">■</span> Low Density Residential (LDR)	○●○ Future Trail
<span style="color: orange;">■</span> Commercial Retail (CR)	<span style="color: darkblue;">■</span> Public / Quasi Public (P/QP)	<span style="color: lightorange;">■</span> Medium to Low Density Residential (MLDR)	..... Ten Foot Contours
<span style="color: purple;">■</span> Mixed Use (MU)	<span style="color: lightblue;">■</span> Stormwater Retention (SRA)	<span style="color: orange;">■</span> Medium to High Density Residential (MHDR)	
<span style="color: pink;">■</span> Business Park Commercial (BP)	<span style="color: grey;">■</span> Drainage Retention Area (DRA)		

**Gretna**

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**Highway 37**



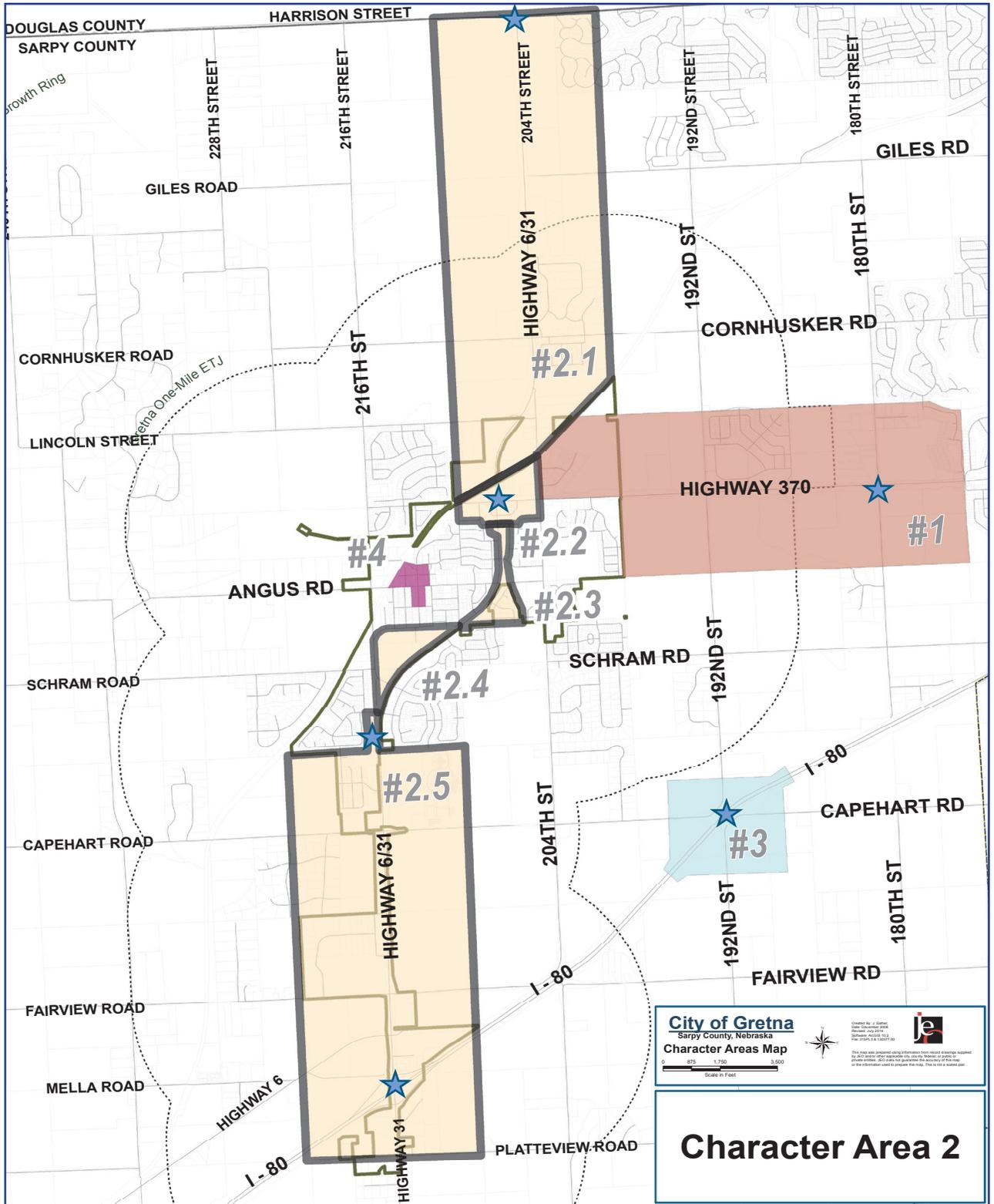
# Gretna, Nebraska

## Highway 370 Corridor Study

Created By: MBG  
 Date: Feb 2014  
 Revised: 12/9/2014  
 Software: ArcGIS 10.2  
 File: 140271.00



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**City of Gretna**  
 Sarpy County, Nebraska  
**Character Areas Map**

0 875 1,750 3,500  
 Scale in Feet

**Character Area 2**

## Character Area 2: Highway 6/31 Corridor

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## Highway 6/31 Corridor

### AREA DEFINITION:

Character Area 2 follows Highway 6/31 from the Interstate interchange north to the county line. A large part of this area encompasses existing developments.

### INTENT:

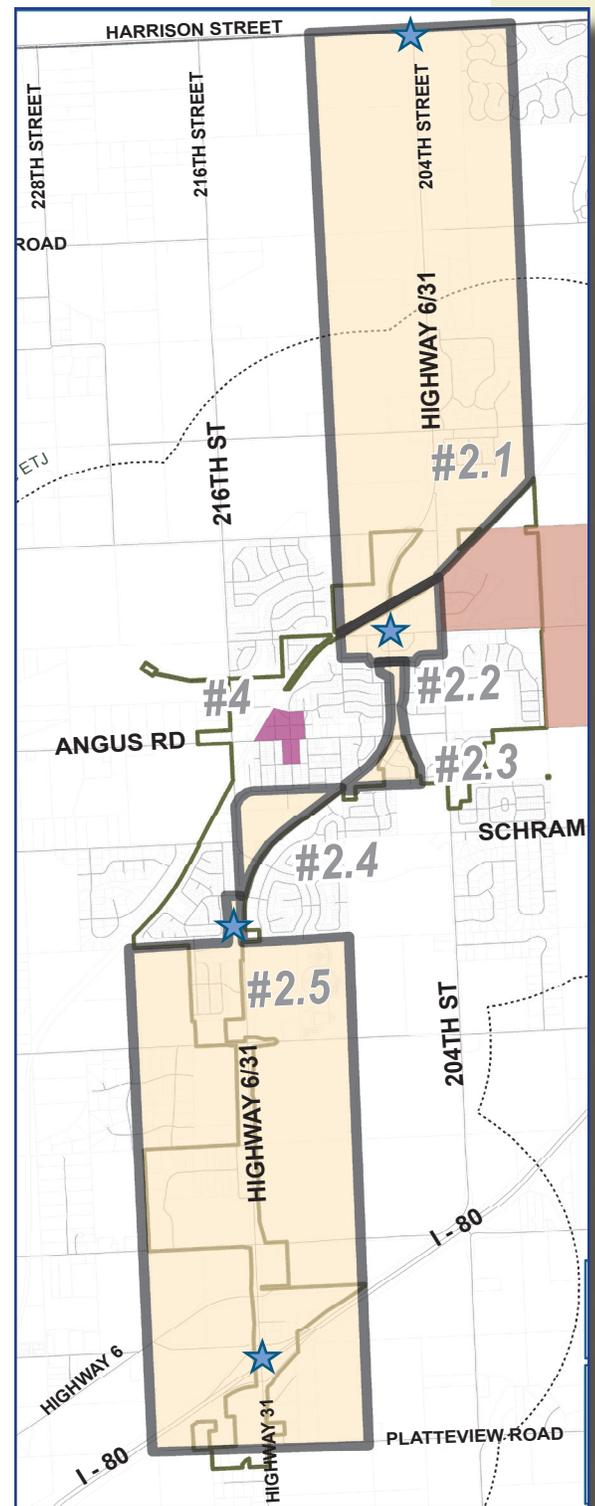
The intent of Character Area 2 is to create a cohesively designed highway corridor through the City of Gretna. This landscape and streetscape elements will provide both pedestrians and automobiles with a positive experience and image of the City. Because some areas of this corridor have already been developed with individual characters, sub-districts have been designated within this character area.

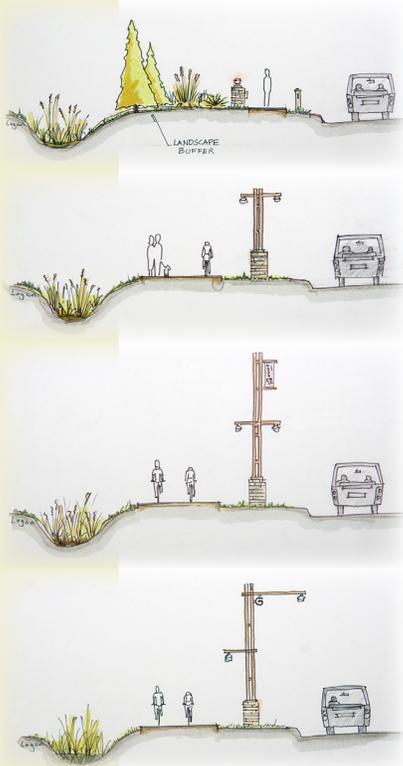
**POLICIES:** (See individual character zones).

### DESIGN STANDARDS:

1. Brick, masonry, and tile materials should be the primary building material palette of choice
  - Integration of materials is encouraged (stucco, ironwork, glass, etc.)
  - Metal and pre-fabricated buildings are strongly discouraged unless designed to meet the character of the area
2. Façades shall consider views of buildings from all public right-of-ways.
3. Developments should be designed to a human scale.
  - The street-facing façades should be penetrable. It should include windows and openings to allow a connection between the building interior and the sidewalk.
  - Shopping and dining areas (including sidewalk cafes) should be oriented toward and friendly to the pedestrian rather than the automobile.
  - Awnings should be placed on the main level to provide shelter and protection for pedestrians on the sidewalk.
4. Mixed-Use and New Urbanist Developments are encouraged. Strip commercial development should be avoided.
  - The integration of uses will provide places to live, work, and shop within the same development. This will contribute to the creation of a lifestyle center.
5. Public spaces should be incorporated into all developments. These spaces can include plazas, parks, pedestrian corridors, seating areas, and more.
6. Parking areas should be designed to include natural landscaping and softer surfaces that break the expanse of hard paving. They should be located away from the street right-of-way or behind buildings and should be well-screened.
7. Landscaping designs should include trees and vegetation that will enhance the Area. Proper plant selection and placement can create buffers between uses, visual screens, and pedestrian protection
8. A variety of paving materials and textures should be utilized to designate significant paved areas. (i.e. crosswalks, sidewalks, etc.)

9. Lighting should be reflective of the character of the Area.
  - Lighting should cater to both pedestrian and automobile traffic
  - Lighting design should allow for connectivity by contributing to a common theme throughout the corridor
  - Lighting shall not unreasonably emanate into residential areas.
10. Wayfinding Signage should be designed to reflect the character of the Area and be strategically located to avoid interference with traffic safety.
11. Signage should be consolidated and limited to those necessary to identify tenants and direct traffic. Such signage should be low monument style or grand base type.
12. Development should incorporate green/sustainable strategies.
  - Existing structures should be revitalized and reused rather than demolished.
  - Pedestrian and bicycle networks should take precedence over the automobile.
  - Natural habitat and drainage and waterways should be preserved and measures to ensure their conservation should be pursued.
  - Buildings should employ energy efficient design.
13. Development sites should be interconnected in order to minimize the need to use the arterial street system to travel between neighboring projects. Access breaks along the roadway should be minimized.
14. Retail establishments, professional and general office space, and other commercial uses should be encouraged to maintain high design and aesthetic standards for the buildings, accessory structures, and signage.



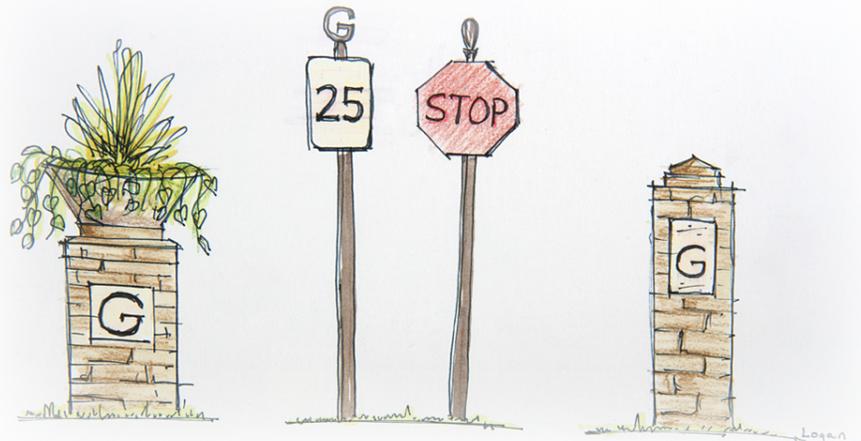


**Unbroken travel lanes** allow drivers time to adjust to the changing character of the roadway before they encounter heavier traffic. **Access points** should be controlled carefully, paying attention to speed and sight distance.

**Scale** will be an important overall design element. Because motorists are moving faster than they will in town, larger objects and spacing that can be recognized and understood easily must be used, i.e. wider landscape strips are preferred, trees should be used rather than shrubs, easily read direction signage should be incorporated.

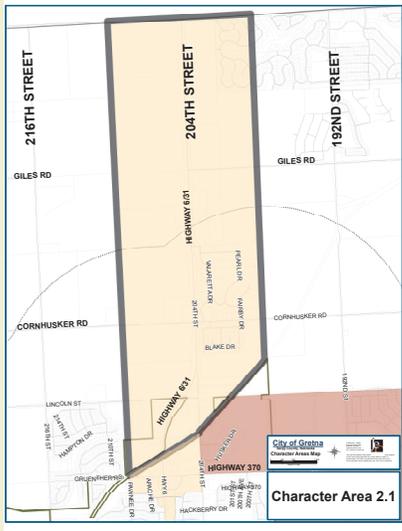
Space should **transition** gradually from wide-open rural areas to built-up communities; funneling refers to designing the corridor with wider open spaces, larger setbacks, and more dispersed landscaping farthest from the City, then allowing elements to condense as the motorist gets closer. This design strategy can also influence drivers to reduce their speed.

**Signage** design should reflect overall community standards. Signage size is important; very large signs and billboards quickly detract from the aesthetics of the corridor and shall only be utilized in conjunction with higher speeds of traffic such as the Interstate. Similarly, a large number of signs can be confusing to motorists unfamiliar with the community. Low, monument style signs should be utilized, and signage should be consolidated to few locations along the roadway.





# Highway 6/31 Corridor 2.1



## AREA DEFINITION:

Character Area 2.1 is approximately one half mile east and west of Highway 6/31 from the railroad north to Harrison Street.

## INTENT:

Highway 6/31 is currently the primary access into Gretna from the north. As the City of Omaha and the Metro area grow, development along this corridor will be prevalent.

## POLICIES:

1. Developments within this Character Area should contain a mixture of uses including commercial, office, flex space, and low- to medium-density residential.
2. All residential uses within this Character Area should have greater setbacks from any highway/arterial.
  - Exception: Mixed-use, multi-story developments in which residential is integrated into commercial/office development.
3. Green Space requirements:
  - A ten-foot landscape buffer along any highway or major arterial in addition to any required yard setbacks. Such buffer should be designated adjacent to the right-of-way.
  - An additional 8% of the entire development site should be allocated for green space in any non-residential development. This green space can include:
    - Required green space within parking areas
    - Internal trails systems
    - Natural drainage ways
    - Internal boulevard systems
    - Green space at the intersection of highways and major arterials
      - Parking requirements for commercial/office/industrial:
        - No more than two drive lanes should be allowed without an intervening green/landscape median/island that shall be at least five feet wide.

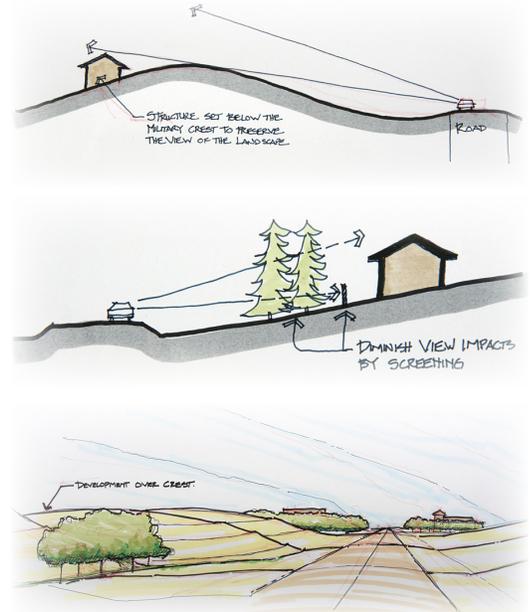
## COMPATIBLE USES:

- |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                                                                                                                                                                                                                                                                                                                                                                         |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p><b>Commercial</b></p> <ul style="list-style-type: none"> <li>• Retail                             <ul style="list-style-type: none"> <li>- Locally Owned businesses</li> <li>- National and Regional Chains</li> </ul> </li> <li>• Restaurants</li> <li>• Office                             <ul style="list-style-type: none"> <li>- Professionals</li> <li>- General</li> </ul> </li> <li>• Commercial Parks</li> <li>• Business Parks</li> </ul> <p><b>Public/Civic</b></p> <ul style="list-style-type: none"> <li>• Governmental Facilities</li> <li>• Libraries</li> <li>• Recreation/Community Centers</li> <li>• Parks/Trails</li> </ul> | <p><b>Residential</b></p> <ul style="list-style-type: none"> <li>• Single-family detached</li> <li>• Townhomes</li> <li>• Condominiums</li> <li>• Apartments</li> </ul> <p><b>Mixed Uses</b></p> <ul style="list-style-type: none"> <li>• Commercial on main level</li> <li>• Office/Residential on second level</li> <li>• Residential on additional levels</li> </ul> |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

## ADDITIONAL DESIGN STANDARDS:

The design standards for this Character Area should be similar to those of Character Area 1. These two areas are and will be comprised of residential and commercial development.

In addition, earthen berms and/or plantings shall be incorporated into site plans to increase aesthetics and reduce negative visual impacts of parking lots.



# Highway 6/31 Corridor 2.2

## AREA DEFINITION:

Character Area 2.2 is defined as the area south of the BNSF railroad tracks, north of Glenmore Drive, and between Pawnee Drive and 204th Street.

## INTENT:

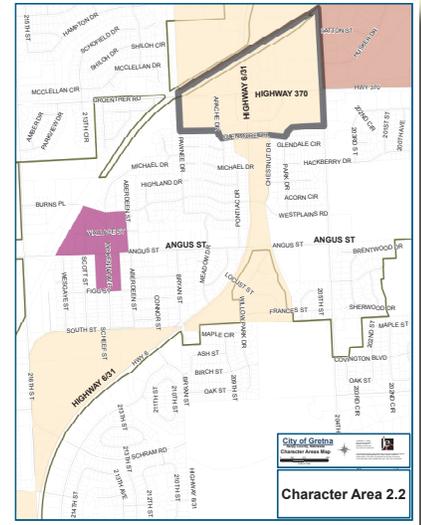
Highways 6/31 and 370 are both prominent corridors within the City of Gretna. Their intersection, consequently, will play a dominant role in the creation of a City identity. The image of this intersection will provide both visitors and residents with an impression of the City that will remain in their minds. The development in this area should tell those approaching they have reached a destination worth noticing.

## POLICIES:

The development/redevelopment should include pathways between parking, various uses and the public areas to create a friendly walkable area.

## ADDITIONAL DESIGN STANDARDS:

- Gateway and Community Entrance elements should be included to designate this intersection as a prominent part of the City.
- Public Art should be included as a part of the streetscape design. Public Art helps enliven the street life and pedestrian experience.
- Design of building shall consider view from all public right-of-ways and residential properties.



### Human Scale

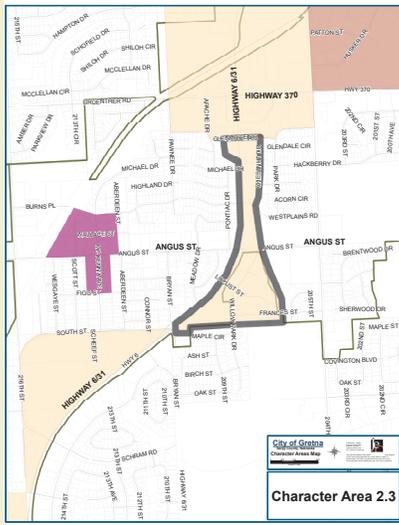
“Man is the measure of all things”  
--Protagoras of Abdera (c. 480-410 B.C.)

Human Scale refers to the perceived relationship between the size or mass of an object as compared to a human. Generally the term is used to describe a development pattern characterized by an integration of design and function in the pedestrian realm, providing vitality and safety through the treatment of entrances, lighting, and spatial arrangement.

## COMPATIBLE USES:

- Commercial
- Retail
    - Locally Owned businesses
    - National and Regional Chains
  - Restaurants
  - Office
    - Professionals (Attorneys, Architects/Planners/Engineers, Accountants)
    - General
  - Commercial Parks
- Public/Civic
- Governmental Facilities
  - Institutional/Educational
  - Libraries
  - Recreation/Community Centers
  - Parks/Trails
  - Water Features
  - Public Art

# Highway 6/31 Corridor 2.3



## AREA DEFINITION:

Character Area 2.3 is defined as the land east of Highway 6/31, bound by Glenmore and Chestnut Streets and the natural drainage ways south of Angus Street.

## INTENT:

A majority of the land in this area along Highway 6/31 has already been developed. The guidelines in this section will play an important role in the development of a cohesive streetscape and redevelopment of the area. The corridor theme should be continued through the area, providing a positive pedestrian experience.

## POLICIES:

1. Development within the area should allow for commercial/office, public/civic, and high density residential.
2. Frontage roads should be considered to lessen congestion.

## ADDITIONAL DESIGN STANDARDS:

1. Design of building shall consider view from all public right-of-ways and residential properties.

### COMPATIBLE USES:

#### Commercial

- Retail
  - Locally Owned businesses
  - National and Regional Chains
- Restaurants
- Office
  - Professionals (Attorneys, Architects/Planners/Engineers, Accountants)
  - General
- Commercial Parks

#### Residential

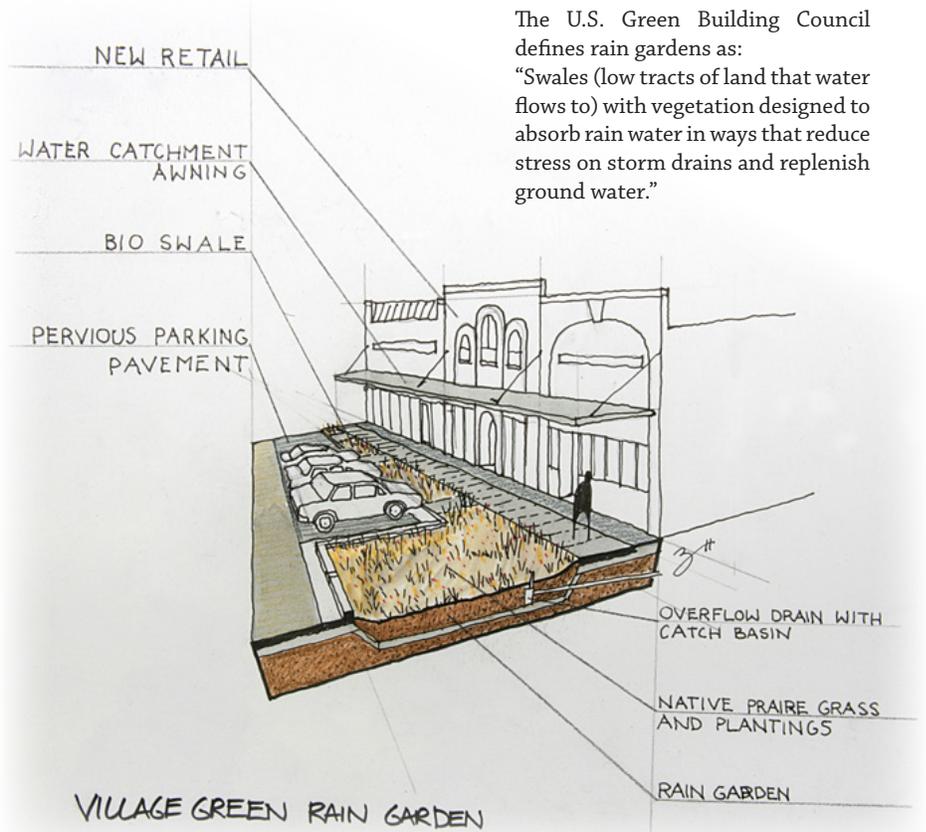
- Apartments

#### Public/Civic

- Governmental Facilities
- Institutional/Educational
- Libraries
- Recreation/Community Centers
- Parks/Trails

### Rain Gardens

The U.S. Green Building Council defines rain gardens as: "Swales (low tracts of land that water flows to) with vegetation designed to absorb rain water in ways that reduce stress on storm drains and replenish ground water."



# Highway 6/31 Corridor 2.4

## AREA DEFINITION:

Character Area 2.4 is located on the west side of Highway 6/31. This is bound on the north by South Street and on the west by 216th Street. The Gretna Middle School and Elementary School are currently located within this area.

## INTENT:

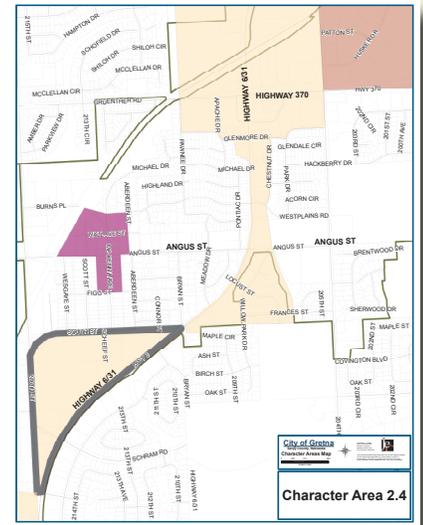
The intent of this area is to promote redevelopment of existing uses into a focal point for travelers on Highway 6/31 while maintaining the corridor streetscape.

## POLICIES:

1. Development within this area allows for public/civil uses, including possible recreational amenities, and for some commercial uses.
2. Frontage roads and improvements to the intersection should be considered to lessen traffic congestion.

## ADDITIONAL DESIGN STANDARDS:

If the area is developed into open space/recreational amenities, such area shall be adequately landscaped and connected by pedestrian traits.



## COMPATIBLE USES:

### Commercial

- Retail
  - Locally Owned, niche businesses
  - National Chain businesses

### Public/Civic

- Recreation/Community Centers
- Educational Facilities
- Governmental Facilities
- Libraries
- Parks/Trails
- Public Art

# Highway 6/31 Corridor 2.5

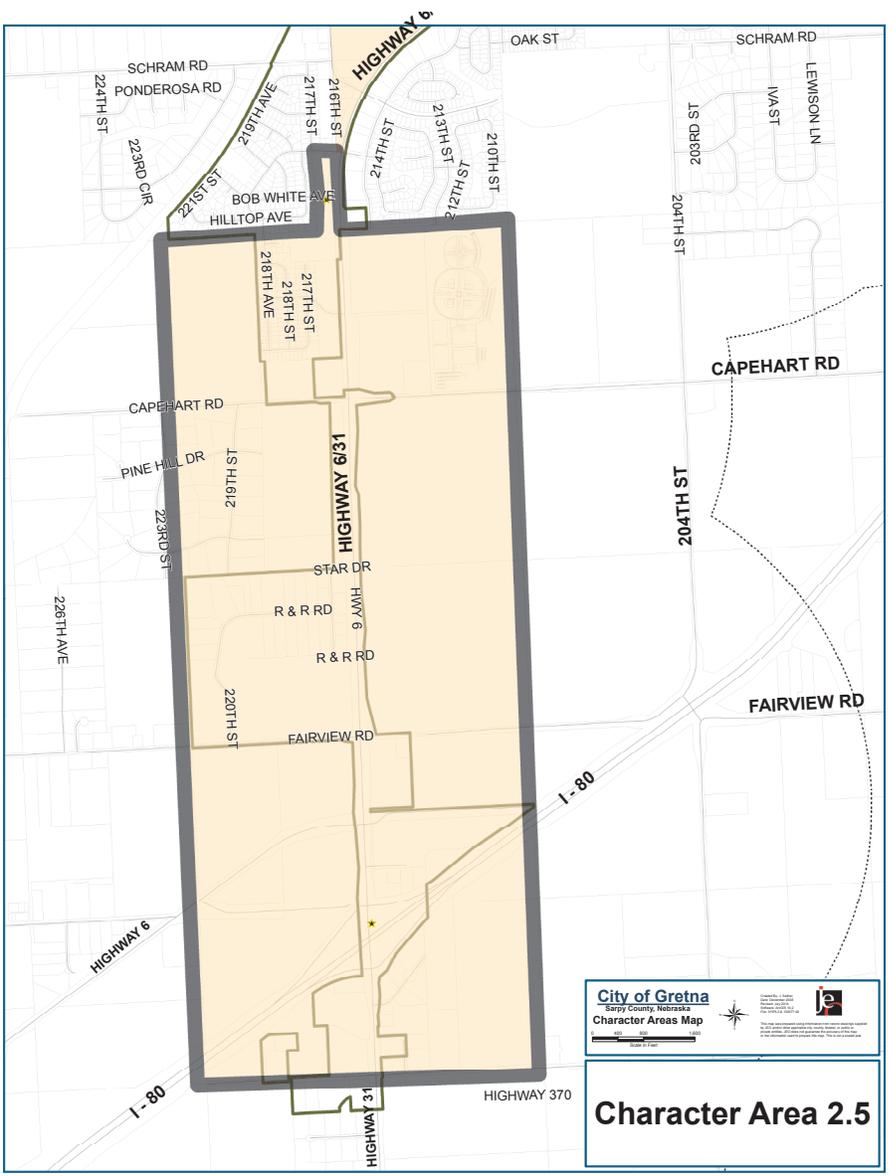
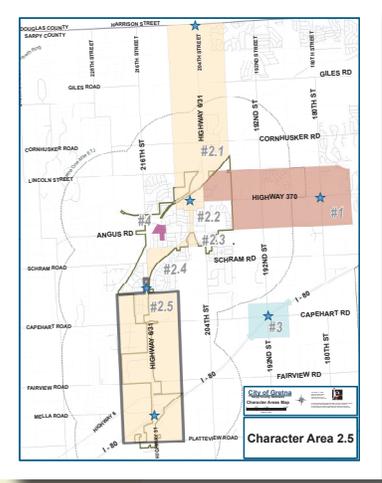
## AREA DEFINITION:

Character Area 2.5 encompasses the Highway 6/31 corridor from Plum Creek Drive to Platteview Road. This corridor is the primary access to I-80 westbound from the heart of the City and for eastbound interstate traffic into Gretna.

## INTENT:

The intent of this area is to create an entry portal into Gretna. The future development within this area should be reflective of the design standards of the Highway 6/31 Corridor, and should provide a positive image of Gretna as travelers enter from Exit 432. Figure 22 provides the master plan for Character Area 2.5.

The redevelopment of the outlet mall has increased visibility and vehicular traffic. Other developments at Exit 432 include the Flying J Truck Stop and surrounding light industrial uses at the interchange's southwest quadrant. The combination of a variety of uses and stringent design guidelines within designated overlay districts will help this area become an attractive gateway and entrance.



**POLICIES:**

1. Development in this area should allow for regional retail, commercial/business park, light industrial, flex, mixed use, and public/civic type uses.
2. Frontage roads should be considered to lessen congestion.
3. Sanitary sewer shall be extended to allow for development within the area.
4. Land shall be marketed by City for development.
5. Increased setbacks, buffering and screening shall be considered when such uses are located adjacent to residential uses.
6. Development of a green corridor along Interstate 80 with intensive landscaping around and within the interchange diamond.
7. Creating thematic gateway features to the Highway 6 corridor west of the interchange; Highway 31 south of the interchange; and Gretna on 6/31 to the north
8. Creation of a main street extending through Nebraska Crossing north to Fairview Road.
9. Business and research park development along I-80 between 204th Street and Highway 31 with landscaping and buffering along the I-80 frontage.
10. Conservation residential development south of I-80 adjacent to Platteview Road.
11. Open space preservation on rugged terrain in the northwest quadrant of the interchange, with access to new visitor services along the Highway 6/31 frontage.
12. Use of sustainable stormwater management practices, maintaining natural drainage patterns as part of a greenway system serving the interchange development.

**ADDITIONAL DESIGN STANDARDS:**

Earthen berms and/or plantings shall be incorporated into site plans to increase aesthetics and reduce negative visual impacts of parking lots.

**COMPATIBLE USES:****Commercial/Business Park**

- Office
- Travelers Services
- Anchor/Destination Retail
- Regional and National Retailers
- Destination Restaurants
- Business Park
- Support Retail
  - Convenience stores
  - Coffee Shops

**Light Industrial - Flex Space**

- Fully enclosed uses
- Low- to Medium-Impact (minimal water usage, minimal smoke output, minimal waste production)
- Warehousing/Distribution

**Public/Civic**

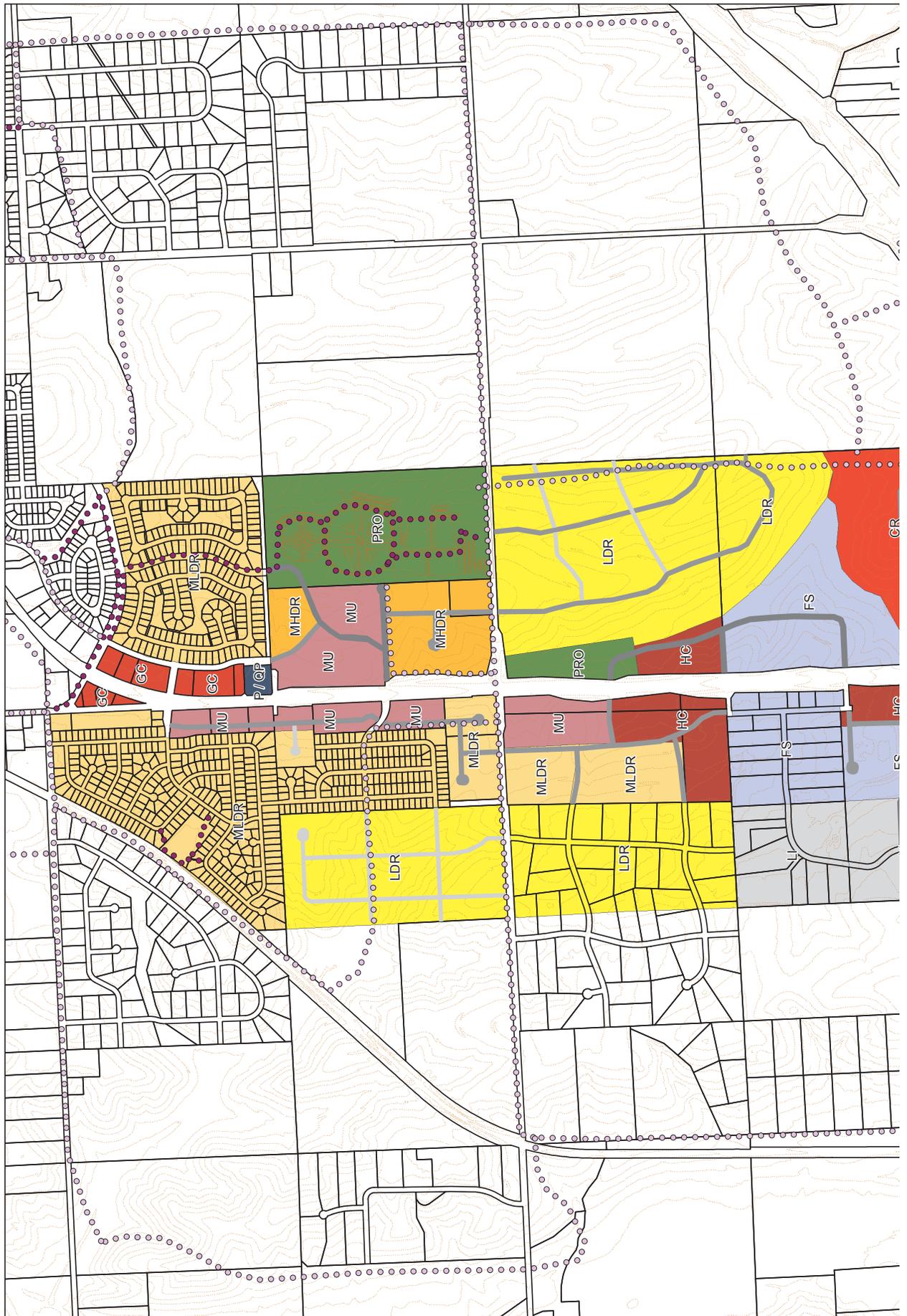
- Recreation/Community Centers
- Secondary Education
- Interactive Education
- Ecclesiastical
- Governmental Facilities
- Parks/Trails
- Public Art

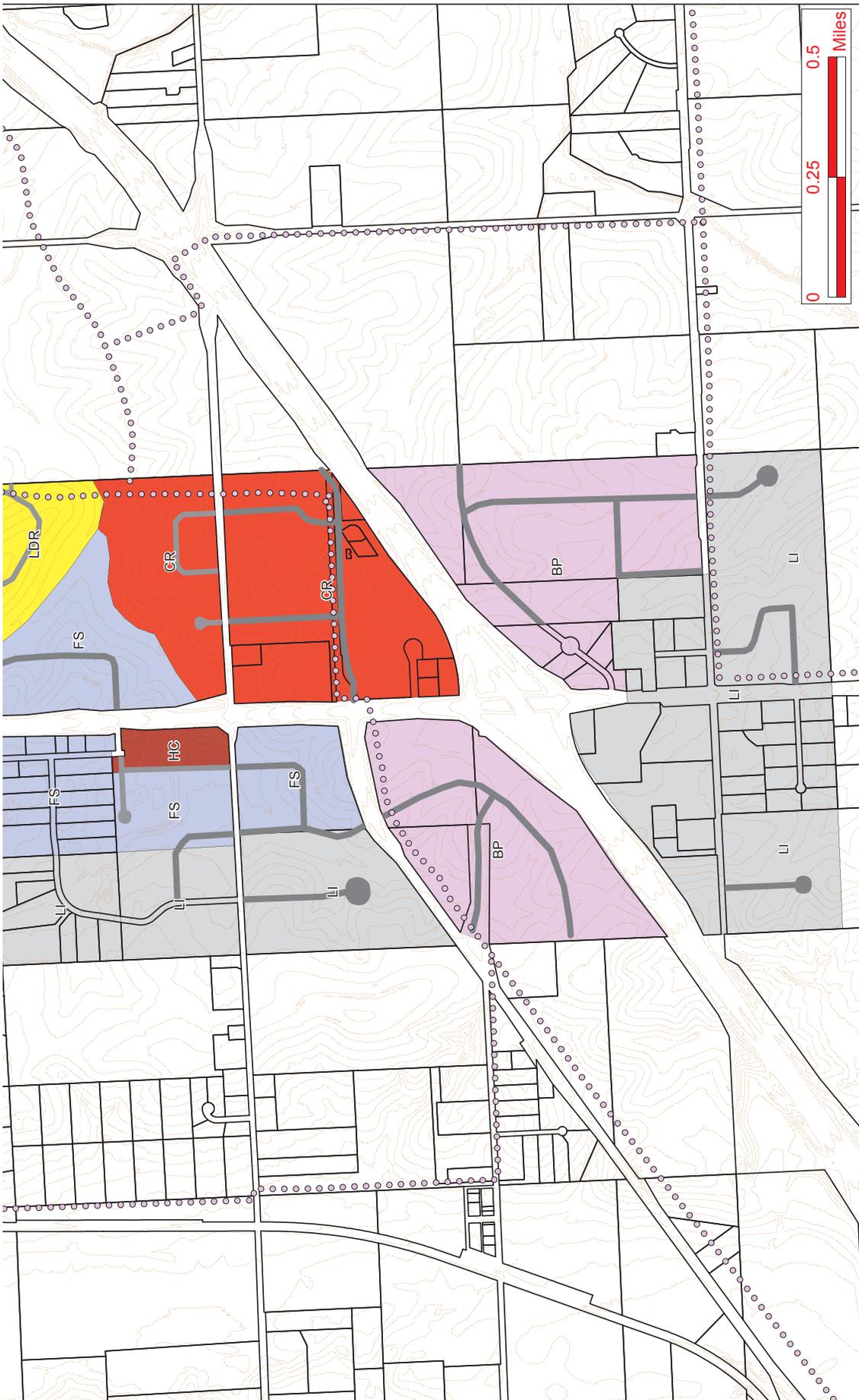
**Residential**

- Multi-family
- Medium to High Density

**Mixed Uses**

- Commercial on main level
- Office/Residential on second level
- Residential on additional levels





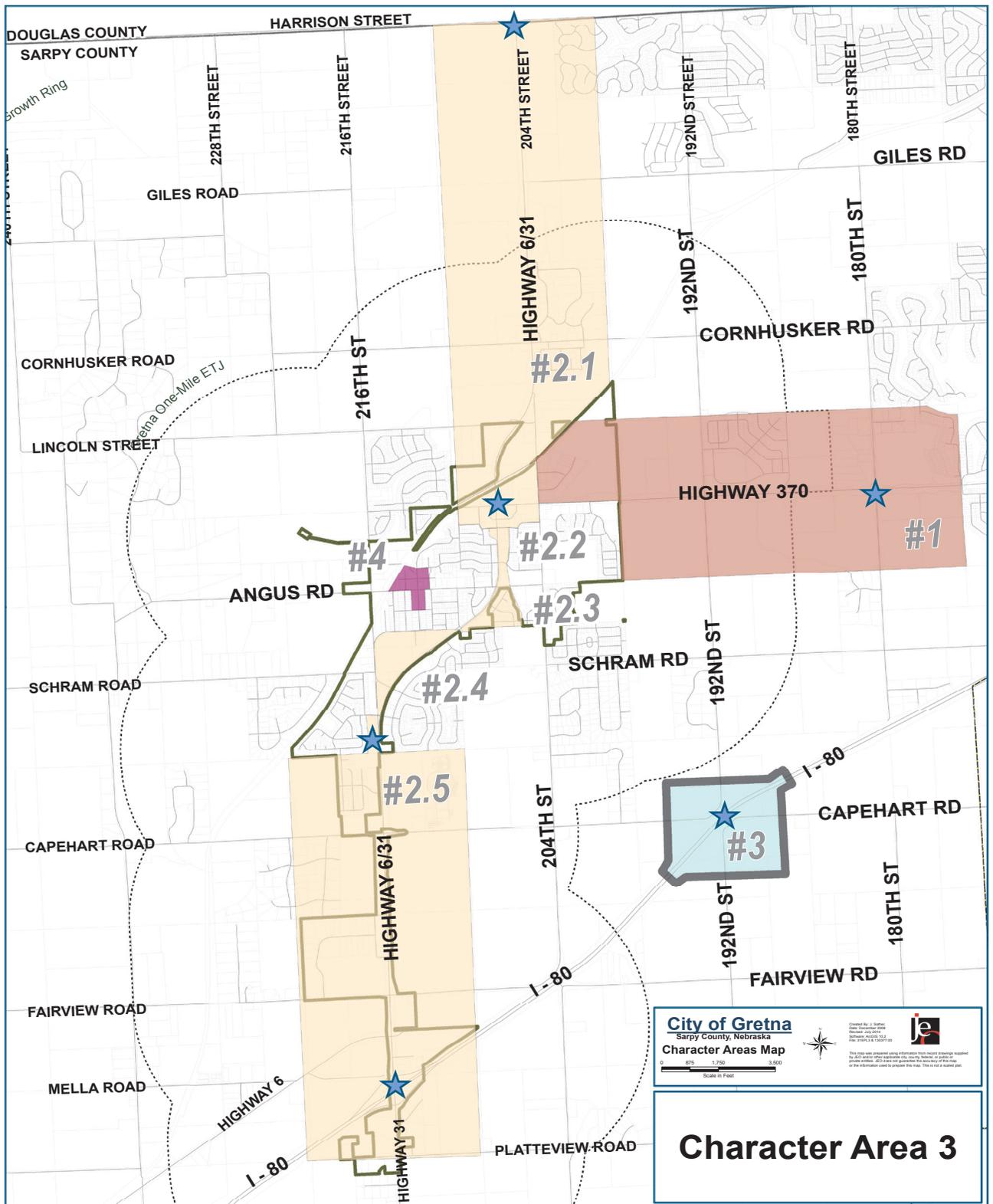
  
 Created By: MBG  
 Date: March 2014  
 Revised: 12/01/14  
 Scale: 1" = 100'  
 File: 140271.00  
  
 This map was prepared using information from record drawings supplied by JEO and/or other applicable city, county, federal, or public or private entities. JEO does not guarantee the accuracy of this map, or the information used to prepare this map. This is not a scaled plot.

## Gretna, Nebraska

### Highway 6/31 Corridor Study

**Legend**

 General Commercial (GC)	 Business Park Commercial (BP)	 Parks, Recreation, and Open Space (PRO)	<b>Trails</b>
 Highway Commercial (HC)	 Park Space (FS)	 Low Density Residential (LDR)	 50 foot ROW
 Commercial Retail (CR)	 Light Industrial (LI)	 Medium to Low Density Residential (MLDR)	 60 foot ROW
 Mixed Use (MU)	 Public / Quasi Public (PQP)	 Medium to High Density Residential (MHDR)	 70 foot ROW
			 Existing Trail
			 Future Trail
			 Ten Foot Contours



**City of Gretna**  
 Sarpy County, Nebraska  
**Character Areas Map**

0 875 1,750 3,500  
 Scale in Feet

Created by J. Larson  
 Revised by J. Larson  
 Date: 10/2014  
 File: 2014\_10\_14\_10:27:00

This data was prepared using information from public sources. The City of Gretna does not warrant the accuracy of this information or the information used to prepare this map. The City of Gretna is not responsible for any errors or omissions.

# Character Area 3

## **Character Area 3: Interstate 80 Corridor**

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## Interstate 80 Corridor

### AREA DEFINITION:

Character Area 3 as shown on the map comprises the area at and around the intersection of Interstate 80 and 192nd Street. As development occurs, the need for a north/south connection from the Interstate is warranted. 192nd Street currently passes over the Interstate and has been proposed to become the new interchange/exit. The City of Gretna shall continue to work with Sarpy County and NDOR in the development of this interchange and the protection of the 192nd Street Corridor. This character area and its associated policies, uses and design standards is also intended to follow a similar pattern along the entire stretch of Interstate 80 through Gretna's Jurisdiction and future growth areas.

### INTENT:

The suggestions set forth for this Area will provide guidance in the development of a new Interstate interchange. Proper uses, site design, and building standards will be designated in order to steer the development in a way that contributes to a positive image for the City of Gretna. The proposed development in this character area and extension along the Interstate would include highway commercial, mix use, light industrial, and high-quality business park development.

### COMPATIBLE USES:

#### Commercial/Business Park

- Traveler Services
- Retail
- Research and Office Parks

#### Light Industry

- High-tech and larger corporate firms

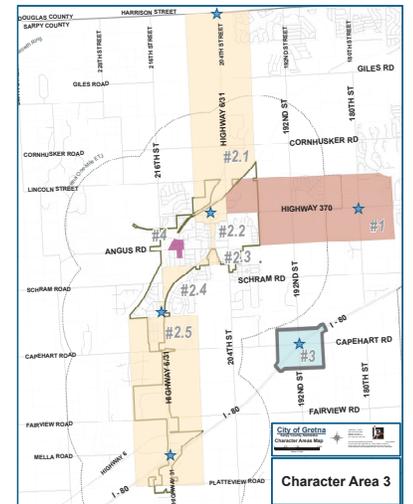
#### Residential

- Moderate- to High-density
- Lofts/Apartments integrated into retail/office development (Mixed-uses)

## POLICIES:

Based upon the approved Nebraska Regional Comprehensive Plan dated May 16, 2008 various land use policies have been identified. Some of these include:

1. Development of a green corridor along I-80 with intensive landscaping around and within the interchange diamond.
2. Innovative design elements at the interchange.
3. Parkway design of 192nd Street, including extensive roadway landscaping and complete street character.
4. Preservation of heritage farm as a living history attraction, with associated incentives to maintain a rural oasis at an urbanizing interchange.
5. Retail and mixed use development around the interchange, with strong pedestrian orientation, connections to surrounding neighborhoods, and greenway links.
6. Substantial business and office park development such as significant future office and research facilities.
7. Mixed density residential areas surrounding major mixed use and office development, and connected by local streets and pathways.
8. Use of sustainable stormwater management practices and maintenance of natural drainage patterns, incorporating water courses into the design of neighborhoods and business park features.
9. Trail and greenway connections under I-80 guidelines to unify developments on both sides of the Interstate.



## DESIGN STANDARDS:

1. Development should incorporate green/sustainable strategies.
  - Natural habitat and drainage and waterways should be preserved and measures to ensure their conservation should be pursued.
  - Buildings should employ energy efficient design
2. Public spaces should be incorporated into all developments. These spaces can include plazas, pedestrian corridors, seating areas, and more.
3. Parking areas should be designed to include natural landscaping and softer surfaces that break the expanse of hard paving. They should be located away from the street right-of-way or behind buildings and should be well-screened.
4. Landscaping designs should include trees and vegetation that will enhance the Area. Proper plant selection and placement can create buffers between uses, visual screens, and pedestrian protection
5. A variety of paving materials and textures should be utilized to designate significant paved areas. (i.e. crosswalks, sidewalks, etc.)
6. Lighting should reflect the character of the Area.
7. Wayfinding Signage should be designed to reflect the character of the Area and be strategically located to avoid interference with traffic safety.
8. Signage should be consolidated and limited to those necessary to identify tenants and direct traffic.
9. Earthen berms and/or plantings shall be incorporated into site plans to increase aesthetics and reduce negative visual impacts of parking lots and structures from public right-of-ways and residential areas.





**Character Area 4:  
Original Downtown Area**

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## Original Downtown

### AREA DEFINITION:

Character Area 4 is the area known as Original Downtown Gretna. It is centered on McKenna Avenue and is adjacent to the BNSF Railroad line.

### INTENT:

The creation of this Character Area is intended to aid in the preservation of the original central business district of the City. Gretna's downtown has been redefined in recent years since its original construction. This area is a viable part of the community and as growth occurs it will be further redefined. The implementation of the following suggestions will help restore the traditional character of the downtown and will support the revitalization of the "heart" of the City.

### COMPATIBLE USES:

Mixed-Uses within single structures

- Commercial on main level
- Office/Residential on second level
- Residential on additional levels

Commercial

- Retail
  - Locally Owned, niche businesses
  - Restaurants
  - Coffee Houses/Cafes
- Office

Residential

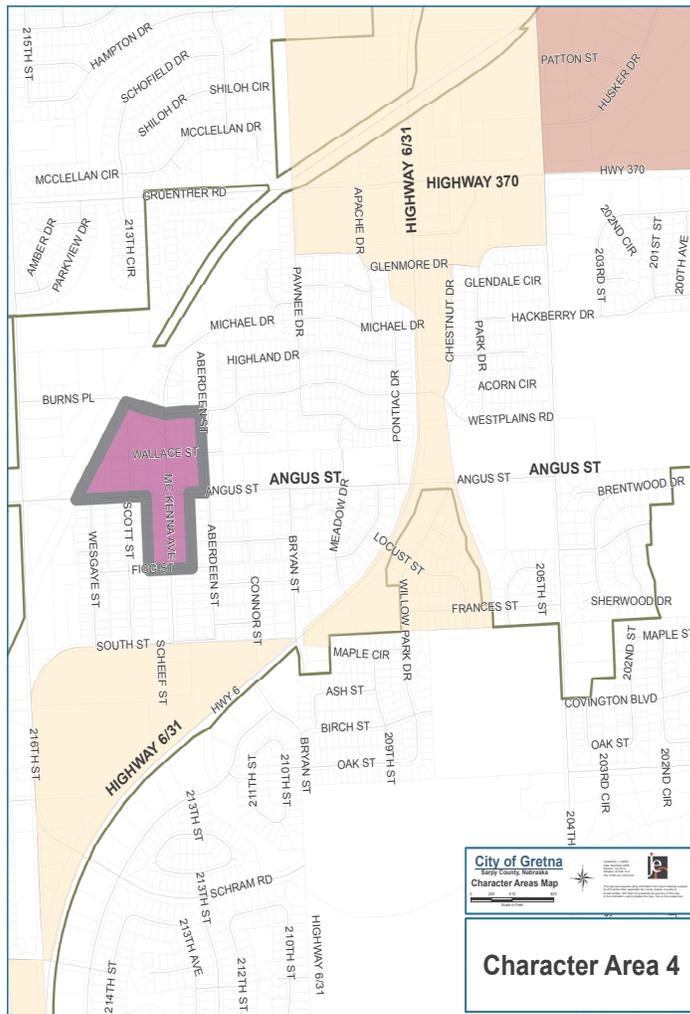
- Lofts/Apartments
- Condominiums

Public/Civic

- Governmental Facilities
- Libraries
- Recreation/Community Centers
- Parks/Trails
- Public Art
- Outdoor spaces/Theaters

### POLICIES:

1. A downtown theme should be developed that identifies the Area as a cultural, entertainment, and small business district with a historical flare. A downtown Master Plan for the area has been developed and should be reviewed and incorporated as redevelopment occurs. Additional concepts are provided on the following pages.
2. The use of development incentives tailored to attract uses to properties within the downtown should be explored as a means to encourage appropriate reinvestment in the Area.



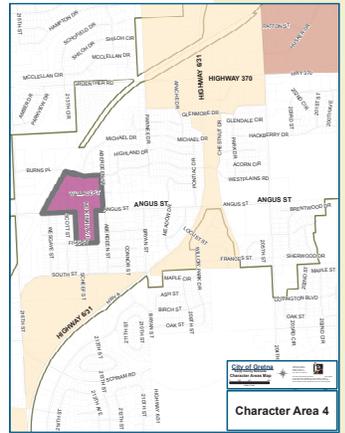
3. The display of exterior private and public art should be encouraged through display methods and locations that enhance the experience of visiting the Area while remaining compatible with Gretna's values and aesthetic character.
4. Additional off-street parking should be created where feasible. Such parking shall be of size and character to fit the "Original Downtown" theme. Large open parking lots shall be avoided.
5. Any lighting used shall compliment the established theme and be pedestrian designed. Such lighting used to illuminate off-street parking areas, signage, or other structures should be arranged so as to deflect light away from any adjacent property and minimize light escape from excessive uplighting, through fixture type, height, and placement.
6. The historical, cultural and institutional resources of the community should be identified, preserved, protected, and promoted.
7. Signage in the area should adhere to the established theme of the "Original Downtown" and be of pedestrian scale.
8. An architectural committee together with the Downtown Association shall be established that would evaluate all new developments and re-developments in the Area, including lighting, signage, landscaping, storefronts, etc.
9. All areas of "Original Downtown" shall be ADA accessible.



**Public Art** can enliven the street life and pedestrian experience throughout the area, showcasing local, regional, and even global art, through indoor and outdoor displays. Walls and fences can be used to display permanent or temporary art exhibits. Art installations can be freestanding or incorporated into building or landscape design.

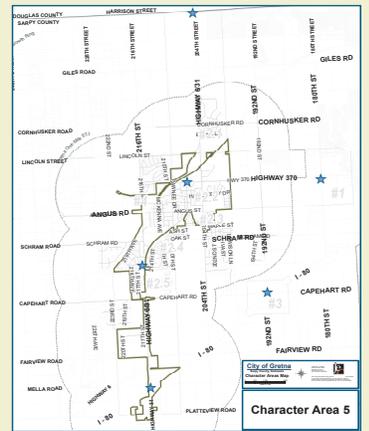
**Living space** will enhance the liveliness and attraction of the area through continuous activity as restaurants and nightlife venues develop to serve the permanent residents and visitors. Live/work residential space can also draw other residents into the community.





## DESIGN STANDARDS:

1. Existing structures should be preserved and restored in order to maintain the historic character of the area.
2. New development in the Downtown should infill vacant land and should complement the existing structures by mimicking the scale and scheme of the area.
3. Development should be designed to a human scale:
  - The street-facing façades should be penetrable. It should include windows and openings to allow a connection between the building interior and the sidewalk.
  - Shopping and dining areas should be oriented toward and friendly to the pedestrian rather than the automobile.
  - Awnings should be placed on the main level to provide shelter and protection for pedestrians on the sidewalk.
4. Brick, masonry, and tile materials should be the primary building material palette of choice
  - Integration of materials is encouraged (stucco, ironwork, glass, etc.)
5. Public Art should be included as a part of the streetscape design. Public Art helps enliven the street life and pedestrian experience
6. Pocket parks and green space should be incorporated into the Downtown Master Plan to create open space for residents, workers, and visitors. Founders Memorial Park should be expanded and made an integral part of the Original Downtown.
7. Structures with more than two levels should step back the façade of additional levels.
8. Patios and balconies should be included as part of the street façade of third levels and above.
9. Landscaping designs should include trees and vegetation that will enhance the Area and its architecture.
10. A variety of paving materials and textures should be utilized to designate significant paved areas. (i.e. crosswalks, sidewalks, etc.)
11. A common lighting theme should reflect the character of the Area. Streetscape furniture should reflect this character as well.
12. Wayfinding signage should be designed to reflect the character of the Area and be strategically located to avoid interference with traffic safety.
13. Parking should be located behind buildings or at public parking lots and should be well-screened.
14. Site furniture, including benches, trash receptacles, etc. shall follow a common theme in this area.



## Character Area 5: Community Entrances

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## Community Entrances

### AREA DEFINITION:

Six community entrance locations have been identified. These locations include:

- Highway 6/31 and Harrison Street
- Highway 6/31 and Highway 370
- Highway 6/31 and Schram Road
- 180th Street and Highway 370
- 192nd Street and I-80
- I-80 Exit 432

### INTENT:

Community entrances are an often overlooked component of community character. Community entrances provide travelers with their first impression, an impression that stays vivid in the minds of visitors, especially for first time visitors. The beauty or ugliness portrayed by the entrance is part of the impression. Community entrances are transitions from the world outside our community into the world within our town.

### COMPATIBLE USES:

See Future Land Use Plan.

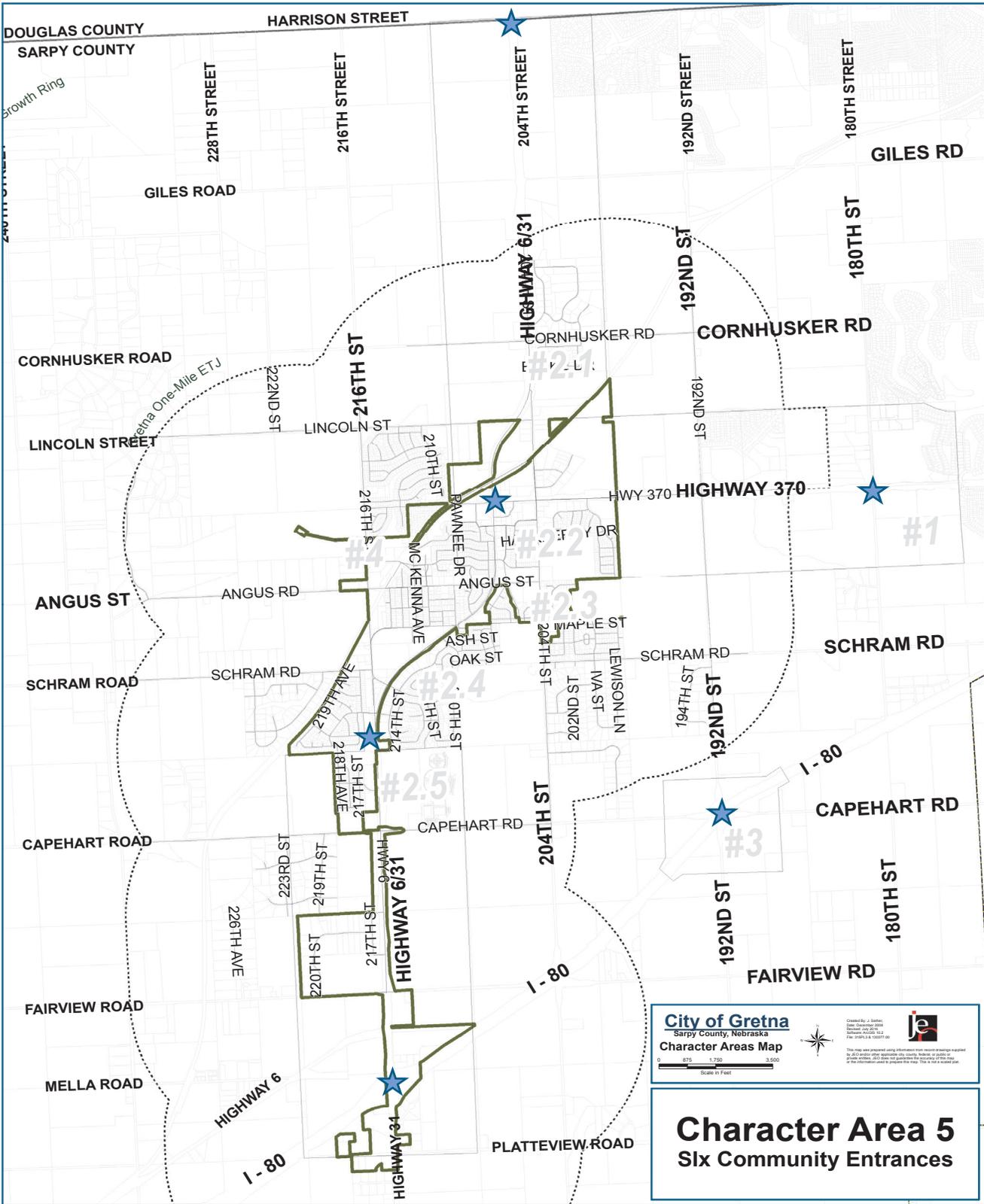
### POLICIES:

1. Major entryways into Gretna will be identified, protected, and enhanced in order to emphasize and preserve the natural setting and appearance of the community.
2. Community entrances should convey a sense of arrival, communicating to travelers that they have moved from one traveling experience to another: from a rural and agricultural setting to an organized pattern of streets, homes, and businesses.
3. Perimeter entrances should identify the community by name and features while internal entrances should call out community events and activities.

### DESIGN STANDARDS:

1. Signage should be designed to reflect the character of the Area/Corridor and be strategically located to avoid interference with traffic safety.
2. Landscaping should be incorporated at community entrances that create attention and interest throughout the year.
3. Public art should be incorporated in the overall design of community entrance where feasible.
4. Lighting should be designed to enhance signage, landscaping, and public art.
5. Roadways shall be designed with medians or with appropriate easements to encourage green space and trees in the corridor at designated entry points.
6. Entry Points should be designed so as to allow continuation of the design elements along the corridor and throughout the community by a type of branding such as repetitive lighting styles, banners, etc.





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# Future Growth Areas

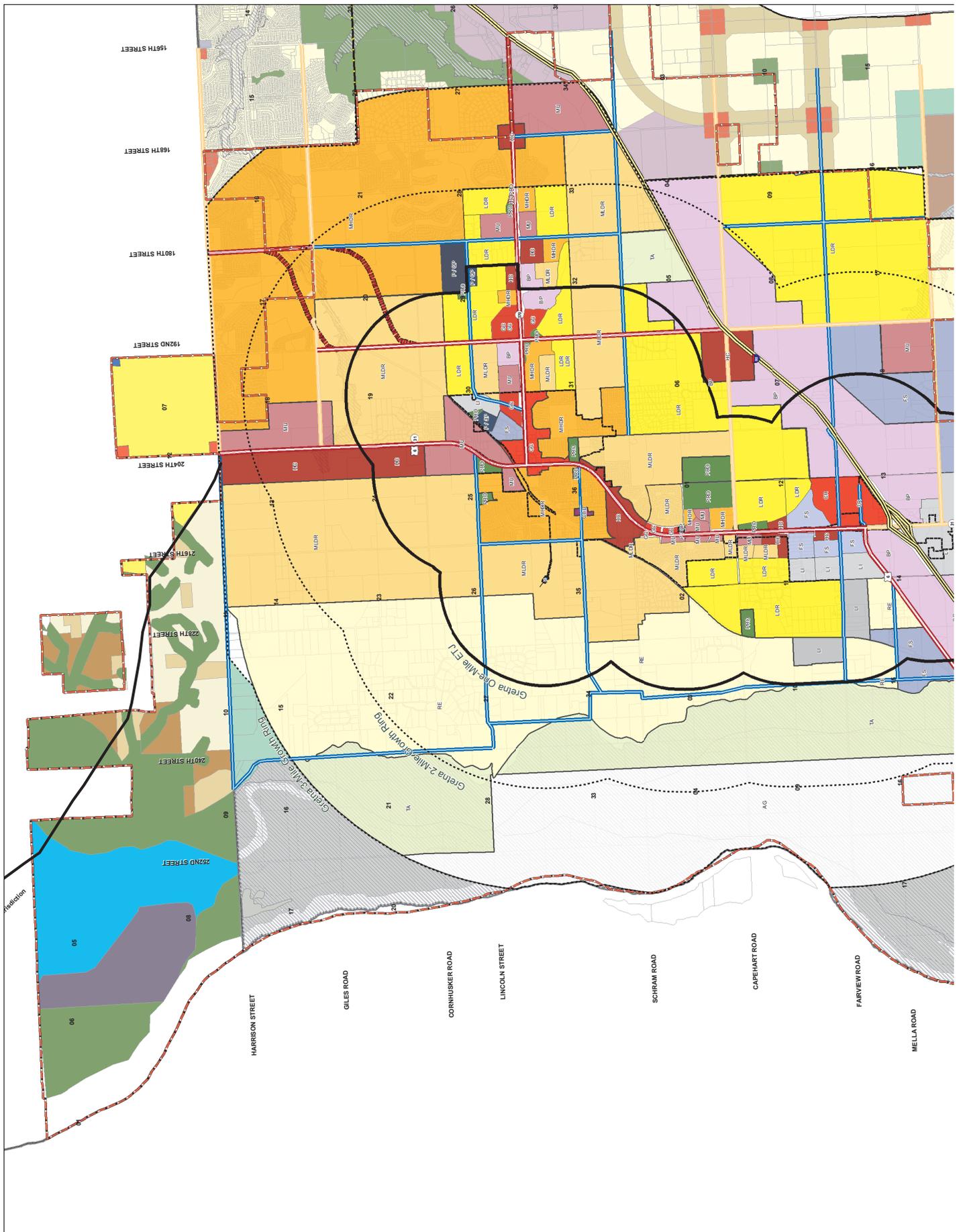
The City of Gretna has also projected a two-mile and three-mile future growth area. These growth areas look at the future land use possibilities beyond the statutory one-mile extraterritorial jurisdiction. This designated land is the area that is likely to become the City's responsibility or could have a significant impact on future growth as the City continues to grow, or changes in the City's classification or legislation allows for Gretna to have additional jurisdiction.

Nebraska Legislation states that a community over 5,000 can designate themselves as First Class City. At the time of Gretna's designation to a First Class City, they may choose to extend their extraterritorial jurisdiction (ETJ) to two miles in order to enforce the City's development rules and regulations. The City of Gretna should coordinate with Sarpy County to simplify boundaries by determining a squared-off jurisdiction instead of a rounded jurisdiction.

The Future Growth Areas of Gretna are represented in the Future Land Use and Transportation Map for Future Growth Areas (Figure 23). This future land use and transportation map looks at the County's land use designations, projected Land Use designations of Gretna's one-mile jurisdiction, inter-local agreements, existing land uses, floodplain, etc. in order to extend proposed land uses and transportation developments in an orderly and meaningful fashion, so that when the City gains control of those areas they are prepared for growth.

These future growth areas should be reviewed consistently with the Future Land Use Plan of Gretna's current jurisdiction. The Future Land Use and Transportation Plan of the Future Growth Areas should likewise be shared with Sarpy County for future county and regional planning projects.

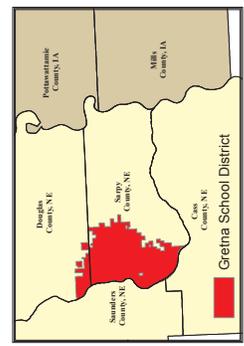
FIGURE 23: FUTURE LAND USE AND TRANSPORTATION, FUTURE GROWTH AREAS





**City of Gretna Future Land Use**

- Interstate Highway
- Major Arterial
- Major Arterial Options
- Other Arterial
- Collector
- Local Roads
- Agriculture
- AG Transitional Agriculture
- TA Residential Estates
- RE Low Density Residential
- LDR Medium to Low Density Residential
- MDR Medium to High Density Residential
- MU Mixed Use
- CC General Commercial
- CR Commercial Retail
- CBD Central Business District
- HC Highway Commercial
- BP Business Park Commercial
- FS Flex Space
- LI Light Industrial
- HI Heavy Industrial
- PCP Public / Quasi Public
- PRC Parks, Recreation, and Open Space



**Legend**

- Existing Interlocal Agreement
- Gretna Public Schools - District Boundary
- One-Mile Extraterritorial Jurisdiction
- Two / Three-Mile Growth Areas
- Gretna Corporate Limits
- Parcel Boundaries
- Existing Floodplain

**Douglas County / Omaha Current Future Land Use**

- LDL Low Density Residential
- UR Urban Residential
- ERS Existing Residential Development
- EC Environmentally Constrained
- CM Community Mixed Use Area
- NC Neighborhood Mixed Use Area
- CNC Civic
- PRC Preserve
- R Resource Extraction

**Sarpy County Current Future Land Use**

- CR Conservation Residential
- ER Estate Residential
- UR Urban Residential
- UR2 Urban Residential II
- RRS Residential - Community Systems
- LRP Long Term Residential Growth
- BP Business Park
- MU Mixed Use
- MUC Mixed Use Center
- PIG Plug In/Exchange Development
- LI Light Industrial/Storage
- I Industrial
- CNC Civic
- PKS Park-School Site
- GC Greenway

**City of Gretna Sarpy County Nebraska Future Land Use and Transportation Map Including School District 77-0037**

Contract No. 2-08-001  
 Revised 02/09/14  
 Project No. 77-0037  
 The City of Gretna, Nebraska, is pleased to announce the release of this map. The map is a public document and is intended for informational purposes only. It is not to be used for any other purpose without the express written consent of the City of Gretna.

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# Transportation Plan

- Street Classifications
- Existing Street Designations
- Future Street Designations
- Street Projects
- Pedestrian System



# Transportation Plan

Transportation networks tie a community together and link a community to the outside world. Adequate circulation systems are essential for the safe and efficient flow of vehicles and pedestrians, to all parts of the community. Communities need to be able to provide adequate transportation services to move people and goods around and through the community. In addition, access to facilities, industries, and businesses should facilitate efficient traffic patterns while minimizing conflicts between vehicular and pedestrian traffic.

Land use and transportation work together to create the pattern for future development. An improved or new transportation route generates a greater level of accessibility and determines how adjacent land may be utilized. Land use shapes the demand for transportation routes and alternatives, while transportation projects can direct new development locations and intensities to areas where the transportation infrastructure can best accommodate their needs. This interaction of land use and transportation is illustrated on the Future Land Use and Transportation Map (Figure 20) of this Plan.

The adequacy of a community's transportation system will have a substantial impact on the rate and pattern of its future growth and development. To ensure the transportation system is able to expand efficiently and remain consistent with PlanGretna requires careful, consistent long-range planning efforts. The transportation needs, in terms of both location and capacity, will change as land uses are proposed, altered, and constructed. It is important that future land use patterns be considered along with the existing street pattern when street improvements are identified. In general, the greater the transportation need or demand generated by a particular land use, the more important it is for that land use to be located near major transportation facilities.

Commercial uses and activities are most sensitive to accessibility since their survival often depends upon the ease with which potential customers can travel to their location. In this case, accessibility refers not only to the distance which must be driven, but also the ease with which the site can be found and accessed, as well as the availability of convenient parking. Therefore, commercial land uses are generally located near the center of their market area, along highways and arterials, and at major intersections. Clustering commercial uses together is also an advantage because it creates an image that is memorable, allows the community to control the flow of high traffic volumes, can provide shared parking facilities, and will allow a pedestrian connection between the uses. Some corridors; depending upon access, will allow for flex space, business park, and mixed-use developments.

Residential uses are very sensitive to traffic patterns. Commercial and industrial traffic should not travel through residential areas in order to access their destination. In residential areas speeds are slower, and roads are typically narrower to encourage heightened driver attentiveness. Pedestrian safety is paramount when planning transportation routes through residential areas.

Industrial uses are also highly dependent on transportation access, but in a different way. For example, visibility is not as critical for an industrial business as it is for most commercial uses. Industrial uses often need access to more specialized transportation facilities, which is why they tend to be located near railroad lines, highways, and reinforced roadways built to handle heavy truck traffic.

Public uses, such as City offices and parks, also require efficient and clear access routes. The public should be able to locate and use public services without difficulty.

Facilities such as community centers and regional parks may generate significant traffic loads, especially during community events, and need to be located near arterial streets. Pedestrian accessibility to these public uses is also very important and trails should be designed to connect such uses to residential areas of the community.

Automobiles are expected to continue to be the primary means of travel for Gretna residents throughout and beyond the planning period of this Comprehensive Plan. Such vehicles depend upon the expansion and continued maintenance of a street and road network allowing ease of mobility throughout the area. In addition, much of the area's freight movement occurs on local streets and highways. Bicyclists also often utilize the street system for their travel. Although investment in other modes of transportation may decrease reliance on the automobile, streets and highways will continue to be the backbone of the region's transportation system. However, evaluation, consideration, and planning efforts need to be undertaken to promote and accommodate other alternative modes of transportation. With the discussion of light rail connecting Lincoln to Omaha, opportunities exist for Gretna to become an important and major link to such a route. As a route and connection develops, Gretna should consider possible locations for a depot as well as park and ride facilities.

## STREET CLASSIFICATIONS

Streets are classified based upon the function they serve. All streets fall within one of four classifications. Utilizing street classifications allows a community to examine their transportation system and identify weaknesses. Using a hierarchical classification system, street facilities and improvements can be planned to address existing and future transportation needs as well as influence land use patterns.

- **Major Arterial** – This functional class of street serves the major portion of through-traffic entering and leaving the community and is designed to carry the highest traffic volumes. These serve intra-area traffic such as between the intense commercial areas and outlying residential areas or traffic between major community facilities. Managing and controlling access to these types of roadways is very important. This access must respect and reflect the land uses and development context adjacent to each major arterial. For example, managing and controlling access to and from a roadway in the established parts of Gretna differs from that in developing locations, because of the varying character of these areas. The major arterial system is stratified into the following two subsystems:
  - **Interstate Highway**– These are divided, limited access facilities with no direct land access. The Interstate does not have at-grade crossings or intersections. The interstate is intended to provide the highest degree of mobility serving potentially large traffic volumes and long trip lengths.
  - **Other Major Arterial** – This functional class of street serves the major portion of inter-community and intra-community traffic movement within the community and is designed to carry high traffic volumes. Facilities within this classification are capable of providing direct access to adjacent land but such access is incidental to the primary functional responsibility of moving traffic within the system.
- **Other Arterial** – This functional class serves trips of moderate length and offers a lower level of mobility than major arterial. This class of street interconnects with and augments major arterials, distributes traffic to smaller areas, and contains streets that place some emphasis on land access. These street/roads are characterized by moderate to heavy traffic volumes.

- **Collector Streets** – These streets serve as a link between local streets and the arterial system. Collectors provide both access and traffic circulation within residential, commercial, and industrial areas. Collector streets also provide more direct routes through neighborhoods for use by transit, pedestrians and cyclists. Moderate to low traffic volumes are characteristic of these streets. There should be one north/south and one east/west continuous, but not straight, collector street within a developing square mile.
- **Local Streets** – These are composed of all lower order facilities that essentially serve as a conduit between abutting properties and higher order streets. Local streets provide the lowest level of mobility and generally exhibit the lowest traffic volumes.

## EXISTING STREET DESIGNATIONS

### Interstate

- I-80

### Major Arterial

- Hwy 6/31
- Hwy 370

### Other Arterial

- Hwy 31 (south of the Hwy 6 junction)

### Collector

- None

## FUTURE STREET DESIGNATIONS

The following street/road classifications are recommended as well as those that are identified in Figure 20, the Future Land Use and Transportation Map.

### Interstate

- I-80

### Major Arterial

- Hwy 6/31
- Hwy 370
- 192nd Street

### Other Arterial

- Hwy 31 (south of the Hwy 6 junction)
- 180th Street

### Collector

- Platteview Road
- Fairview Road
- Angus Road
- South 234th, 237th, and 240th Streets as they extends from Hwy 6 to Harrison
- Schram Road
- Lincoln Road
- Capehart Road
- Nebraska Crossing Drive

## STREET PROJECTS

Long-range transportation projects that provide appropriately timed transportation enhancements and expansions will be a deciding influence in the future growth and development of Gretna. The efforts needed to make these improvements happen will take years to complete and many millions of dollars to fund.

## FUTURE PROJECTS

The list below highlights several of the projects identified by PlanGretna as desired or necessary to facilitate the community's growth. These transportation projects will not only be the responsibility of the City of Gretna, but will require participation from other governmental entities. These include a range of projects for which the City has already identified a specific need, as well as longer term projects that do not have specific plans or designs. As development occurs in and around Gretna, additional road projects may be identified. Additional information and short-term street projects, such as re-surfacing, can be found in the city's current one-six year road plan. The projects shown are not listed in any particular order.

1. **Improvement of 192nd Street with an Interchange at I-80** – The Metropolitan Area Planning Agency (MAPA) Heartland 2050 has identified 180th Street as being a major arterial roadway through the City of Omaha with a connection south through Sarpy County and the possibility of an interchange at I-80 as development occurs. Due to the existing developments, this connecting corridor has been identified as 192nd Street with the possible interchange at I-80.
2. **Expansion of Hwy 370** – NE Hwy 370 has been expanded to a four-lane separated highway. The Nebraska Department of Roads has indicated Highway 370 as a possible six lane highway from the City of Gretna to the I-80 interchange.
3. **New Beltway System** – MAPA has identified a need for a second beltway system to ring the Omaha Metropolitan Area. This Beltway system would most likely follow along Platteview Road in Sarpy County and Intersect I-80 somewhere southwest of Gretna and heading north on the west side of Gretna.

## RIGHT-OF-WAY CONSIDERATIONS

As Gretna develops, it will be important to plan for the necessary street improvements to support the development. To facilitate these street improvements, the appropriate right-of-way will need to be acquired. Right-of-way will be obtained through purchase, either outright or through condemnation. However, when land subdivision projects are proposed along routes identified for future improvement, the City can require the dedication of the right-of-way necessary to support the improvement.

The required right-of-way width will vary according to the classification of the street being developed or improved, the nature of any public utilities that will share the right-of-way with the street, and any sidewalk and trail requirements along the corridor. Additional right-of-way may be needed for boulevards or green streets where landscaping is required or encouraged. These requirements can be found the City's Subdivision Regulations.

## PEDESTRIAN SYSTEM

Another type of transportation system that is recognized and shall be considered in the built environment is the use of sidewalks, trails and designated pedestrian routes (safe routes to schools). Sidewalks or trails shall be placed on both sides of the street, where feasible, according to the City's specifications and ADA requirements. Multi-use trails shall be designated to accommodate pedestrians and bicyclists as alternative modes of transportation interconnecting residential areas and linking them to commercial and public uses. Such trails are hard-surfaced within and adjacent to the City limits and in subdivisions, while trails in undeveloped areas may be constructed of crushed limestone or a similar product that meets ADA accessibility requirements.

### Pedestrian Level of Service Factors:

**Continuity** - the public sidewalk system should be complete, without gaps, and maintained in good repair; the pedestrian network in shopping centers should be integrated with adjacent activities.

**Security** - pedestrians should be visible to motorists and other pedestrians, separated from motorists and bicyclists, and have adequate lighting.

**Visual Interest** – pedestrians should enjoy a visually appealing environment; street lights, fountains, and benches should match local architecture; pedestrian amenities should include attractive landscaping.

**Directness** - pedestrians should be able to walk in a reasonably direct path to destinations like transit stops, schools, parks, and commercial and mixed-use activity centers.

**Street Crossing** - street crossings should be safe and feel comfortable, with consideration given to the number of lanes to cross, traffic volumes, turning movements, and speed, signalization, curb radius and ramps, crosswalks with buttons, lighting, medians, visibility, and convenience.

The Gretna Trail System is illustrated on the Gretna Parks and Trails Map (Figure 27). Existing, planned, and proposed trails are shown on the map and connect neighborhoods to commercial areas and to various parks and community amenities. Trail design and construction shall be completed to accommodate multiple uses and meet ADA standards. The Gretna Trail System shall not only connect various portions of the community to each other but it will also connect the City with regional amenities, other communities and other trail systems.

Walking is an essential part of every resident's daily activities, whether for work, shopping, or recreation. Pedestrian facilities are often overlooked or under planned. Many times, these facilities are added onto street projects without enough forethought, and result in inefficient, unused, or unsafe pedestrian walkways. The quality of life for Gretna residents and the goals and objectives set forward in this plan depend upon safe and adequate pedestrian alternatives to motorized transportation. In order to make Gretna a more walkable and pedestrian friendly community, the existing system should be maintained and additional facilities should be planned. Planning for pedestrian facilities should consider:

- Location of proposed facilities in relation to planned activity centers, such as the Gretna Original Downtown, schools, City parks, ball fields, City offices, and Metro Area trails connections.
- Design guidelines for trails, sidewalks, and other pedestrian facilities, including surfacing material, paving width, lighting and benches.
- Pedestrian level of service factors, including continuity security, visual interest, directness, and street or railroad crossing.
- Available programs to assist with repairing existing neighborhood systems that are deteriorating.
- Requirements of the Americans with Disabilities Act (ADA).
- Needs of the senior population of Gretna.

## TRAILS

Multi-use trails are a critical resource for pedestrian users and bicyclists. Such trails not only provide an alternative means of transportation, they provide connectivity between neighborhoods linking the community together, buffer between different land uses, and offer many recreational opportunities. As a strength of the community and a benefit to the quality of life, the maintenance, development and expansion of a City-wide multi-use trail system should occur along with other transportation improvements. Pedestrian and bicycle amenities play an important role in the community by providing a healthy alternative to the automobile, reducing traffic congestion, improving air quality, and creating a more balanced overall City transportation system.

Paths of Discovery is a network of multi-use trails in the Omaha Metropolitan Area. The City of Gretna needs to work with the Papio Missouri River NRD, the City of Omaha, and Sarpy County to connect the community into this network. The closest existing trail system is runs through the Chalco Hills Recreation Area east of Gretna along Hwy 370. The handful of existing trails located within a few Gretna subdivisions need to connect to the area's trails network. Such connections and overall Trails Plan will allow the community to provide cohesive growth and apply for funding assistance. The Gretna Trails Plan is incorporated into the Gretna Parks and Trails Map (Figure 27) where existing, planned and proposed trials within Gretna and its planned area are identified.

# Park and Trails Plan

- Existing Conditions
- Mini Parks
- Neighborhood Parks
- Community Parks
- School Facilities
- Special Use Facilities
- Sports Complex
- Recommendations
- Park Land Dedication/Fee



# Parks and Trails Plan

The Gretna Parks and Recreation portion of PlanGretna will create a tool for the City for developing priorities regarding the improvement of existing facilities and the expansion of the overall parks system.

The City's first Parks and Trail Plan was adopted as part of the 2009 Comprehensive Plan. The development of this Parks and Trails Plan occurred under the direction of the Comprehensive Development Plan Steering Committee, with the assistance of a special focus group made up of local athletic groups, as well as the ideas and comments that came out of the Town Hall meetings, charrette process and other various focus groups.

Based upon public input, current conditions, notable deficiencies, and future growth projections of Gretna, a proposed Parks and Trails Map has been created along with a list of recommendations for each existing and proposed park and recreational facility.

Since 2009, the Parks and Trails Plan has been used to locate parks and trails in new developments, helped determine the location of a new Gretna school, and has guided the City in purchasing the park ground for The Fields at Gretna. Changes in land use and current development has warranted amendments to the 2014 Parks and Trails Plan and map.

## EXISTING CONDITIONS

The City of Gretna manages four park facilities and is currently developing The Fields at Gretna. There are four other parks within subdivisions just outside of Gretna's corporate limits. School facilities are considered neighborhood parks and also analyzed in the park system. This section has a listing of the condition and capacity of all these facilities along with photos of the facilities. Table 35 lists the national accepted standard criteria for various types of parks and recreation facilities that was followed in Gretna.

TABLE 35: PARKS AND OPEN SPACE CLASSIFICATIONS

Classification	Location Criteria/ General Description	Service Area	Size Criteria
Mini-Park	Used to address limited, isolated or unique recreational needs.	Less than a ¼ mile distance in residential setting.	Between 2500 sq. ft. and one acre in size.
Neighborhood Park	Basic unit of the park system and serves as the recreational and social focus of the neighborhood. Focus is on informal active and passive recreation.	¼ to ½ mile distance and uninterrupted by non-residential roads and other physical barriers.	5 acres is considered minimum size. 5 to 10 acres is optimal.
School-Park	Depending on circumstances, combining parks with school sites can fulfill the space requirements for other classes of parks, such as neighborhood, community, sports complex, and special use.	Determined by location of school district property.	Variable – depends on function.
Community Park	Serves broader purpose than neighborhood park. Focus is on meeting community-based recreation needs, as well as preserving unique landscapes and open spaces.	Determined by the quality and suitability of the site. Usually serves two or more neighborhood and ½ to 3 mile distance.	As needed to accommodate desired uses. Usually between 30 and 50 acres.
Large Urban Park	Serve a broader purpose than community parks and are used when community and neighborhood parks are not adequate to serve the needs of the community. Focus is on meeting community-based recreational needs, as well as preserving unique landscapes and open spaces.	Determined by the quality and suitability of the site. Usually serves the entire community.	As needed to accommodate desired uses. Usually a minimum of 50 acres, with 75 or more acres being optimal.
Natural Resource Areas	Lands set aside for preservation of significant natural resources, remnant landscapes, open space, and visual aesthetics/buffering.	Resource availability and opportunity.	Variable.
Greenways	Effectively tie park system components together to form a continuous park environment.	Resource availability and opportunity.	Variable.
Sports Complex	Consolidates heavily programmed athletic fields and associated facilities to larger and fewer sites strategically located throughout the community.	Strategically located community-wide facilities.	Determined by projected demand. Usually a minimum of 25 acres, with 40 to 80 acres being optimal.
Special Use	Covers a broad range of parks and recreation facilities oriented toward single-purpose use.	Variable – dependent on specific use.	Variable.
Private Park/ Recreation Facility	Parks and recreation facilities that are privately owned yet contribute to the public park and recreation system.	Variable – dependent on specific use.	Variable.

*Parks, Recreation, Open Space and Greenway Guidelines.* A Project of the National Recreation and Park Association and the American Academy for Park and Recreation Administration. A Publication of the National Recreation and Park Association. James D. Mertes, Ph.D., CLP and James R. Hall, CLP

MINI PARKS

**Condition and Capacity Report – Chestnut Ridge Park**

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry (non-irrigated); Young trees; well maintained
Play Areas		X			X		Small greenspace for simple games
Sports Fields							None at location
Sports Courts							None at location
Walks/Trails							None at location
Play Equipment			X		X		New playstructure
Structures		X			X		Picnic shelter
Picnic Facilities			X		X		Picnic tables and a trash can
Drinking Water				X			No access to water on the premises
Restrooms							No permanent facilities, small park not needed
Parking					X		On Street parking in residential neighborhood, specific facilities are not needed
Lighting		X		X			No lights other than street lights
Benches			X		X		Benches surround playstructure
Signage				X			none



## Condition and Capacity Report – Crystal Creek Park

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry (non-irrigated); Young trees; well maintained
Play Areas		X			X		Small greenspace for simple games
Sports Fields							None at location
Sports Courts							None at location
Walks/Trails							None at location
Play Equipment			X		X		New playstructure
Structures		X			X		Picnic shelter
Picnic Facilities			X		X		Picnic tables and a trash can
Drinking Water				X			No access to water on the premises
Restrooms							No permanent facilities, small park not needed
Parking					X		On Street parking in residential neighborhood, specific facilities are not needed
Lighting		X		X			No lights other than street lights
Benches			X		X		Benches surround playstructure
Signage				X			none



### Condition and Capacity Report – Founder’s Memorial Park

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry (non-irrigated); Young trees; well maintained
Play Areas		X			X		Small greenspace for simple games
Sports Fields							None at location
Sports Courts							None at location
Walks/Trails			X				Small trail system through park
Play Equipment				X			None at location
Structures			X		X		Picnic shelter
Picnic Facilities			X		X		Picnic tables and a trash can
Drinking Water				X			No access to water on the premises
Restrooms							No permanent facilities, small park not needed
Parking		X			X		Two-stall gravel parking lot and on street parking available, specific facilities are not needed
Lighting	X			X			No lights other than street lights
Benches				X			No benches at location
Signage		X			X		Small wood pole sign



NEIGHBORHOOD PARKS

Condition and Capacity Report – Peterson Memorial Park

Park Component	Condition		Capacity		Notes
	Poor	Fair/Good	Under	At/Over	
Vegetation (Trees, shrubs, turf)		X			Turf dry, lots of large old growth trees
Play Areas		X		X	Open greenspace
Sports Fields				X	None at this location
Sports Courts		X		X	Basketball Court
Walks/Trails		X		X	Walking Trails runs the perimeter
Play Equipment		X		X	Playstructure, swingset, slide, and various other equipment, separate play areas/equipment for older and younger children
Structures		X		X	2 Picnic shelters, a gazebo, swimming pool
Picnic Facilities		X		X	Numerous Picnic tables and grills
Drinking Water		X		X	Water spigots
Restrooms			X		None at this location
Parking		X		X	Off street parking on east and west ends of the park
Lighting		X		X	Several pole lights
Benches		X		X	At various locations around the park
Signage		X		X	
Misc		X		X	Flagpole Memorial, 2 Flower Garden areas



### Condition and Capacity Report – Lincoln Place Park

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry, new trees planted near playstructure
Play Areas		X			X		Large open greenspace
Sports Fields							None at this location, room for several practice fields
Sports Courts							None at this location
Walks/Trails		X		X			Path leading out to road between houses
Play Equipment			X	X			Large playstructure
Structures							None at this location
Picnic Facilities		X			X		Picnic table
Drinking Water							None at this location
Restrooms							None at this location
Parking				X			On street parking only
Lighting				X			None at location
Benches			X		X		Benches at edge of the playstructure
Signage							None at this location



### Condition and Capacity Report – Plum Creek Park

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry, new trees planted near playstructure and trail, older trees along the edges separating the park from homes
Play Areas		X			X		Large open greenspace
Sports Fields		X			X		1 softball/baseball field, 2 portable soccer goals
Sports Courts							None at this location
Walks/Trails		X			X		Trail leading through the park
Play Equipment			X		X		Large playstructure swing set
Structures							None at this location
Picnic Facilities				X			None at this location
Drinking Water							None at this location
Restrooms				X			None at this location
Parking					X		Small off-street parking lot
Lighting				X			None at location
Benches			X		X		Benches at edge of the playstructure
Signage							None at this location



COMMUNITY PARKS

Condition and Capacity Report – North Park

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry, Lots of Large old growth trees
Play Areas		X			X		Several areas of open greenspace
Sports Fields		X			X		3 softball/baseball fields, 1 tee ball field, 1 regulation baseball stadium
Sports Courts		X			X		2 lit tennis courts, 1 lit basketball court
Walks/Trails		X		X			None at this location
Play Equipment	X			X			Swing set, tornado slide, and merry-go-round, toddler play equipment
Structures							None at this location
Picnic Facilities		X		X			One picnic shelter
Drinking Water					X		Drinking fountains
Restrooms	X				X		Permanent restrooms
Parking					X		Two off-street parking lots
Lighting					X		Parking was lit and the courts and some of the fields
Benches				X			None at this location
Signage				X			None at this location
Miscellaneous		X			X		15 lit horseshoe pits



SCHOOL FACILITIES (ELEMENTARY SCHOOLS ARE CONSIDERED NEIGHBORHOOD PARKS)

**Condition and Capacity Report – Gretna Elementary**

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry
Play Areas		X			X		Large paved multipurpose play areas
Sports Fields		X			X		1 Little League Field
Sports Courts		X			X		1 Outdoor Basketball Court
Walks/Trails							None at this location
Play Equipment			X		X		Playground Equipment
Structures							None at this location
Picnic Facilities							None at this location
Drinking Water							None at this location
Restrooms							None at this location
Parking		X			X		Paved parking lot
Lighting							None at this location
Benches							None at this location
Signage		X			X		School sign



### Condition and Capacity Report – Squire John Thomas Elementary School

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry
Play Areas		X			X		Large open greenspace
Sports Fields		X			X		1 soccer field
Sports Courts		X			X		1 basketball court, 4 tennis courts
Walks/Trails							None at this location
Play Equipment			X		X		Playground equipment
Structures							None at this location
Picnic Facilities							None at this location
Drinking Water							None at this location
Restrooms							None at this location
Parking		X			X		Paved parking lot
Lighting							None at this location
Benches							None at this location
Signage		X			X		School sign



## Condition and Capacity Report – Palisades Elementary School

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry
Play Areas		X			X		Large open greenspace
Sports Fields		X			X		1 soccer field, 2 ball fields
Sports Courts		X			X		1 basketball court
Walks/Trails							None at this location
Play Equipment			X		X		Playground equipment
Structures							None at this location
Picnic Facilities							None at this location
Drinking Water							None at this location
Restrooms							None at this location
Parking		X			X		Paved parking lot
Lighting							None at this location
Benches							None at this location
Signage		X			X		School sign



### Condition and Capacity Report – Gretna Middle School

Park Component	Condition Poor/Fair/Good			Capacity Under/At/Over			Notes
Vegetation (Trees, shrubs, turf)		X					Turf dry, irrigated sports fields
Play Areas		X			X		Greenspace/Practice fields
Sports Fields		X			X		Track, 1 football field, baseball field
Sports Courts							None at this location
Walks/Trails							None at this location
Play Equipment							None at this location
Structures							None at this location
Picnic Facilities							None at this location
Drinking Water							None at this location
Restrooms							None at this location
Parking		X			X		Paved parking lot
Lighting							None at this location
Benches							None at this location
Signage		X			X		School sign



### Condition and Capacity Report – Gretna High School

Park Component	Condition		Capacity			Notes
	Poor	Fair/Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X				Turf dry, irrigated sports fields
Play Areas		X		X		Greenspace/Practice fields
Sports Fields		X		X		Track/football stadium, baseball field, softball field, little league field, soccer stadium
Sports Courts						None at this location
Walks/Trails						None at this location
Play Equipment						None at this location
Structures						None at this location
Picnic Facilities						None at this location
Drinking Water						None at this location
Restrooms						None at this location
Parking		X		X		Paved parking lot
Lighting						None at this location
Benches						None at this location
Signage		X		X		School sign



SPORTS COMPLEX

Condition and Capacity Report – Leo Royal Park

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Irrigated fields
Play Areas		X			X		Open greenspace
Sports Fields			X		X		5 soccer fields of various sizes
Sports Courts							None at this location
Walks/Trails			X		X		Running through the complex
Play Equipment							None at this location
Structures							None at this location
Picnic Facilities			X		X		Picnic shelter
Drinking Water		X			X		Drinking fountain
Restrooms			X		X		Permanent restrooms
Parking		X			X		Paved parking lot
Benches			X		X		Bleachers along fields
Signage			X		X		Wood sign with landscaping



SPECIAL USE FACILITIES

**Condition and Capacity Report – Swimming Pool**

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)		X					Turf dry
Play Areas		X			X		Open greenspace located in Peterson Memorial Park
Sports Fields							None at this location
Sports Courts							None at this location
Walks/Trails							None at this location
Play Equipment							None at this location
Structures			X		X		Bath house, Concession stand
Picnic Facilities							None at this location
Drinking Water		X			X		Drinking fountain
Restrooms		X			X		Permanent restrooms
Parking		X			X		Paved parking lot
Lighting		X			X		Pool area is lit
Benches			X		X		Lounge chairs surrounding the pool
Signage			X		X		



URBAN / REGIONAL PARK

**Condition and Capacity Report – The Fields at Gretna\***

Park Component	Condition			Capacity			Notes
	Poor	Fair	Good	Under	At	Over	
Vegetation (Trees, shrubs, turf)							
Play Areas					X		Open greenspace east of dedicated ballfields
Sports Fields							Four small and four large baseball/softball fields, four soccer/football fields planned
Sports Courts							None at this location
Walks/Trails							Trails surrounding fields and connecting to other trails planned
Play Equipment							Play equipment between fields planned
Structures			X		X		Concession stand - new
Picnic Facilities			X	X			Picnic shelter
Drinking Water							Drinking fountain planned
Restrooms							Permanent restrooms planned
Parking					X		Gravel parking lots
Lighting							Lighted ballfields and parking lots are planned
Benches							Seating for games and benches along trails planned
Signage							

\*Currently under construction



*Perspective Rendering facing Southeast Direction*

source: Adopted Master Plan

## RECOMMENDATIONS

Recommendations for Gretna’s park and recreation system are based upon a number of factors, including:

- National standards,
- Regional opportunities and plans, and
- Identified needs and desires of the City of Gretna

Community input has shown that Gretna residents who attended the focus group workshops and town hall meetings are somewhat satisfied with the amount and quality of the existing facilities, programs, and activities available, but there is definitely room for improvement and expansion. Recommendations are provided for existing and proposed parks, and additional recreation opportunities such as adding an indoor facility and/or an additional sports complex.

These recommendations may change over time, but will provide a basis for developing individual park master plans/layouts and developing the City’s Parks Action Plan. Implementing such recommendations will improve and expand park and recreation facilities and activities for all residents of the City of Gretna and the surrounding area.

*Figure 27: Parks and Trails Map* shows trails, both existing and future, and existing and proposed parks, along with the service areas for mini-parks, neighborhood parks, community parks, and urban/regional parks. Park locations are centralized inside the service areas. Locations of the proposed parks as shown on the maps are approximate. Parks are shown in the general area where the facilities may be located. These proposed locations may shift or be altered when the planning and development of subdivisions or roadways becomes more detailed. Likewise, the type of a park may change based upon changing situations.

## REQUIRED AND OPTIONAL FACILITIES, AMENITIES, AND SERVICES

When new development occurs and there is to be a park incorporated within the new development, Table 36 will serve as a guide for the City as to what should be put in the park based on the type of park that is being built. These required and optional facilities and amenities for all park classifications listed in *Table 36* are divided into three categories, including:

- Park and Recreation Facilities, such as play structures and basketball courts
- Park and Recreation Amenities, such as security lighting and drinking fountains
- Park and Recreation Services, such as reservation requirements and maintenance

TABLE 36: REQUIRED AND OPTIONAL FACILITIES, AMENITIES, AND SERVICES

Facilities, Amenities, and Services	Mini-Park	Neighborhood Park	Community Park	Large Urban/Regional Park	School-Park	Special Use Facility	Sports Complex	Trail/Greenway
<b>Park and Recreation Facilities</b>								
Play Equipment/Structures	R	R	R	O	R	O	O	O
Open Play Area	R	R	R	R	R	O	O	O
Soccer Fields	NA	O	O	O	O	O	O	NA
Softball Fields	NA	O	O	O	O	O	O	NA
Baseball Fields	NA	O	O	O	O	O	O	NA
Paved Multi-use Areas	O	R	R	O	R	O	O	NA
Tennis Courts	O	O	O	O	O	O	O	NA
Basketball Courts	O	O	O	O	O	O	O	NA
Volleyball Courts	O	O	O	O	O	O	O	NA
Multi-Purpose Trails	O	R	R	R	R	O	O	O
Picnic Facilities (shelters)	R	R	R	R	O	O	O	O
Special/Unique Features	O	R	R	R	O	O	R	O
Natural Areas	O	O	O	R	O	O	O	O
Trees/Shaded Areas	R	R	R	R	R	O	R	R
Special Use Facilities	NA	O	O	O	O	R	O	O
Swimming Pool	NA	O	O	O	O	O	O	NA
Aquatic Center	NA	NA	O	O	O	O	O	NA
Wading Pool	O	O	O	O	O	O	NA	NA
Ice Skating Rink	NA	O	O	O	O	O	NA	NA
Amphitheater/Outdoor Gathering Area	NA	O	O	O	O	O	NA	O
Arboretum/Botanical Gardens	NA	O	O	O	NA	O	O	O
Fine Arts Facility/Public Art Displays	NA	NA	O	O	NA	O	NA	O
Community Center or Indoor Rec.	NA	O	O	O	O	O	O	NA
Camping Facilities (RV facilities)	NA	NA	NA	O	NA	O	NA	NA
Dog Park	NA	NA	O	O	NA	O	NA	O
Horseshoes	O	O	O	O	O	O	O	NA
Disc/Frisbee Golf	NA	O	O	O	O	O	O	O
Roller Hockey	O	O	O	O	O	O	O	NA
Football/Rugby Field	NA	NA	O	O	O	O	O	NA
Outdoor Exercise Circuit	NA	O	O	O	O	O	O	O
Skating Facility (in-line/skateboard)	NA	O	O	O	NA	O	O	O
High-Risk Area	NA	NA	O	O	NA	O	O	NA
Golf Course	NA	NA	O	O	NA	O	O	O
Youth Sports Complex	NA	O	O	O	NA	O	O	NA
Competitive Sports Facility	NA	NA	O	O	NA	O	O	NA

Facilities, Amenities, and Services (continued)	Mini-Park	Neighborhood Park	Community Park	Large Urban/Regional Park	School-Park	Special Use Facility	Sports Complex	Trail/Greenway
<b>Park and Recreation Amenities</b>								
Security Lighting	R	R	R	R	R	R	R	O/R*
Activity Lighting	O	O	R	O	O	O	R	NA
Public Telephones	O	O	R	R	R	R	R	O
Off Street Parking	O	R	R	R	R	R	R	O/R*
Bike Racks	R	R	R	R	R	R	R	O/R*
Restrooms	O	R	R	R	R	O	R	O/R*
Drinking Fountains	R	R	R	R	R	R	R	O/R*
Benches	R	R	R	R	R	R	R	R
Picnic Tables	O	R	R	R	O	O	R	O
Signage	R	R	R	R	R	R	R	R
Information Kiosks	NA	NA	O	O	NA	O	O	O
ADA Accessibility	R	R	R	R	R	R	R	R
<b>Park and Recreation Services</b>								
Security**	R	R	R	R	R	R	R	R
Emergency Telephone Service	O	O	O	O	O	O	O	O
Reservations for Facility Use (shelters, group picnics, sports leagues, for-profit use)	R	R	R	R	R	R	R	NA
Activities/Facilities for Groups, Companies, Teams	NA	O	R	R	O	O	R	O
Special Events (programs, concerts, fairs)	O	O	O	O	O	O	O	O
Facilities and Grounds Maintenance	R	R	R	R	R	R	R	R
R - Required Facility/Service O - Optional Facility/Service NA - Not Appropriate * Optional for Greenway, Required for Trail  ** May include, but not limited to, police patrols, private security, neighborhood watches, park design to eliminate hidden places, structure design and lighting, and/or location markers on trail.  Note: This does not preclude the addition of other unlisted facilities and services as optional.								

Required and optional facilities, amenities, and services are to serve as a guide for the City of Gretna. It is the responsibility of the City to determine which facilities, amenities, and services are or are not feasible in existing and proposed parks. The required facilities are recommended for existing parks, but due to physical limitations and space constraints they may not be added. In proposed parks, future demand for certain facilities, amenities, and services and the recreational preferences of users may change over time. Therefore, these guidelines may change or be revised to respond to the future demand of Gretna residents. Each park will be looked at individually to determine the physical

capacity of providing basic requirements and to determine needs and wants of residents served by that park. Each park is different and this difference will be considered when determining which facilities will be included in each park. The City shall strive to provide the basic requirements in all of its parks and careful consideration shall be given to each proposed park and trail through the City's approval of such facility.

Even though the list of facilities, amenities, and services is extensive, it is likely other items not listed will be requested to be included in the park and recreation system. Each new facility and service requested shall be analyzed according to public demand, site/location criteria, operating implications, and other relevant criteria.

## PARK AND TRAIL RECOMMENDATIONS

### **Mini-Parks**

It is the goal of Gretna to provide the required facilities and services where possible in existing and proposed parks. There are three existing mini-parks within Gretna's one-mile jurisdiction. Additional mini-parks are not recommended because many new single-family homes offer their own play equipment and facilities that act in a similar manner as mini-parks. The following recommendations pertain to the existing mini-parks.

As general guidelines, mini-parks should strive to include the following:

- a site between 2,500 square feet to one acre
- a service area of a maximum  $\frac{1}{4}$  mile radius
- a site with a less than 4% slope
- a site that takes advantage of vegetation and other natural resources of the area
- a site that is located in residential areas

### **Existing Mini-Parks**

#### **Chestnut Ridge Park**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities where feasible
- Provide ADA accessibility and ADA facilities where feasible
- Continue to provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

#### **Crystal Creek Park**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities where feasible
- Provide ADA accessibility and ADA facilities where feasible
- Continue to provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Founder's Memorial Park**

- Develop a park master plan/layout through public input
- Construct additional walking trail through the park connecting to existing trail and adjacent neighborhoods
- Provide aesthetic and identification amenities where feasible.
- Provide ADA accessibility and ADA facilities where feasible
- Continue to provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Neighborhood Parks**

It is the goal of the City of Gretna to provide the required facilities and services where possible in existing and proposed neighborhood parks. In addition to the existing neighborhood parks, there are nine proposed neighborhood parks to be built as they are needed due to the expansion of the City. Neighborhood parks should be the backbone for the City's park and recreation system comprising the vast majority of park space within the City.

As general guidelines, neighborhood parks should strive to have the following:

- a site of approximately five to ten acres
- a service area with a maximum ½ mile radius
- not more than 50% of the site should have a slope greater than 4%
- a site that takes advantage of vegetation and other natural resources of the area
- a site located in primarily residential areas

**Existing Neighborhood Parks****Peterson Memorial Park**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Lincoln Place Park**

- Develop park master plan/layout through public input
- Add practice sports field to the large open area
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Plum Creek Park**

- Develop park master plan/layout through public input
- Construct new rest rooms
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Gretna Elementary School-Park** (See also School-Parks)

- Develop joint use agreements between the school district and City that would establish rules and criteria
- Work with the school district to develop park master plan/layout with public input
- Provide aesthetic and identification amenities and rules of play
- Provide ADA accessibility and ADA facilities where feasible
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Squire John Thomas Elementary School-Park** (See also School-Parks)

- Develop joint use agreements between the school district and City that would establish rules and criteria
- Work with the school district to develop park master plan/layout with public input
- Provide aesthetic and identification amenities and rules of play
- Provide ADA accessibility and ADA facilities where feasible
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Palisades Elementary School-Park** (See also School-Parks)

- Develop joint use agreements between the school district and City that would establish rules and criteria
- Work with the school district to develop park master plan/layout with public input
- Provide aesthetic and identification amenities and rules of play
- Provide ADA accessibility and ADA facilities where feasible
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**Proposed Neighborhood Parks** (Those located within Gretna’s Extraterritorial Jurisdiction)

**N-1**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-2**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-3**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-4**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-5**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-6**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-7**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-8**

- Develop park master plan/layout in conjunction with the NRD and through public input to compliment proposed lake/potential required detention site
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**N-9**

- Develop park master plan/layout through public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

### **Community Parks**

As previously stated it is the goal of the City of Gretna to provide the required facilities and services where possible in existing and proposed parks. North Park is the only existing community park and there are no additional ones recommended. There are, however, some recommendations for North Park.

As general guidelines, community parks should strive to have the following:

- a site of approximately 30 to 50 acres
- a service area with a maximum three mile radius, typically a one to two mile radius
- surrounding land uses are primarily residential
- located adjacent to arterial or collector street(s)

### **Existing Community Park**

#### **North Park**

- Review and alter existing park master plan/layout with public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

### **Special Use Facilities**

It is the goal of the City of Gretna to provide the required facilities and services where possible in existing facilities. There is one proposed special use facility, an indoor facility. The facility is not shown on *Figure 27 Parks and Trails Map* in a specific location and could be placed anywhere the city deems to be feasible.

As general guidelines, special use facilities should strive to have the following:

- size of the site is variable
- a service area that is community-wide
- surrounding land uses are variable

## Existing Special Use Facilities

### Gretna Swimming Pool

- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

## Proposed Special Use Facilities

### Indoor Multi-Purpose Facility

- Develop park master plan/layout through public input for such facility
- Suggested amenities include gym space, walking track, fitness space and a new maintenance shop
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

## Large Urban/Regional Parks

In striving to provide recreational opportunities to Gretna's entire jurisdiction, the city has an opportunity to work with Papio-Missouri River Natural Resources District to develop a regional park in conjunction with a flood control structure north of Gretna. This location is shown in Figure 27 Parks and Trails Map. A note should be made that the lake plans have not been fully developed by the Papio NRD and the Papillion Creek Watershed Partnership. An additional large urban/regional park is suggested south of town in conjunction with The Fields at Gretna as a destination park. Such large urban/regional park could have an amusement facility, outdoor theatre, arboretum, picnic areas, trails as well as an additional sports complex.

As general guidelines, regional parks should strive to have the following:

- a site of approximately 50 to 100+ acres
- a service area of the entire community and surrounding rural areas
- surrounding land uses are primarily agricultural/open space
- located adjacent to arterial or collector street(s)

## Existing Large Urban/Regional Parks (and Proposed Expansion)

### The Fields at Gretna

- Continue construction and development of soccer/football fields and softball/baseball complex on the west 80 acres
- Develop master plan for the east 80 acres with public input
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Connect park to Regional trail system
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

## Proposed Large Urban/Regional Parks

### U-1

- Develop park master plan/layout in conjunction with the NRD and with public input to compliment the proposed lake/potential required detention site
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

## Sports Complexes

It is the goal of the City to provide the required facilities and services where possible in existing and proposed sports complexes. There is a proposed sports complex planned at the Fields at Gretna that is located on 160 acres and being developed as a large urban/regional park.

As general guidelines, sports complexes should strive to have the following:

- size of the site is greater than 25 acres
- a service area that is community-wide
- surrounding land uses are variable
- adjacent to arterial or collector street(s)
- adequate parking and accessibility

## Existing Sports Complexes

### Leo Royal Park

- Review and alter existing park master plan/layout with public input, by expanding and making improvements
- Design and incorporate a Frisbee golf course
- Provide aesthetic and identification amenities
- Provide ADA accessibility and ADA facilities where feasible
- Connect park with adjacent residential development with trails
- Provide basic requirements that are feasible and optional additions that are desired (See *Table 36*)

**School-Parks**

The guidelines for school-parks are listed in *Table 36: Required and Optional Facilities and Services*. For the proposed school-parks, it is the goal of the City to work with the school district to provide the required facilities and services where possible. Adjacent land to the proposed school sites may be required to supply required facilities and services to fulfill provisions of a neighborhood park without building an additional park. These needed provisions may include parking areas, playsets for toddlers, drinking fountains, and restrooms. The goal is not to duplicate facilities but make the recreational use of the land more efficient.

As general guidelines, school-parks should be constructed as neighborhood parks or in conjunction with neighborhood parks and should strive to have the following:

- size of the site is variable (typically around five acres)
- service area is variable (typically ½ mile radius)
- a site that takes advantage of the trees and other natural resources of the area
- located primarily in residential zoned areas

Sites that include schools should be large enough to accommodate school needs and neighborhood park uses, where feasible. A committee of City personnel and representatives of the school district should be established to discuss joint use facilities, joint maintenance possibilities, and joint improvement possibilities to maximize community use of facilities. The committee should also establish a process whereby new schools that may fall under formal joint use agreements are planned and designed jointly by the school district and the city. Master plans for each school park should be developed through public input by such committee.

**Proposed School-Parks.**

(See Neighborhood Parks)

## Green Streets

Green streets are streets designed to extend a park-like appearance through the community and serve to create an interconnected network of parks, recreation areas, schools, and other civic facilities. Green streets should be designed or redesigned when feasible to have one or more of the following elements:

- one or more rows of trees along both sides of the roadway (along city right-of-way or on private property)
- one or more rows of trees down the center of the street/roadway located within islands.
- space for wide sidewalks or off-street trails on one or both sides of the roadway
- no overhead utility wires that interfere with the growth of overstory trees

Green streets may include signage, benches, nodes, and landscaping. Existing street right-of-way widths would dictate specific design on a street-by-street basis. *Figures 24, 25, and 26* show typical cross-sections of the three types of green streets. The hierarchy of green streets is neighborhood, secondary, and primary green streets. Neighborhood green streets are through streets within a neighborhood, secondary green streets are traffic collector routes, and primary green streets are major traffic arteries. *Figure 26* shows an alternative cross-section with plant material in the center of the street.

*Figure 24: Neighborhood Green Street Section (Typical)*

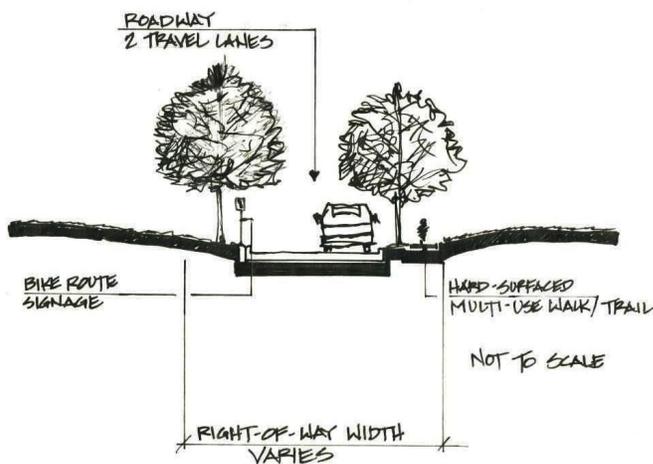


Figure 25: Secondary Green Street Section (Typical)

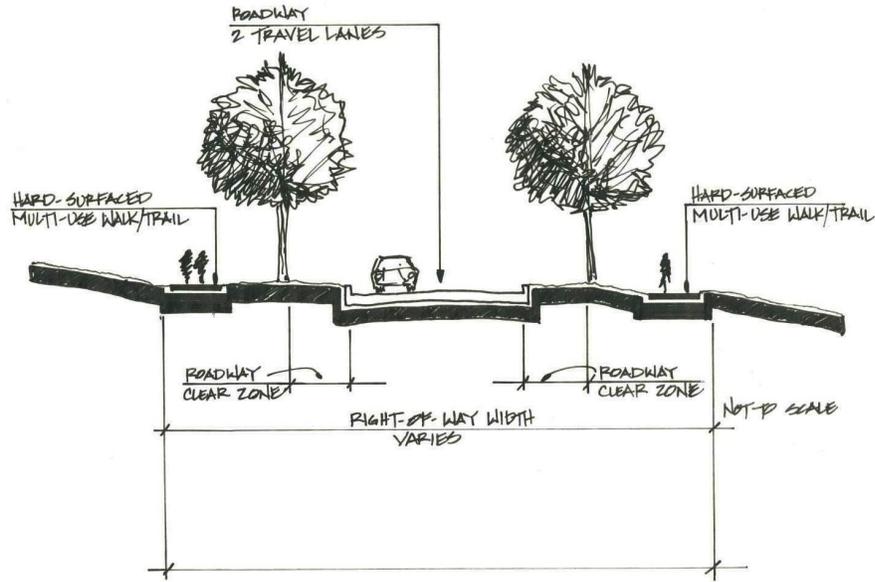
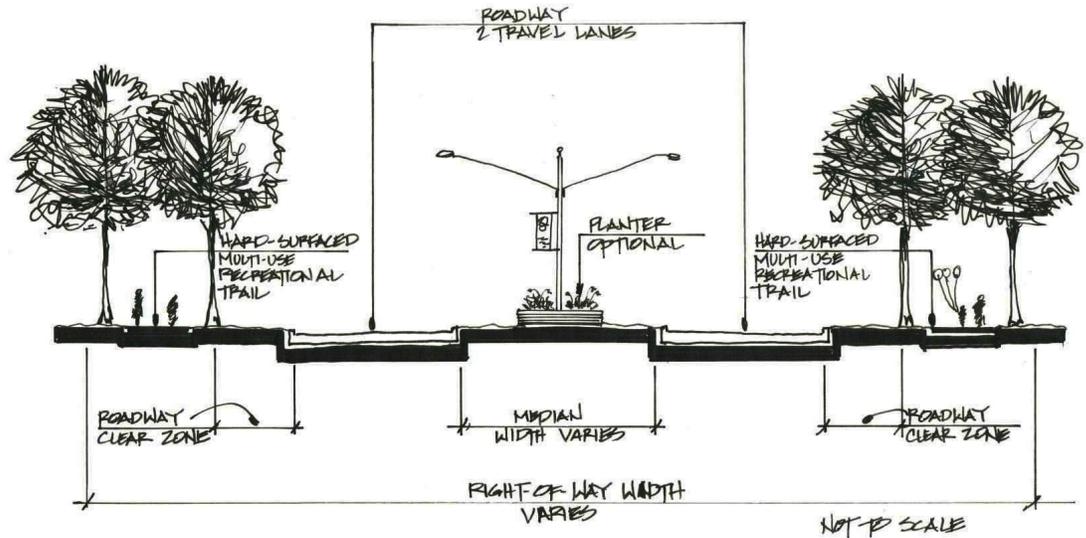


Figure 26: Primary Green Street Section (Typical)



All proposed street trees should have approval by City staff on species type and location regarding all existing and proposed infrastructure. Tree height near power lines and root systems near sewer and water lines shall be considered. All proposed green streets should be planned/designed accordingly with all existing or proposed utilities. With typical green street sections, the existing walk on both sides of the street should be widened to eight feet, extending it toward the street. If there is less than two feet remaining between the widened walk and the curb, it is recommended this strip also be paved, since an area less than two feet wide could likely not be maintained efficiently and effectively in grass or any other plant material. An additional five-foot easement should be acquired from adjoining private property, if necessary, in which street trees should be planted. Trees should be located three feet from the edge of the walk, spaced at approximately forty-foot intervals.

Allowances should be made for existing items in the R.O.W., such as driveways or fire hydrants. In summary, green streets that cannot follow the typical sections should include the following, starting from the street curb:

- grass strip, if more than two feet between curb and walk (if less than two feet from street to walk, strip should be paved)
- eight-foot wide walk
- five-foot easement, in which street trees are planted three feet from the walk

Implementation of designated green streets in Gretna would ensure that the tree-lined streets become part of the landscape throughout the City. It would also help create a pedestrian and bicycle movement network through the City that would link parks and public recreational areas together as a continuous, interconnected system. The City should consider implementation of such green streets where feasible.

## PARK LAND DEDICATION/FEE

### **New Development Dedication and/or Fee**

As a way of assuring adequate land is available for new parks and facilities and that all existing and new recreational amenities are properly maintained, the City of Gretna has implemented a policy for the dedication of land for such parks and facilities and a park fee in-lieu of such dedication.

When there is a new subdivision platted within Gretna's jurisdiction, the City shall require either a dedication of parkland (for parks, trails, open space, or other recreational facilities) and/or a park dedication fee. Such dedication policy is made part of the Gretna Subdivision Regulations and Master Fee Ordinance. Logistics of the recreational facility type and its exact location shall be determined through the pre-application meetings of the platting process. The parcel of ground to be dedicated or the amount of fee to be paid will be negotiated and written into the subdivision agreement. Master plans for such parks and facilities shall be laid out at time of preliminary plat and approved at time of final platting and prior to execution/filing of final plat. Any required park development fees shall be submitted to the City at the time of final platting and placed into the City's established park fund. Such funds shall be used for the acquisition of land, development, and maintenance of Gretna's park system.

When deciding whether or not the developer should dedicate land, pay the fee, or both, the City and developer shall consult Figure 27 Parks and Trails Map within this Plan. If there is a future park, trail, open space, or other recreational facility located in whole or in part of the new subdivision, the City may require that the subdivider dedicate land for such improvements. Development and maintenance of each park, trail, etc. shall be determined in the individual subdivision agreements. Any land that is dedicated should be buildable land (non-floodplain or non-floodway designated ground or areas of less than 15% slope) and be of sufficient size for the type of park or recreational facility designated in this Plan.

If the Parks and Trails Map does not identify a proposed park or facility in the platted area and the subdivider is directed to provide the City with fee payment in lieu of parkland dedication, then such subdivider shall pay a park development fee based upon a set multiplier. Said multiplier and corresponding fees are identified in Gretna’s Master Fee Ordinance and Subdivision Regulations and shall be paid prior to execution of the final plat.

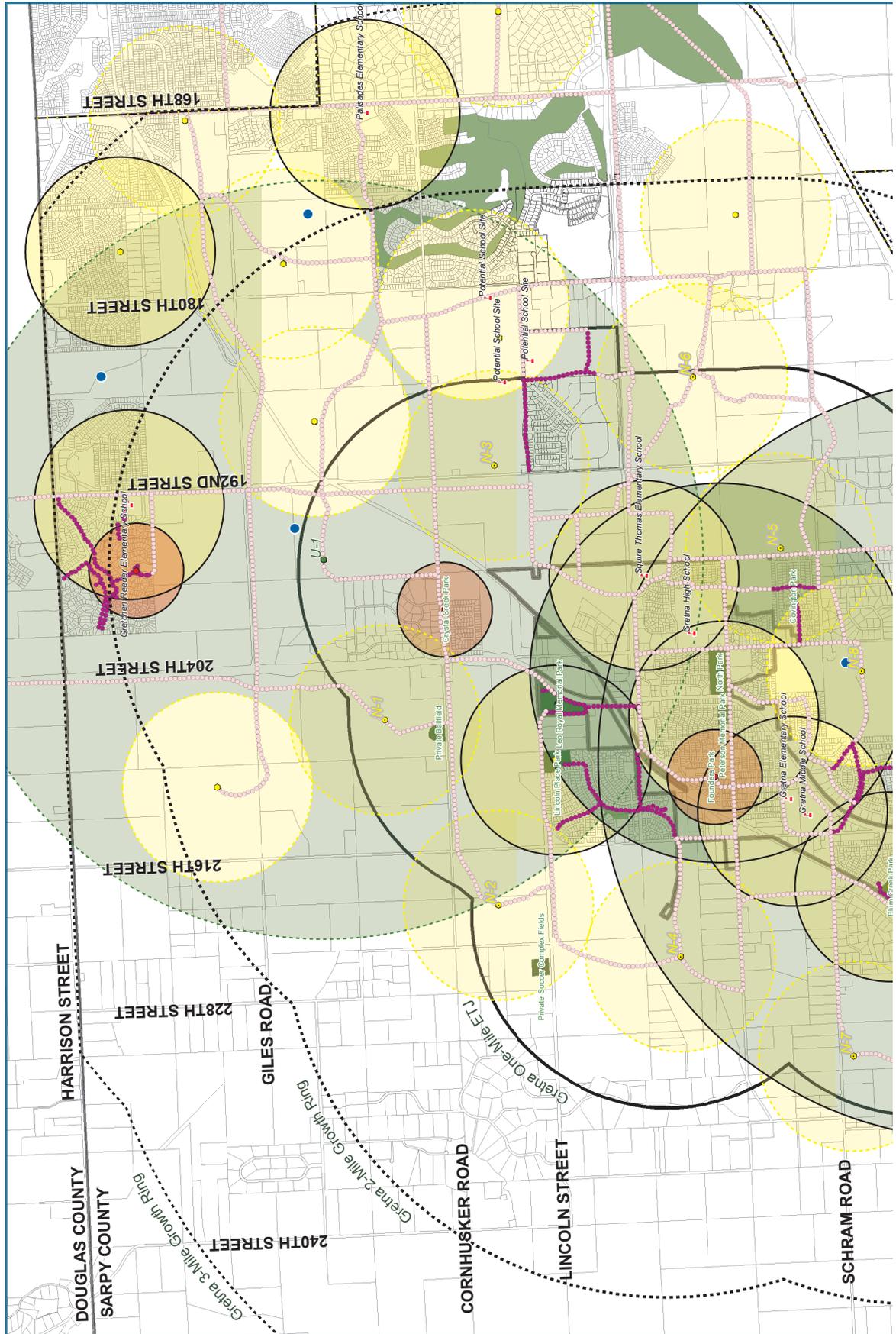
## Park Maintenance

The City should adopt a maintenance policy for each level of park and facility. A maintenance standards schedule could be developed that places each park and recreational facility into one of three levels for maintenance. Level 1 would be the most intense level of care, with Level 3 being the most natural and least maintenance intense areas. For example, mini-parks should be maintained higher than some greenways. The park department may not only create levels within the system but also may create levels of care with individual parks. For example, the park entry and sports fields may be Level 1, the majority of the park may be Level 2, and the creek or wooded area may be Level 3. A maintenance policy and detailed scheduling may include the following areas of service within each park or recreational facility:

- Lawn care
- Sports turf care
- Litter control
- Lighting
- Hard/multi-purpose surfaces maintenance
- Graffiti control
- Repairs
- Inspections
- Tree and shrub care
- Native grass care and control
- Floral care (perennials and annuals)
- Restroom maintenance
- Ballfield maintenance and preparation
- Fence and gate construction and repairs
- Playground maintenance
- Trails
- Waterway management
- Drainage structures
- Pedestrian bridges
- Retaining walls
- Site amenities (picnic tables, goals, etc.)
- Picnic shelters (including reservations)
- Signage
- Unique/Special Features (historical, natural, etc.)
- Concessions

Policies should also be adopted to address signage and color schemes for parks and recreational facilities located with Gretna's jurisdiction. Signs are recommended to be of the same style and color for all Gretna parks so that they represent one park system. Suggested colors for park shelter, playground structure, benches, etc. should include greens, tans, browns, and maroon colors. Such colors are less obtrusive to the park environment. All park plans with proposed subdivisions shall be submitted for approval, including all proposed structures, materials, and colors.

FIGURE 27: PARKS AND TRAILS MAP





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# Implement Gretna

Introduction

Implementation Tools

Achieving Gretna's Future Plan

Annexation

Plan Maintenance

Methods for Evaluating Land  
Development Proposals

Recommendations



## INTRODUCTION

Implementation refers to the objectives, policies and actions that have been identified to carry out PlanGretna. This portion of the comprehensive plan includes actions designed to improve the long-range planning process, strengthen links between the plan and capital improvement budgeting, establish a process reporting system to monitor the progress of PlanGretna, and a schedule for updating and amending the plan in the future.

Successful comprehensive plans are ones that are implemented. Developing a plan is relatively easy compared to the implementation of it. What happens with the plan, how it is used in day-to-day decision-making, and the extent to which it is followed over time all influence its overall success. PlanGretna is a living, breathing document; it is not the end of a planning process but the beginning of a coordinated effort to direct growth and development in ways that are important to the citizens of Gretna.

Rarely will a single tool or category of tools be sufficient to achieve the goals in a comprehensive plan. Most policies, and most of the preferred mix of land uses shown on the future land use map, require the use of several tools from different categories in order to be realized, obtained, and sustained. The City of Gretna must develop its own set of implementation tools and strategies, recognizing that each has unique strengths and weaknesses.

## IMPLEMENTATION TOOLS

The City of Gretna has a number of tools at its disposal to shape development patterns, protect natural resources and human infrastructure, and protect the quality of life for its residents. Implementation strategies can be separated into several distinct tool categories and programs, each with its distinct characteristics that make it suitable for specific goals and circumstances.

## SUPPORT PROGRAMS

Five programs will play a vital role in the success of PlanGretna.

1. Capital Improvements Financing--an annual predictable investment plan that uses a six to ten-year planning horizon to schedule and fund projects integral to the plan's implementation.
2. Zoning Regulations--updated land use districts and regulations, including design guidelines, can allow the City to provide direction for future growth.
3. Subdivision Regulations--establish criteria for erosion control and runoff and dividing land into building areas, parks, trails, utility easements, and streets. Implementing the Transportation Plan is a primary function of subdivision regulations.
4. Plan Maintenance--an annual and five-year review program will allow the City flexibility in responding to growth and a continuous program of maintaining the plan's viability.
5. Strategic Plan-- The City has adopted a Strategic Plan and Goal Prioritization that identifies the projects of most importance along with a set of action items. This plan should be updated on a regular basis.

## PUBLIC EDUCATION

In addition to the identified programs, broad public support and involvement is necessary in the development and use of any implementation policy or program. If adequate support is to be developed, a program educating residents is necessary. People who understand the needs and ways of meeting those needs of the community must take the initiative to stimulate the interest and the understanding required to ensure action is taken. The governing body of Gretna should annually strive to implement an active public participation process by creating an educational process on land use issues. The City should continue to use its website and make the comprehensive plan and development regulations available online. The use of web based social media will also be useful in going valuable feedback. Ongoing education and promotion will be an important factor in sustaining interest and motivation from community members.

Some of the objectives of the comprehensive plan cannot be achieved unless the actions of two or more public agencies or private organizations can be coordinated. Frequently, constraints prevent organizations from working with one another (i.e. financial resources, legal authority, restriction of joint uses of facilities, etc). Efforts should be made to bridge this gap with open communication, cooperation and the realization that the issue at hand could benefit the health, safety, and general welfare of the residents in Gretna.

## MANAGING PUBLIC PROCESSES AND RESOURCES

Public investments also include the time and effort that go into management decisions for infrastructure, public services, public lands, and public processes. Such investment or decisions can include:

- Coordinating capital improvements planning throughout all departments to meet the comprehensive plan goals and remain consistent with plan policies.
- Investments and improvements in water, wastewater, and transportation infrastructure made by the City.
- The acquisition, sale, or exchange of land for the purpose of preservation or development.
- Decisions on expenditures for public resources such as streets, parks, and trails, property management and the provision of public services.

## SPECIAL STUDIES AND PLANS

Additional studies and plans can be helpful to further explore and define a vision of a certain area, corridor, or development site. Conducting studies and making decisions area by area can enhance the Comprehensive Plan. Some examples of additional planning efforts that can further develop ideas expressed in PlanGretna include:

- Downtown Master Plan and Implementation
- Facilities Management Plan
- Site Development Plan and Master Plan for City properties
- Strategic Plan Updates and Goal Prioritization

## LAND USE SUITABILITY

How will PlanGretna be implemented? The major assumption of this Comprehensive Plan is:

***“Specific development criteria will be adopted to help guide builders, investors, and community leaders in making good decisions concerning the future of Gretna.”***

These criteria will be specific statements that:

- Describe the relationship between/among land uses.
- Establish criteria or design standards that new development must meet.
- Minimize land use conflicts between neighboring land owners.
- Create consistent characteristics within each land use district.

## LAND USE TRANSITIONS

New development should provide, if needed, screening, buffers, or additional setback requirements when located next to existing uses. Screening or buffers may be plant material, low earthen berms, solid fences, or any combination of the above. Boundaries between different land uses are done along streets, alleys, or natural features (streams, railroads, etc.) whenever possible.

## COMMUNITY ENTRANCES

First impressions of the community are made at the entrances. These impressions are critical to a community’s overall image. New development should have larger setbacks and higher landscaping standards when located at any of the present or future entrances to the community. This in turn with appropriate sign regulations along the major transportation corridors into and from the City will promote a better community image and improve the quality of life in the City of Gretna. Community entrance policies, guidelines and locations are identified in the Goals and Objectives Section and the established Character Areas of this plan.

## ACHIEVING GRETNA'S FUTURE PLAN

In order to solve community problems or concerns, successful city comprehensive plans have the key ingredients of consensus, ideas, hard work, and the application of each of these. This section of the plan contains the inspiration of the many City officials, school district, chamber, economic corporation, and concerned citizens who have participated in the planning process. Nevertheless, the ultimate success of PlanGretna remains in the dedication offered by each and every resident.

There are numerous goals and objectives in this plan and it is recommended to review the relevant goals and objectives during the City planning and budget sessions. However, it is also recommended that the city select elements of the plan for immediate action; the goals of highest priority which will ultimately lead to the success of other goals and objectives. It will be critical to earmark the specific funds to be used and the individuals primarily responsible for implementing the goals, objectives and policies in Gretna. These goals may be identified more specifically and should be consistent with those in the City's strategic plan and/or goal prioritization.

## COMMUNITY GROWTH

New development should, to the greatest extent possible, be contiguous to existing development or services. This would allow for the logical and cost effective extension of streets and utility services. The City may authorize non-contiguous development if:

- The developer pays for the “gap” costs of extending services from the existing connections to the proposed development.
- The extension would open up needed or desirable areas of the community for additional growth.
- Issues related to adjacent/transitional agriculture are properly addressed.

The Future Land Use Plan is one of the statutory requirements of a Comprehensive Development Plan, as stated in the Nebraska State Statutes. The Land Use Plan, along with the Transportation Plan, provides the necessary tools to direct future development in and around Gretna. The City's Land Use Plan is based on existing conditions and projected conditions for the community.

The need for residential uses will be driven by the future population, the ratio of owner-occupied to renter-occupied housing units, and the projected number of future dwelling units needed. New residential development can drive the need for additional commercial development, additional streets, public and park facilities, and industrial development. Therefore, decisions regarding future residential development will have a direct impact on other uses throughout the entire community. Conversely, commercial and industrial development in a community will lead to further economic development and the need for more residential units. Although Gretna continues to grow in population which is contributing to some commercial growth, it is the possibility of industrial and business park developments along major transportation corridors that will lead to additional opportunities and population growth.

## PHASING PLAN

Development in Gretna should occur on an incremental basis to expand services in a cost effective manner. This approach will allow the City to avoid areas of development that are not served by roads, water, sewer, or other utilities. The phasing plan is intended to serve as a guideline to developers and the City to avoid unnecessary costs and promote positive development.

### **Phase One Development**

This area includes infill development within the corporate limits. Not all of the existing vacant properties within Gretna’s corporate limits will be capable of Phase One development. Development constraints such creeks or steep slopes may not be suitable for residential development; however, such area may be suitable for a park or trail system. The primary concern within this area should include the following:

- Development of vacant lots already served by established utilities and community infrastructure
- Redevelopment of substandard or underutilized properties

### **Phase Two Development**

This area includes land located along Highways 6/31 both north and south of Gretna and along Highway 370 east of town. The area needs sewer and water but is serviced by arterial and collector transportation routes that make these areas prime for commercial, industrial, flex space and mixed use development. The primary concern within this area should include the following:

- Construction of a interceptor sanitary sewer line.
- Where necessary, installation of lift stations and force mains
- Construction of collection mains for sewer and water distribution mains
- Protection and planning of the 192nd Street Arterial Corridor

### **Phase Three Development**

Phase Three includes land southeast and northeast of Gretna. Opportunities may be available for development of the Phase Three area prior to completion of Phase Two development; however, these areas are designated for further residential growth and will be more difficult and costly to install new services in the near future until such lines are installed in Phase Two areas. These areas, if developed ahead of schedule, should have services constructed with either the developer assisting with financing the infrastructure upgrades or the developer creating on-site infrastructure that meets Gretna’s, Sarpy County’s, and the State of Nebraska requirements.

Primary concerns with Phase Three include:

- Continued extension of a interceptor line sewer
- Where necessary, a lift station and force main due to changes in topography
- Service mains to collect wastewater and transport to the lift station
- Extension of water distribution lines and continued looping of the system to allow for adequate pressure and flows
- Development of the 192nd Street Arterial and Interchange

**Phase Four Development**

Phase Four areas are considered those areas along and adjacent to Interstate 80. Such areas are designated for business park and mixed use developments and may be dependent on the development/construction of 192<sup>nd</sup> Street and the West Outer Bypass. These areas would have a much higher cost than other phases closer to existing development. Like phase three, these areas, if developed ahead of schedule, should have services constructed with either the developer assisting with financing the infrastructure upgrades or the developer creating on-site infrastructure that meets Gretna's, Sarpy County's, and the State of Nebraska requirements.

- Construction of sanitary sewer interceptor lines
- Where necessary, installation of lift stations and force mains
- Service mains to collect wastewater and transport to the lift station
- Construction of wastewater treatment plant according to the Sarpy County Plan and the Nebraska Department of Environmental Quality Regulations
- Construction of collection mains for sewer and water distribution mains

## ANNEXATION

As the City grows in size it must look for opportunities to extend its borders to continue to provide a high quality of life for its residents. To do this, the State of Nebraska has established a process for communities to expand their municipal boundary via annexation so as to include any contiguous or adjacent lands, lots, tracts, streets, or highways as are urban or suburban in character and in such direction as may be deemed proper.

There are three ways annexation can be pursued. These include:

1. Property owners can request annexation.
2. The municipality can annex any contiguous or adjacent lands, lots, tracts, streets, or highways that are urban or suburban in nature.
3. At the time land is platted adjacent to Gretna's corporate limits it could be annexed at the time of approval of the final plat with a pre-annexation agreement.

In the case of the first method, the property owner must submit a plat prepared by a licensed surveyor. The plat must be approved by the City Engineer and filed with the Clerk along with a written request signed by a majority of the property owners and inhabitants in number and value of the proposed annexation properties. Annexations must be approved by both the Planning Commission and City Council.

To adopt an annexation ordinance, a majority of affirmative votes are required by the governing body at each reading of the ordinance. Then the certified map is filed with the County Assessor, County Clerk, and Register of Deeds along with a certified copy of the ordinance. The City then shall provide substantially the same services as the other inhabitants of the City as soon as practicable, and adequate plans and necessary City Council action to furnish such services shall be adopted not later than one year after the date of annexation.

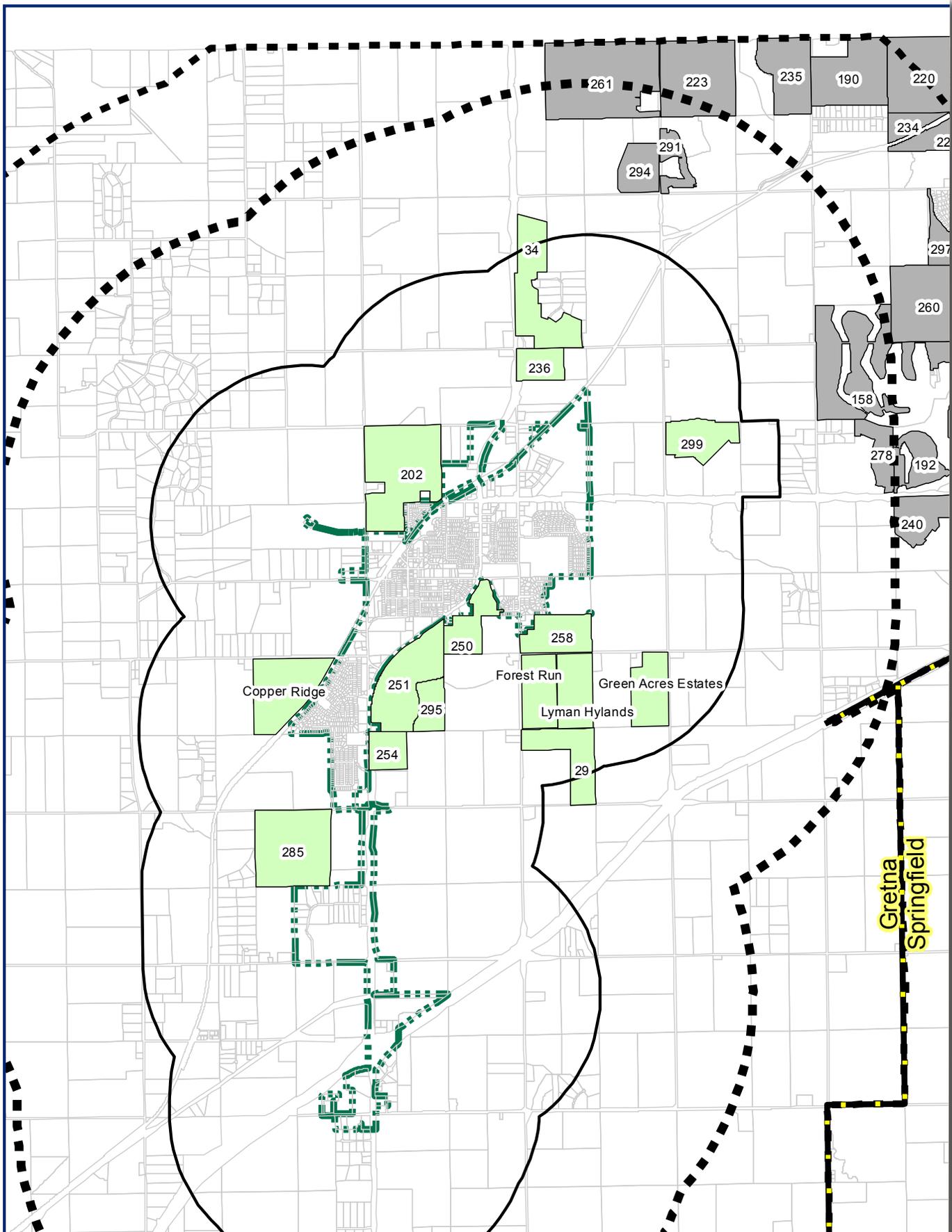
## AREAS FOR POTENTIAL ANNEXATION FOR GRETNA

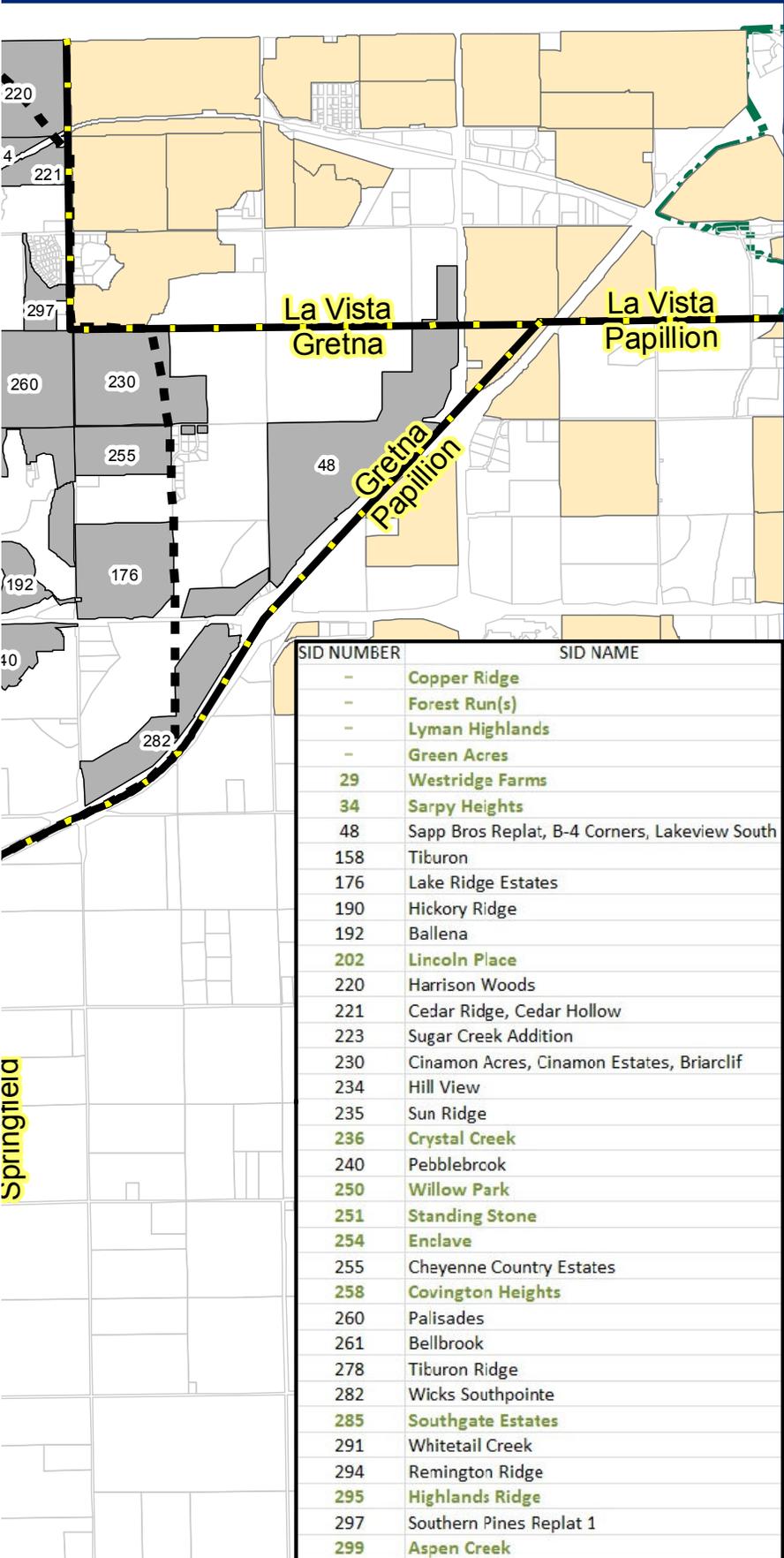
Within Gretna's extraterritorial jurisdiction and the identified growth areas of the City there are currently 31 Sanitary Improvement Districts (SIDs). Of the 31 districts there are a number of these located along the City's current corporate limits which will likely be the first ones considered for annexation in the future. A large number of the remaining SID's are located between US Highway 6/31 and 168<sup>th</sup> Street along Harrison Street and a grouping along Nebraska Highway 370 and Interstate 80. The SIDs surrounding Gretna are identified on the SID and Annexation Map in Figure 28.

The qualifications to be met for an area to be included in the City's corporate limits are requirements imposed by State statutes. The matters to be considered when any area is being evaluated for inclusion in the City's corporate limits include: the requirements of State statutes, the conditions of the infrastructure, the existing and available services, the maintenance of existing services, the extension of City services, the method of finance for the extension of city services, the time for extension of city services, population, taxes and revenue, the area's debts, obligations and assets, land uses and valuation, and the interests of the City.

The City of Gretna has identified several areas or SIDs adjacent to the corporate boundaries as potential areas of annexation. Such areas or SIDs include Lincoln Place, Willow Park, Standing Stone, Southgate Estates, Sarpy Heights, Enclave, Covington Heights, Westridge Farms, and Crystal Creek. Possible rural subdivisions include Copper Ridge, Forest Run, Lyman Highlands, and Green Acres Estates. The City shall review these areas and others when appropriate to consider future growth and annexation. Areas for potential annexation are displayed on the SID and Annexation Map in Figure 28.

FIGURE 28: SID AND ANNEXATION MAP





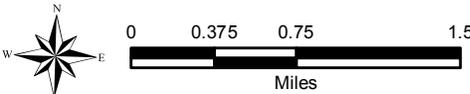
# City of Gretna

## SID and Annexation Map

### Legend

-  SIDs and Subdivisions within the study area that may be eligible for annexation in the future
-  SIDs within the study area but are not currently eligible for annexation
-  Corporate Limits
-  One-Mile ETJ
-  2 Mile Growth Area
-  3 Mile Growth Area

SID NUMBER	SID NAME
-	Copper Ridge
-	Forest Run(s)
-	Lyman Highlands
-	Green Acres
29	Westridge Farms
34	Sarpy Heights
48	Sapp Bros Replat, B-4 Corners, Lakeview South
158	Tiburon
176	Lake Ridge Estates
190	Hickory Ridge
192	Ballena
202	Lincoln Place
220	Harrison Woods
221	Cedar Ridge, Cedar Hollow
223	Sugar Creek Addition
230	Cinamon Acres, Cinamon Estates, Briarclif
234	Hill View
235	Sun Ridge
236	Crystal Creek
240	Pebblebrook
250	Willow Park
251	Standing Stone
254	Enclave
255	Cheyenne Country Estates
258	Covington Heights
260	Palisades
261	Bellbrook
278	Tiburon Ridge
282	Wicks Southpointe
285	Southgate Estates
291	Whitetail Creek
294	Remington Ridge
295	Highlands Ridge
297	Southern Pines Replat 1
299	Aspen Creek



Created By: J. Sather  
 Date: Feb 2009  
 Revised: Aug 2014  
 Software: ArcGIS 10.2  
 File: 140271.00

This map was prepared using information from record drawings supplied by JEO and/or other applicable city, county, federal, or public or private entities. JEO does not guarantee the accuracy of this map or the information used to prepare this map. This is not a scaled plot.



## PLAN MAINTENANCE

PlanGretna is the community’s collective vision, yet, change is inevitable. Major technologies and new community needs will arise during the planning period, which were not foreseen during the plan’s development. Jobs, housing, transportation, goods and services will shift over time. The amendment process to the Comprehensive Plan must accommodate and help manage the inevitable change in a way that best promotes, and does not compromise, the community’s core values, health and well being. The plan amendment process must be an open and fair process, utilizing sound planning, economic, social and ecological principals.

If major, new, innovative development opportunities arise which impact several elements of the plan, and are determined to be of importance, a plan amendment may be proposed and considered separately from the annual review and other proposed plan amendments. The City Council or Planning Commission shall compile a list of proposed amendments received during a year in preparation for a report to provide pertinent information on each proposal, and recommend action on the proposed amendments. The comprehensive plan amendment process should adhere to the adoption process specified by the Nebraska State Statutes and should provide for organized participation and involvement of interested citizens.

Since this plan is a living, breathing document, it needs to be monitored for continued relevancy. Although PlanGretna uses a 20-year planning time period, intervening time points should be utilized to measure progress toward long term goals, to make adjustments based on changed conditions or preferences, and to provide short-and mid-term guidance for land use decisions. In doing so, the ultimate planning time period will move as well, constantly advancing into the future as the currency and relevancy of the plan is maintained by a cycle of periodic updates and amendments.

Once every five years, the Comprehensive Plan should undergo a major update. Five years is recommended as the appropriate time interval for major updates for several reasons. Experience in other communities suggests that more frequent major updates are too much of a burden on staff, while less frequent updates lead to plans that can become seriously out of date. Finally, federal census data is available every decade, making a five year review schedule that falls at the mid-point between census years reasonable and logical. The common elements of a five-year update include reviewing and extending growth projections, reviewing community goals, and analyzing amendments.

## ANNUAL REVIEW OF THE PLAN

A relevant, up to date plan is critical to the on-going planning success. To maintain the confidence of both public and private sectors, evaluate the effectiveness of planning activities and, most importantly, to make mid-plan corrections on the use of City resources, the plan must be current. Thus, an annual review should occur where the Comprehensive Plan Steering Committee, Planning Commission, City Council, residents, and staff are able to review the plan and recommend necessary changes.

After adoption of the comprehensive plan, opportunities should be provided to identify any changes in conditions that would impact elements or policies of the plan. The annual review process needs to involve regularly monitoring trends and changes in the local, regional, state and federal landscape. Such trends and changes may include changes in development activity and use, trends in development regulation amendments, and changes in planning and zoning law. Each year at the annual review, a report should be prepared by the Comprehensive Plan Steering Committee and/or the Planning Commission that provides information and recommendations on whether the plan is current in respect to population and economic changes and if the recommended policies are still valid for the City and its long-term growth.

The Planning Commission should hold a public meeting or hearing on this report to:

1. Provide citizens or developers with an opportunity to comment and/or present possible changes to the plan;
2. Identify any changes in the status of projects or action items called for in the plan; and
3. Bring forth any issues, or identify any changes in conditions, which may impact the validity of the plan.

If the Commission finds major policy issues or major changes in basic assumptions or conditions have arisen which could necessitate revisions to the plan, they should recommend changes or further study of those changes. This process may lead to identification of amendments to PlanGretna that would be processed by staff, recommended by the Planning Commission and considered by the Gretna City Council.

## CONDITIONS FOR PLAN AMENDMENT

Comprehensive Plan amendment procedures are necessary to determine what constitutes conformity or non-conformity with the plan. It is impossible to set hard and fast rules for such decisions but consistent criteria should be used when making this determination. The following criteria are recommended:

- A request for increases in residential density or non-residential floor area in excess of the guidelines established in the plan, depending upon the degree of increase, may require a plan amendment.
- Land use request involving minor differences in boundaries from those shown in the plan should be considered in conformity with the plan unless precedent would be set for more extensive and non-conforming changes in adjacent areas.
- Requests for variations or changes in the alignment of designated roadways should be considered in conformity if the continuity of the roadway is maintained, the alignment does not result in traffic safety problems or reductions in needed capacity, does not constrain the proper development of contiguous properties, and does not conflict with or preempt other planned uses or facilities.
- Requests to deviate from plan-specified requirements such as open space and traffic reduction measures generally should not be permitted in order to ensure equitable treatment of all property owners and to avoid arbitrary decisions which would undermine the legal foundations of the plan. If changes are to be made, they should be done through a plan amendment process.
- The final criteria must always be whether the request, whatever its nature, will set a precedent for cumulative changes which are not consistent with the plan. Therefore, in those instances where the implications of the request are not easily observed or detected, a request for a plan amendment should be required.

## METHODS FOR EVALUATING LAND DEVELOPMENT PROPOSALS

The interpretation of the plan should be comprised of a continuous and related series of analyses, with references to the goals and objectives/policies, the overall land use plan, and specific land use policies. Moreover, when considering specific proposed developments, interpretation of the plan should include a thorough review of all sections of the plan.

If a development proposal is not consistently supported by PlanGretna, serious consideration should be given to making modifications to the proposal, or the following criteria should be used to determine if a comprehensive plan amendment would be justified:

- The character of the adjacent neighborhood
- The zoning and uses on nearby properties
- The suitability of the property for the uses allowed under the current zoning designation
- The type and extent of positive or negative impact that may affect adjacent properties, or the City at large, if the request is approved
- The impact of the proposal on public utilities and facilities
- The length of time that the subject and adjacent properties have been utilized for their current uses
- The benefits of the proposal to the public health, safety, and welfare compared to the hardship imposed on the applicant if the request is not approved
- Comparison between the existing land use plan and the proposed change regarding the relative conformance to the goals and objectives/policies
- Consideration of professional staff recommendations

## RECOMMENDATIONS

As part of the implementation process for PlanGretna, various recommendations or actions are suggested. These recommendations are based upon public input, staff input, observations, and experience. Such recommended actions are classified as immediate, short-term, long-term and continuing. These actions are only recommendations and may change annually. Prioritization of Goals have been adopted in the City’s Strategic Plan and should be referenced.

### IMMEDIATE RECOMMENDATIONS

1. Adopt the Update to PlanGretna .
2. Evaluate current zoning and subdivision regulations and zoning districts based upon the adopted comprehensive plan as updated.
3. Pursue funding opportunities to provide improvements/development of City infrastructure.
4. Work with the Gretna Area Development Corporation, Gretna Chamber, Sarpy County Economic Corporation and other groups to market Gretna for quality commercial and industrial developments.
5. Work with developers along Highways 6/31 and 370 to ensure quality development.
6. Adopt a site/concept plan for the City property and future City facilities.

### SHORT TERM RECOMMENDATIONS

1. Work with Downtown Association, property owners and residents to develop methods and strategies to redevelop the downtown.
2. Develop methods to provide for ongoing maintenance and replacement of facilities and equipment.
3. Work with developers and residents to develop and incorporate trails to connect the various neighborhoods.
4. Work with Sarpy County to establish a layout and protective corridor for 192nd Street and the future I-80 Interchange.

### LONG TERM RECOMMENDATIONS

1. Educate residents, businesses, and property owners with respect to develop methods and policies that reduce the amount of runoff, erosion, and pollutants.
2. Develop a Facilities Management Plan to determine needs for all City facilities.
3. Consider developing and implementing a Capital Improvements Plan.

### CONTINUING RECOMMENDATIONS

1. Develop an annual public education program for residents, businesses, and property owners that provides information about the ongoing planning process and how they can be involved.
2. Organize an annual ‘town hall meeting’ or utilization of social media such as MindMixer to evaluate how implementation strategies are working, if goals and objectives are being achieved, and to identify new issues.



# Appendix

Gretna School District  
Demographics, Sept. 2014

Telecommunications List  
for Omaha/ Council Bluffs Area

Future Land Use and  
Transportation Map (11x17)

Parks and Trails Map (11x17)